

MUNICIPALITY OF ANCHORAGE

**PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT**

**W. 30TH AVENUE & NORTH STAR STREET UPGRADES
SPENARD ROAD TO ARCTIC BOULEVARD**

**PROJECT NUMBER: 16-29
NOVEMBER 2019**

65% DESIGN

PREPARED BY:



3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

APPROVED BY:

KENT KOHLHASE, P.E.
MUNICIPAL ENGINEER

SHEET INDEX		
SHEET NO.	DESCRIPTION	WORK SCHEDULE
GENERAL		
G1	COVER SHEET	ALL
G2	INDEX	ALL
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G5	KEY MAP	ALL
SURVEY		
V1	SURVEY CONTROL	ALL
V2	SURVEY CONTROL	ALL
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V4	RIGHT OF WAY MAP	ALL
V5	RIGHT OF WAY MAP	ALL
V6	RIGHT OF WAY MAP	ALL
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V8	RIGHT OF WAY MAP	ALL
V9	EASEMENT & PERMIT INDEX MAP	ALL
V10	EASEMENT & PERMIT INDEX MAP	ALL
V11	EASEMENT & PERMIT INDEX MAP	ALL
V12	EASEMENT & PERMIT INDEX MAP	ALL
V13	EASEMENT & PERMIT INDEX MAP	ALL
DEMOLITION		
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B2	DEMOLITION PLAN	ALL
B3	DEMOLITION PLAN	ALL
B4	DEMOLITION PLAN	ALL
B5	DEMOLITION SUMMARY TABLES	ALL
B6	DEMOLITION SUMMARY TABLES	ALL
B7	DEMOLITION SUMMARY TABLES	ALL
TYPICAL SECTIONS		
C1	TYPICAL SECTIONS	SCHED A
C2	TYPICAL SECTIONS	SCHED A
C3	TYPICAL SECTIONS	SCHED A
C4	TYPICAL SECTIONS	SCHED A
C5	TYPICAL SECTIONS	SCHED A
ROADWAY		
R1	ROADWAY PLAN & PROFILE	SCHED A
R2	ROADWAY PLAN & PROFILE	SCHED A
R3	ROADWAY PLAN & PROFILE	SCHED A
R4	PATHWAY PLAN & PROFILE	SCHED A
R5	INTERSECTION LAYOUT PLAN	SCHED A
R6	INTERSECTION LAYOUT PLAN	SCHED A
R7	INTERSECTION LAYOUT TABLE	SCHED A
R8	INTERSECTION LAYOUT PLAN	SCHED A
R9	INTERSECTION LAYOUT PLAN	SCHED A
R10	INTERSECTION LAYOUT PLAN	SCHED A
R11	DRIVEWAY RECONSTRUCTION PLAN	SCHED A
ROADWAY SUMMARY TABLES		
T1	ROADWAY SUMMARY TABLES	SCHED A
T2	ROADWAY SUMMARY TABLES	SCHED A
T3	ROADWAY SUMMARY TABLES	SCHED A
ROADWAY DETAILS		
D1	ROADWAY DETAILS	SCHED A
D2	ROADWAY DETAILS	SCHED A
D3	ROADWAY DETAILS	SCHED A
D4	ROADWAY DETAILS	SCHED A
D5	ROADWAY DETAILS	SCHED A
RETAINING WALLS		
RW1	RETAINING WALL PLAN & PROFILE	SCHED A
RW2	RETAINING WALL DETAILS	SCHED A

SHEET INDEX		
SHEET NO.	DESCRIPTION	WORK SCHEDULE
SIGNING & STRIPING		
S1	SIGNING & STRIPING PLAN	SCHED A
S2	SIGNING & STRIPING PLAN	SCHED A
S3	SIGNING & STRIPING PLAN	SCHED A
S4	SIGNING & STRIPING PLAN	SCHED A
S5	SIGNING & STRIPING PLAN	SCHED A
S6	SIGN SCHEDULE SUMMARY	SCHED A
S7	SIGN SCHEDULE SUMMARY	SCHED A
STORM DRAIN		
SD1	STORM DRAIN PLAN & PROFILE	SCHED B
SD2	STORM DRAIN PLAN & PROFILE	SCHED B
SD3	STORM DRAIN PLAN & PROFILE	SCHED B
SD4	STORM DRAIN DETAILS	SCHED B
SD5	STORM DRAIN DETAILS	SCHED B
SD6	STORM DRAIN DETAILS	SCHED B
ILLUMINATION		
I1	ILLUMINATION PLAN	SCHED C
I2	ILLUMINATION PLAN	SCHED C
I3	ILLUMINATION PLAN	SCHED C
I4	ILLUMINATION SCHEDULES	SCHED C
I5	LOAD CENTER DETAILS	SCHED C
I6	PEDESTRIAN LIGHT COLUMN DETAILS	SCHED C
LANDSCAPE		
L1	LANDSCAPE PLAN AND SCHEДУLE	SCHED D
L2	LANDSCAPE ENLARGEMENT	SCHED D
L3	LANDSCAPE ENLARGEMENT	SCHED D
L4	LANDSCAPE ENLARGEMENT	SCHED D
L5	LANDSCAPE DETAILS	SCHED D

WORK SCHEDULES	
A	ROADWAY IMPROVEMENTS
B	DRAINAGE IMPROVEMENTS
C	ILLUMINATION IMPROVEMENTS
D	LANDSCAPING IMPROVEMENTS




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Working Set\01 Civil\02 Design Phase 1\10138.00 Index-Notes.dwg

GENERAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE (MOA) STANDARD SPECIFICATIONS, DATED 2015, (HEREINAFTER REFERRED TO AS MASS), THE LATEST EDITION OF THE ANCHORAGE WATER AND WASTEWATER UTILITY (AWWU) DESIGN AND CONSTRUCTION PRACTICES MANUAL (DCPM) AND THE SPECIAL PROVISIONS.
2. THE LOCATION OF THE EXISTING FEATURES AND UTILITIES SHOWN IN THESE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL FEATURES AND UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE ENGINEER.
3. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION. THE PERMITS SHALL BE MAINTAINED ON THE PROJECT SITE. COPIES SHALL BE GIVEN TO THE ENGINEER.
4. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD TELEPHONE AND ELECTRIC UTILITIES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE AND LOCAL STATUTES, CODES AND GUIDELINES AND THE SHORING AND CLEARANCE REQUIREMENTS OF THE SERVING UTILITY.
5. LIMITS OF ROADWAY EXCAVATION SHOWN ON THE DRAWINGS ARE APPROXIMATE. ACTUAL LIMITS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER DURING CONSTRUCTION OPERATIONS.
6. GEOTECHNICAL (SOILS) INFORMATION IS INCLUDED IN THE CONTRACT DOCUMENTS.
7. ALL WORK SHALL BE PERFORMED WITHIN PUBLIC RIGHT-OF-WAY, PUBLIC USE EASEMENT, SLOPE EASEMENT, TEMPORARY CONSTRUCTION EASEMENT, DRAINAGE EASEMENT, ELECTRIC EASEMENT, INTRAGOVERNMENTAL USE PERMIT OR, TEMPORARY CONSTRUCTION PERMIT AREAS. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL CONDITION, UNLESS OTHERWISE NOTED. REVEGETATION SHALL BE IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS.
8. CONTRACTOR SHALL RESTORE DISTURBED PROPERTY TO PRE-CONSTRUCTION CONDITIONS, UNLESS OTHERWISE DIRECTED BY ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY OUTSIDE OF IDENTIFIED CONSTRUCTION LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE. DISTURBED AREAS NOT BEING PAVED SHALL BE TOPSOILED AND SEEDED WITH SCHEDULE A SEEDING MIX UNLESS OTHERWISE NOTED.
9. PROJECT CLEARING AND GRUBBING LIMITS SHALL COINCIDE WITH THE LIMITS OF DISTURBANCE AS SHOWN ON THE DEMOLITION (B) SHEETS. CONTRACTOR SHALL OBTAIN APPROVAL OF THE CLEARING AND GRUBBING LIMITS BY THE ENGINEER PRIOR TO CLEARING AND GRUBBING, SEE SPECIFICATIONS FOR MORE INFORMATION.
10. SLOPE LIMITS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE ACTUAL SLOPE LIMITS BASED ON PRECONSTRUCTION SURVEY DATA.
11. IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, THE CONTRACTOR SHALL SAW CUT AND REMOVE ADDITIONAL PAVEMENT BEYOND THE INITIAL SAW CUT, A MINIMUM OF 1-FOOT ONTO UNDISTURBED ASPHALT. AT TRANSVERSE JOINTS FINAL SAW CUT LINE SHALL BE SKEWED 15° – 25° PER DETAIL 3, SHEET D4. TACK COAT SHALL BE APPLIED TO THE SAWN FACE OF ASPHALT PRIOR TO BEGINNING PAVING.
12. PAVEMENT CROSS SLOPE ON SIDE STREETS SHALL VARY AT INTERSECTIONS TO PROVIDE POSITIVE DRAINAGE. SEE ROADWAY (R) SHEETS FOR INTERSECTION LAYOUTS.
13. ALL WORK AND MATERIALS REQUIRED FOR REMOVING ANY LITTER OR DEBRIS CREATED BY CONSTRUCTION OPERATIONS WITHIN THE PROJECT LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE.
14. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE SUBGRADE TO A DEPTH TO BE DETERMINED BY THE ENGINEER. NO ORGANIC MATERIAL OR OTHER DELETERIOUS MATERIAL SHALL BE UTILIZED FOR BACKFILL.
15. THE CONTRACTOR SHALL SUBMIT RECORD SURVEY NOTES WITH THE RECORD DRAWINGS.
16. EXCAVATION SHALL BE MEASURED BY EXCAVATED CROSS-SECTION AND SHALL BE LIMITED TO THE PAY LIMITS IDENTIFIED IN THE TYPICAL CROSS SECTIONS, UNLESS ADDITIONAL EXCAVATION IS DIRECTED BY THE ENGINEER.
17. THE PROJECT CENTERLINE STATIONING IS NOT RIGHT-OF-WAY CENTERLINE PER SURVEY CONTROL DRAWING UNLESS OTHERWISE NOTED. SEE SURVEY CONTROL DRAWING FOR HORIZONTAL AND VERTICAL CONTROL AND LAYOUT OF THE PROJECT CENTERLINE.
18. THE EASEMENTS AND TEMPORARY CONSTRUCTION PERMITS ACQUIRED FOR THIS PROJECT MAY HAVE RESTRICTIONS. SEE CONTRACT DOCUMENTS FOR RESTRICTIONS.
19. ALL CURB LOCATIONS, RADIUS MEASUREMENTS AND ELEVATIONS ARE TO THE TOP BACK OF CURB (TBC) UNLESS OTHERWISE NOTED.
20. FURNISH AND INSTALL 4" PIPE INSULATION BOARD (R-20) BETWEEN THE STORM DRAIN IMPROVEMENTS AND THE WATER AND SEWER UTILITIES WHEN THE VERTICAL CLEARANCE IS LESS THAN THREE FEET. IF 18 INCHES OF VERTICAL SEPARATION BETWEEN WATER AND SEWER/STORM DRAINS CANNOT BE MAINTAINED THEN WATER RELOCATION WILL BE REQUIRED. SEWER/STORM DRAIN PIPE JOINTS SHALL BE PLACED AT LEAST NINE (9) FEET FROM A WATER CROSSING.
21. EXISTING WATER AND SEWER SERVICE LINES ARE NOT SHOWN IN THE PROFILES UNLESS SPECIFICALLY CALLED OUT.
22. WATER RESULTING FROM THE CONTRACTOR'S DEWATERING EFFORT MAY NOT BE PUMPED OR OTHERWISE DIVERTED INTO EXISTING STORM DRAINS OR SANITARY SEWERS UNLESS REQUIRED PERMITS, INCLUDING, BUT NOT LIMITED TO, PERMITS FROM THE MOA STORM WATER PLAN REVIEW OFFICE, AWWU, AND THE ALASKA DEPARTMENT OF ENVIRONMENTAL CONSERVATION, ARE OBTAINED BY THE CONTRACTOR. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO DIVERT WATER FROM EXCAVATION ONTO ROADWAYS. THE CONTRACTOR SHALL PROVIDE DISPOSAL SITE FOR EXCESS WATER AND SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS AND APPROVALS. THE CONTRACTOR SHALL PROVIDE COPIES OF PERMITS AND APPROVALS TO THE ENGINEER AND MOA ROW PERMIT OFFICE PRIOR TO BEGINNING DEWATERING.
23. ALL CURB AND GUTTER SHALL BE PAID AS "P.C.C. CURB AND GUTTER (ALL TYPES)" EXCEPT FOR CURBS WITH STEEL CURB FACING WHICH SHALL BE PAID AS "P.C.C. CURB AND GUTTER (TYPE 1, STEEL CURB FACING)".
24. EXISTING UTILITIES AND PROPOSED UTILITIES ARE NOT SHOWN IN THE TYPICAL CROSS SECTIONS.
25. THE MATCH EXISTING ELEVATIONS AS SHOWN IN THE PLANS ARE APPROXIMATE. CONTRACTOR SHALL ADJUST PROPOSED GRADES AS REQUIRED TO MATCH INTO EXISTING ELEVATIONS PER THE DIRECTION OF THE ENGINEER.
26. ALL FILL, USABLE EXCAVATION, AND TRENCH BACKFILL SHALL BE COMPACTED TO NINETY-FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT, PER MASS DIVISION 20 EARTHWORK, BASED ON MODIFIED PROCTOR TEST VALUES. ALL FILLS SHALL BE PLACED IN LIFTS NOT EXCEEDING 12-INCHES.
27. CAUTION!!! THERE ARE EXISTING BUILDING FOUNDATIONS AT UNKNOWN LOCATIONS AND DEPTHS NEAR OR WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION AND DEPTH OF EXISTING BUILDING FOUNDATIONS PRIOR TO CONSTRUCTION. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE. CONTRACTOR SHALL REPAIR BUILDING FOUNDATIONS THAT ARE DAMAGED BY CONTRACTOR'S OPERATIONS AT NO COST TO OWNER.
28. FIRE HYDRANTS WILL BE ADJUSTED TO FINAL GRADE BY AWWU O&M DIVISION ON A REIMBURSABLE BASIS. THE CONTRACTOR IS TO PROVIDE WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE NEED FOR FINAL FIRE HYDRANT ADJUSTMENT. THE WRITTEN NOTICE IS TO CONTAIN, AT A MINIMUM, THE MANUFACTURER AND MODEL NUMBER OF THE HYDRANT AND VERTICAL ADJUSTMENT NEEDED IN SIX (6") INCREMENTS.

CALL BEFORE YOU DIG!!!	
Alaska Digline, Inc. Statewide.	811
Alaska Railroad.	265-2520
Military Fuel Lines	552-3760
State Storm Drains	333-2411

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____		<table><tr><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>BW</td><td>BW</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>RK</td><td>JK</td></tr><tr><td>GAS</td><td>RK</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>RK</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>		DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	BW	BW	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	RK	JK	GAS	RK	JK	TELEPHONE	RK	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td></tr><tr><td>DESIGN CRW BOOK No. 149, 169,</td><td>CB 7D</td><td>See MOA Benchmark Book, Page D-56</td><td>94.77</td><td></td><td></td><td></td><td></td></tr><tr><td>195 & 196</td><td>CB 7C</td><td>See MOA Benchmark Book, Page D-17</td><td>106.10</td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td>GAAB 86</td><td>See MOA Benchmark Book, Page D-18</td><td>104.53</td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR</td><td colspan="3">BASIS OF THIS DATUM GAAB 1972 ADJUST</td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr></table>		FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53																																					ASBUILT								CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST							INSPECTOR																								  		<table><tr><th colspan="4">PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</th></tr><tr><td>16-29</td><td colspan="2">W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1</td><td>ALL</td></tr><tr><th colspan="4">GENERAL NOTES</th></tr><tr><td>SCALE</td><td>HOR. N/A VER. N/A</td><td>GRID SW1629, SW1630 DATE NOV 2019</td><td>STATUS 65% SHEET</td></tr><tr><td colspan="3"></td><td>G3 of G5</td></tr></table>		PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT				16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		ALL	GENERAL NOTES				SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET				G3 of G5
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PROPERTY		
EXISTING	PROPOSED	
		CENTERLINE
		EASEMENT LINE
		PROPERTY LINE
		ROW LINE
		SECTION LINE
		TEMPORARY CONSTRUCTION EASEMENT/PERMIT
UTILITY		
EXISTING	PROPOSED	
		ABANDONED UTILITY
		CABLE TV LINE (UNDERGROUND)
		CABLE TV LINE (OVERHEAD)
		CABLE TV PEDESTAL
		CONTROLLER OR ATR CABINET
		CULVERT
		DRY WELL
		ELECTRIC LINE (UNDERGROUND)
		ELECTRIC LINE (OVERHEAD)
		ELECTRIC & CABLE TV LINE (OVERHEAD)
		ELECTRIC & TELEPHONE LINE (OVERHEAD)
		ELECTRIC, TELEPHONE, CABLE, FIBER OPTIC (OVERHEAD)
		ELECTRIC JB TYPE IA
		ELECTRIC JB TYPE II
		ELECTRIC JB TYPE III
		ELECTRIC LOAD CENTER
		ELECTRIC MANHOLE/JB
		ELECTRIC METER
		ELECTRIC PEDESTAL UNDERGROUND
		ELECTRIC SIGN
		ELECTRIC SWITCH CABINET
		ELECTRIC TRANSFORMER
		ELECTRIC VAULT
		FIBER OPTIC LINE (UNDERGROUND)
		FIBER OPTIC VAULT
		FLOOR DRAIN
		FOOTING DRAIN SERVICE LINE
		FOOTING DRAIN SERVICE CONNECTION
		FUEL LINE
		GAS LINE
		GAS METER
		GAS VALVE
		GUY POLE
		GUY ANCHOR
		JOINT USE UTILITY POLE
		LIGHTING LINE
		LUMINAIRE
		LUMINAIRE (PEDESTRIAN)
		SANITARY SEWER LINE
		SANITARY SEWER MANHOLE
		SANITARY SEWER SERVICE CONNECTION
		SANITARY SEWER CLEANOUT
		STORM DRAIN LINE
		SUBDRAIN LINE
		STORM DRAIN CATCH BASIN
		STORM DRAIN CATCH BASIN MANHOLE OR MH
		STORM DRAIN MANHOLE (TYPE VARIES)
		STUBOUT CAPPED OR PLUGGED END

EXISTING	PROPOSED	
—OT/C—		TELEPHONE & CABLE TV LINE (OH)
—OT—		TELEPHONE LINE (OVERHEAD)
—T—		TELEPHONE LINE (UNDERGROUND)
⊙		TELEPHONE MANHOLE
□ U.T.		TELEPHONE PEDESTAL
413	414	TRAFFIC DETECTOR LOOPS
—TF—		TRAFFIC LINE (UNDERGROUND)
⊙	⊙	TRAFFIC SIGNAL POLE
⊙ ●	⊙ ●	TRAFFIC SIGNAL POLE/LUMINAIRE
○		UTILITY POLE
—W—	—W—	WATER LINE
⌒	⌒	WATER FIRE HYDRANT
⋈	⋈	WATER KEY BOX
⊙ W		WATER MANHOLE
⋈	⋈	WATER VALVE
⊙		WATER WELL
ROADWAY		
EXISTING	PROPOSED	
	— — — — —	APPROX SLOPE LIMITS (CUT)
	⋯ ⋯ ⋯	APPROX SLOPE LIMITS (FILL)
— — — — —	— — — — —	CURB & GUTTER
— — — — —	— — — — —	EDGE OF PAVEMENT
— — — — —	— — — — —	EDGE OF CONCRETE
⋯ ⋯ ⋯	⋯ ⋯ ⋯	GUARDRAIL, BARRIER RAIL
▭	▭	CONCRETE JERSEY BARRIER
▭	▭	RETAINING WALL (TYPE VARIES)
⌒	⌒	STREET SIGN
⋯ ⋯ ⋯	⋯ ⋯ ⋯	UNPAVED (GRAVEL) EDGE OF ROAD/DWY
MISCELLANEOUS		
EXISTING	PROPOSED	
▽	●	BLUFF AREA/ EARTHWORK SLOPE
○		BOLLARD/POST
○		BOULDER
▨		DECK
— — — — —	— — — — —	DRAINAGE ARROW (DIRECTION OF FLOW)
	— — — — —	DRAINAGE DITCH BOTTOM
	— — — — —	DRAINAGE SWALE
— x — x — x — x —	— x — x — x — x —	FENCE (TYPE VARIES)
	— o — o — o —	FENCE (DECORATIVE)
▭	▭	HOUSE OR STRUCTURE
□ M.B.	■ M.B.	MAILBOX (INDIVIDUAL)
▭	▭	MAILBOX (CLUSTER)
● N.B.		NEWS BOX
⊙	⊙	PARKING METER
	1	PARCEL NUMBERS
⊙		SPRINKLER HEAD
⊙		SPRINKLER CONTROL BOX
— — — — —	— — — — —	STREAM/EDGE OF WATERWAY
●		TREE/SHRUB (CONIFEROUS)
●		TREE/SHRUB (DECIDUOUS)
●		TEST BORING OR TEST HOLE
— — — — —		VEGETATION & BRUSH

Diagram illustrating the components of a test hole log:

- TEST HOLE NUMBER
- TOP ELEV. OF TEST HOLE
- TH-XX
- TOP ELEV.=118.87
- TOP OF TEST HOLE
- ASPHALT
- GP, NFS
- SM, F2/F3
- FROST CLASSIFICATION
- SOIL CLASSIFICATION
- WATER LEVEL AND DATE MEASURED
- SP-SM to SP, F1 to NFS
- BOTTOM OF TEST HOLE

1. STANDARD LEGEND AND ABBREVIATIONS SHOWN. NOT ALL LEGEND ITEMS AND ABBREVIATIONS ARE PART OF THIS CONTRACT.
2. SOIL CLASSIFICATION IS BASED UPON UNIFIED SOIL CLASSIFICATION (ASTM D 2487-00), SEE GEOTECHNICAL SOIL BORING LOGS FOR MORE INFORMATION.
3. SEE LEGEND ON SHEET V1 FOR SURVEY CONTROL SYMBOLS. ADDITIONAL LEGEND AND ABBREVIATION ITEMS NOT SHOWN HERE ARE PROVIDED ON SPECIFIC SHEETS THROUGHOUT THE DRAWINGS.

ABBR.	DESCRIPTION	ABBR.	DESCRIPTION
AC	ASPHALT CONCRETE	OC	ON CENTER
AC	ASBESTOS CONCRETE	OCEW	ON CENTER EACH WAY
APPROX	APPROXIMATE	OD	OUTSIDE DIAMETER
BM	BENCH MARK	OGS	OIL AND GRIT SEPARATOR
BOP	BEGINNING OF PROJECT	OH	OVERHEAD
C&G	CURB AND GUTTER	PC	POINT OF CURVATURE
CB	CATCH BASIN	PCC	PORTLAND CONCRETE CEMENT
CBMH	CATCH BASIN MANHOLE		POINT OF CONTINUOUS CURVATURE
CI	CAST IRON	PI	POINT OF INTERSECTION
C/L, CL	CENTERLINE	PL, P/L	PROPERTY LINE
CMP	CORRUGATED METAL PIPE	PCMP	PRECOATED CORRUGATED METAL PIPE
CO	CLEANOUT	PCPEP	PERFORATED CORRUGATED POLYETHYLENE PIPE
CONST	CONSTRUCTION	PT	POINT OF TANGENCY
CPEP	CORRUGATED POLYETHYLENE PIPE		POINT OF TANGENCY
DIA	DIAMETER	PUE	PUBLIC USE EASEMENT
DIP	DUCTILE IRON PIPE	PVC	POINT OF VERTICAL CURVATURE
DW	DETECTABLE WARNING	PVC	POLYVINYL CHLORIDE
DWY	DRIVEWAY	PVI	POINT OF VERTICAL INTERSECTION
E	EAST	PVT	POINT OF VERTICAL TANGENT
ELEC	ELECTRIC / ELECTRICAL	REINF	REINFORCEMENT
ELEV, EL	ELEVATION	ROW, R/W	RIGHT OF WAY
EOP	END OF PROJECT / EDGE OF PAVEMENT	RT, R	RIGHT
F&I	FURNISH AND INSTALL	S	SOUTH
FG	FINISHED GRADE	S/W	SIDEWALK
GALV	GALVINIZED	SS	STAINLESS STEEL
GB	GRADE BREAK	SEC COR	SECTION CORNER
JB	JUNCTION BOX	SI	STREET INTERSECTION
LC	LOAD CENTER	ST	STREET
IAW	IN ACCORDANCE WITH	STA	STATION / STATIONING
ID	INSIDE DIAMETER	STD	STANDARD
IE	INVERT ELEVATION	STRUCT	STRUCTURE
INTX	INTERSECTION	TBC	TOP BACK OF CURB
INV	INVERT	TBM	TEMPORARY BENCH MARK
LF	LINEAR FOOT	TCP	TEMPORARY CONSTRUCTION PERMIT
LT, L	LEFT	TELE	TELEPHONE
LUM	LUMINAIRE	TH	TEST HOLE
MAX	MAXIMUM	TW	TOP OF WALL
ME	MATCH EXISTING	TYP	TYPICAL
MH	MANHOLE	UG	UNDERGROUND
MIN	MINIMUM	UON	UNLESS OTHERWISE NOTED
MON	MONUMENT	UTIL	UTILITY
MSL	MEAN SEA LEVEL	VERT	VERTICAL
N	NORTH	VB	VALVE BOX
N/A	NOT APPLICABLE	VC	VERTICAL CURVE
N.I.C.	NOT IN CONTRACT	W	WEST
NTS	NOT TO SCALE	W/	WITH
NWT	NO WATER TABLE		

1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION
OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____
BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

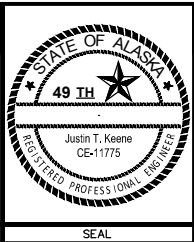
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT
SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

FIELD BOOKS	BW NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR							
CONSTRUCTION RECORD	VERTICAL DATUM			REVISIONS			

CRW
ENGINEERING GROUP, LLC
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

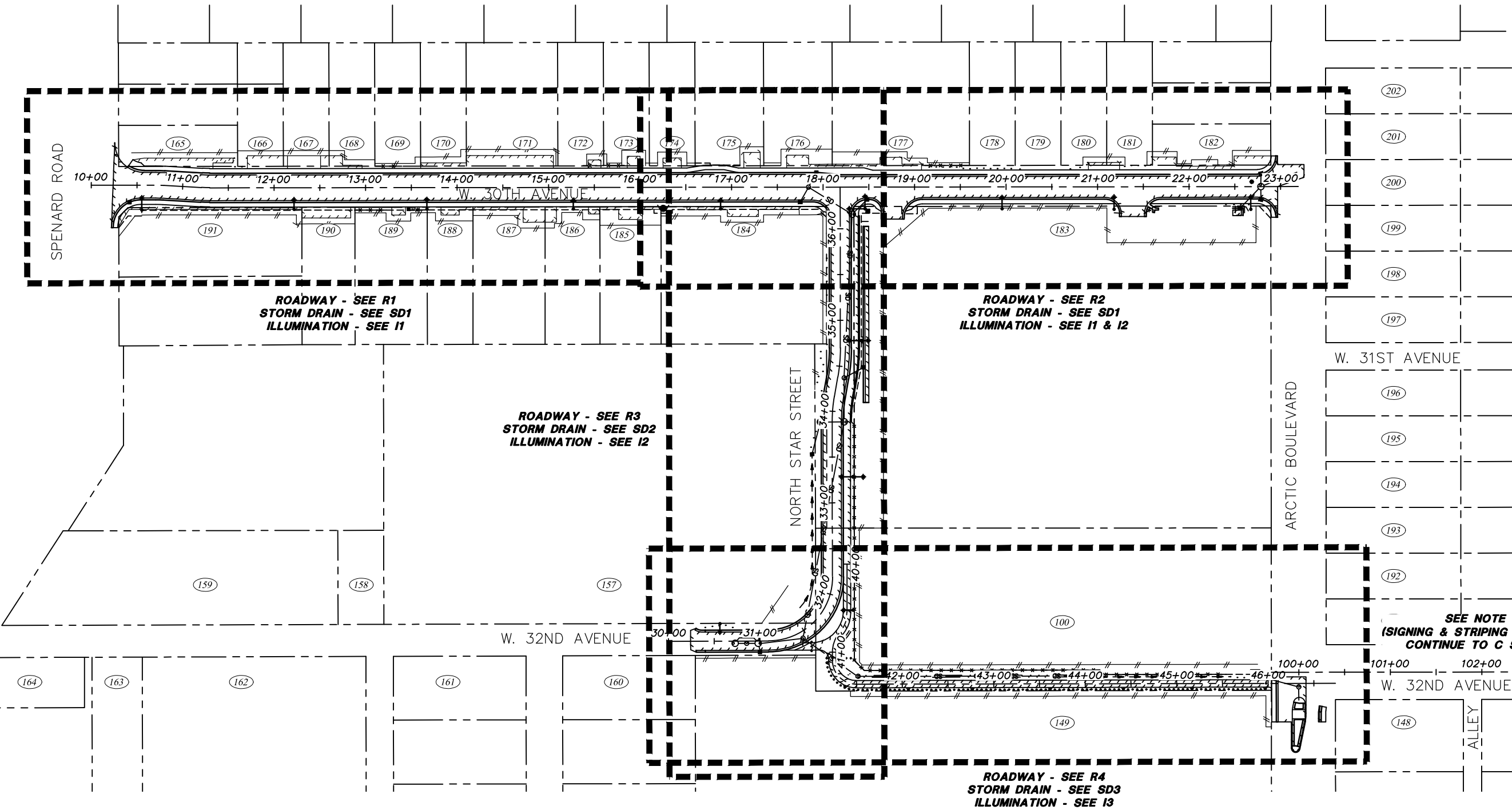
CONSULTANT



16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	AL				
LEGEND AND ABBREVIATIONS						
SCALE	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td colspan="2" style="text-align: center;">GRID SW1629, SW1630</td> </tr> <tr> <td style="width: 50%; text-align: center;">DATE NOV 2019</td> <td style="width: 50%; text-align: center;">STATUS 65%</td> </tr> </table>	GRID SW1629, SW1630		DATE NOV 2019	STATUS 65%	<div style="display: flex; align-items: center; justify-content: center;"> <div style="text-align: right; margin-right: 10px;">SHEET</div> <div style="text-align: center;"> G^4_{of} G5 </div> </div>
GRID SW1629, SW1630						
DATE NOV 2019	STATUS 65%					

NOTES:

1. EXISTING UTILITIES, FEATURES & EASEMENTS ARE NOT SHOWN FOR CLARITY.
2. NOT ALL SHEETS ARE CALLED OUT FOR CLARITY.



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

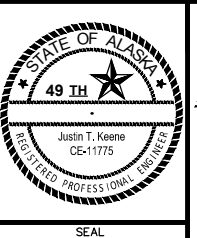
DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
PLAN CHECK								
CONSTRUCTION RECORD								
VERTICAL DATUM								
REVISIONS								
CONSULTANT								
SEAL								

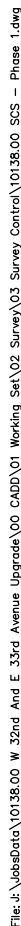


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL






KEY MAP




SCALE HOR. 1"=60' GRID SW1629, SW1630 VER. N/A DATE NOV 2019 STATUS 65% SHEET 5 of 5



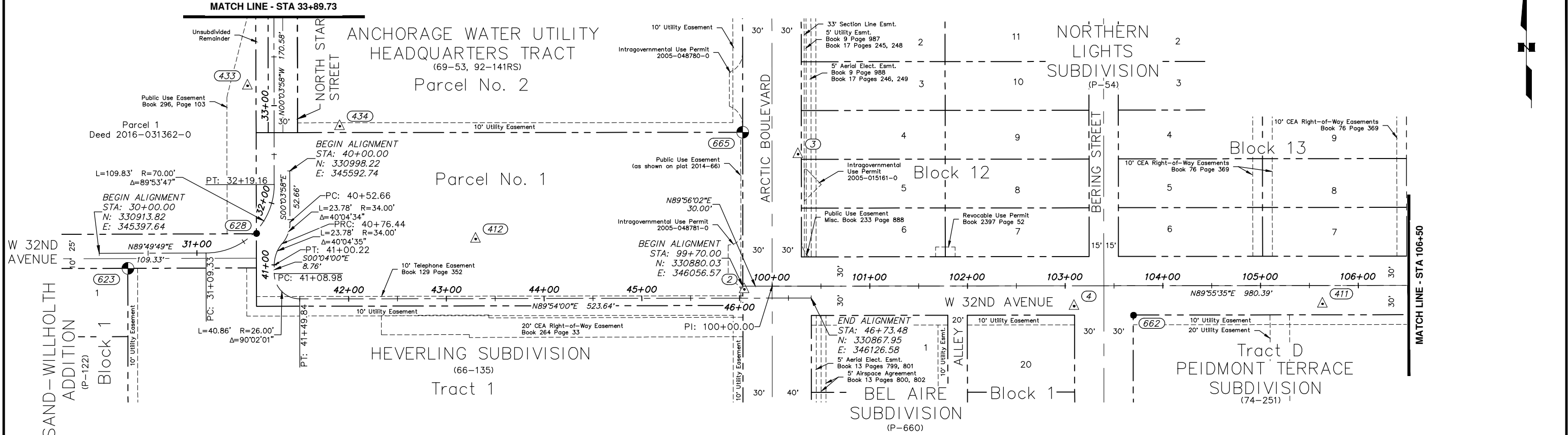
Horizontal Control – North Star Street Alignment					
Point	Station	Offset	Northing	Easting	Description
432	34+26.57	7.77 Rt	331189.97	345588.33	Set 1 1/8" Copper Survey Marker flush with pavement
435	34+56.96	73.55 Rt	331216.22	345658.39	Set 1 1/8" Copper Survey Marker flush with pavement
625	36+24.43	15.00 Rt	331388.87	345600.29	Found 1 1/4" Brass Cap in 2" Iron Pipe 0.8' below grade
430	36+28.34	198.71 Lt	331392.54	345386.57	Set 1 1/8" Copper Survey Marker flush with pavement
436	36+30.66	177.16 Rt	331395.29	345762.44	Set 1 1/8" Copper Survey Marker flush with pavement
431	--	--	331431.47	345584.73	Set 1 1/8" Copper Survey Marker flush with pavement

LEGEND

	Existing Brass Cap
	Existing Aluminum Cap
	Existing Rebar or Iron Pipe
	Control set by CRW
	Control Point Number

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____			DATA DRAWN BY: _____ CHECKED BY: _____ BASE TS MJ TOPOGRAPHY BW BW PROFILE RB JK STORM SEWER MV JH WATER/SANITARY SEWER RK JK GAS RK JK TELEPHONE RK JK ELECTRIC JH TK DESIGN RB JK QUANTITIES RB JK PRELIMINARY/FINAL RB JK MUNICIPAL/STATE RB JK			GRAPHIC SCALE 100 50 0 50 100 FIELD BOOKS CB NO. LOCATION ELEV. REV. DATE DESCRIPTION BY DESIGN CRW BOOK NO. 149, 169, CB 7D See MOA Benchmark Book, Page D-56 94.77 195 & 196 CB 7C See MOA Benchmark Book, Page D-17 106.10 STAKING GAAB 86 See MOA Benchmark Book, Page D-18 104.53 ASBUILT CONTRACTOR BASIS OF THIS DATUM GAAB 1972 ADJUST INSPECTOR										 3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #A0CLB82-AK						PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL SURVEY CONTROL W. 30TH AVENUE - SPENARD ROAD TO ARCTIC BLVD NORTH STAR ST - STA 33+89 TO STA 36+46									
2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____																CONSULTANT		SEAL		SCALE HOR. 1"=50' VER. N/A		GRID SW6629, SW6630 DATE NOV 2019 STATUS 65%		SHEET V1 of V13							
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____			PLAN CHECK			CONSTRUCTION RECORD										VERTICAL DATUM		REVISIONS													

File: s:\jobdata\10136.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey Control\10136.00 SC5 - Phase 1.dwg



Horizontal Control – North Star Street Alignment						
Point	Station	Offset	Northing	Easting	Description	
623	30+29.29	15.57 Rt	330898.33	345426.97	Found 1/2" Copperweld in 2" Iron Pipe 0.4' below ground	
628	31+65.38	2.12 Rt	330934.08	345558.68	Found 5/8" Rebar with Orange Plastic Cap 0.1' above grade	
412	32+05.14	211.90 Rt	330928.38	345783.09	Set 5/8" Rebar with Red Plastic Cap	
434	32+78.86	67.31 Rt	331044.00	345644.00	Set 1 1/8" Copper Survey Marker flush with pavement	
433	33+20.33	27.05 Lt	331085.36	345549.59	Set 1 1/8" Copper Survey Marker flush with pavement	

Horizontal Control – Pathway Alignment						
Point	Station	Offset	Northing	Easting	Description	
628	40+81.35	27.38 Rt	330934.08	345558.68	Found 5/8" Rebar with Orange Plastic Cap 0.1' above grade	
623	41+03.48	149.91 Rt	330898.33	345426.97	Found 1/2" Copperweld in 2" Iron Pipe 0.4' below ground	
434	41+91.20	176.90 Lt	331044.00	345644.00	Set 1 1/8" Copper Survey Marker flush with pavement	
412	43+30.09	61.03 Lt	330928.38	345783.09	Set 5/8" Rebar with Red Plastic Cap	
665	46+03.47	169.88 Lt	331037.70	346056.27	Found 3 1/4" Brass Cap 0.35' below rim of monument case	
2	46+05.04	9.03 Lt	330876.86	346058.12	Set 1 1/8" Copper Survey Marker flush with sidewalk	
3	46+60.26	147.41 Lt	331015.33	346113.10	Set 1 1/8" Copper Survey Marker flush with sidewalk	

Horizontal Control – W 32nd Avenue Alignment						
Point	Station	Offset	Northing	Easting	Description	
665	--	--	331037.7	346056.27	Found 3 1/4" Brass Cap 0.35' below rim of monument case	
2	99+71.55	3.18 Rt	330876.86	346058.12	Set 1 1/8" Copper Survey Marker flush with sidewalk	
3	100+26.71	135.23 Lt	331015.33	346113.10	Set 1 1/8" Copper Survey Marker flush with sidewalk	
4	103+09.15	20.87 Rt	330859.60	346395.74	Set 1 1/8" Copper Survey Marker flush with sidewalk	
662	103+69.84	30.27 Rt	330850.27	346456.45	Found 5/8" Rebar 0.1' below grade	
411	105+63.42	18.78 Rt	330862.01	346650.01	Set 1 1/8" Copper Survey Marker flush with sidewalk	

- LEGEND**
- Existing Brass Cap
 - Existing Aluminum Cap
 - Existing Rebar or Iron Pipe
 - Control set by CRW
 - Control Point Number

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

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DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

<div><div>10050050100</div><div>GRAPHICSCALE</div></div>								
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR								
CONSTRUCTION RECORD		VERTICAL DATUM			REVISIONS			



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

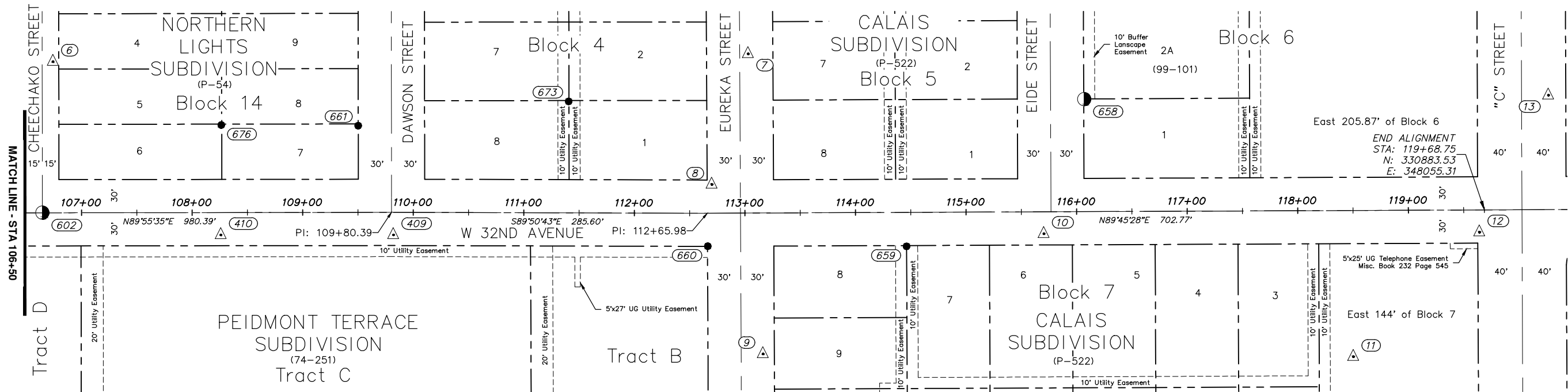
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

SURVEY CONTROL

NORTH STAR ST-STA 30+00 TO STA 33+89/PATHWAY - STA 40+00 TO ARCTIC BLVD/ W. 32ND AVE-ARCTIC BLVD TO STA 106+50

SCALE HOR. 1"=50' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V2 of V13

File: s:_data\10136.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey Control\10136.00 SCS - Phase 1.dwg



Horizontal Control – W 32nd Avenue Alignment					
Point	Station	Offset	Northing	Easting	Description
602	106+64.63	0.07 Lt	330881.00	346751.20	Found 1 1/2" Aluminum Cap flush with asphalt
6	106+74.47	136.71 Rt	331017.64	346760.86	Set 1 1/8" Copper Survey Marker flush with pavement
410	108+26.07	20.19 Rt	330860.94	346912.66	Set 1 1/8" Copper Survey Marker flush with sidewalk
676	108+26.52	79.16 Lt	330960.29	346912.98	Found 5/8" Rebar 0.6' below grade
661	109+50.34	78.41 Lt	330959.70	347036.80	Found 5/8" Rebar flush with ground
409	109+82.26	20.38 Rt	330860.95	347068.77	Set 1 1/8" Copper Survey Marker flush with sidewalk
673	111+40.30	100.81 Lt	330981.70	347227.13	Found 5/8" Rebar, bent
660	112+66.27	29.88 Rt	330850.68	347352.96	Found 5/8" Rebar 0.3' below grade
8	112+70.24	25.93 Lt	330906.51	347356.70	Set 1 1/8" Copper Survey Marker flush with sidewalk
7	113+03.61	144.00 Lt	331024.71	347389.57	Set 1 1/8" Copper Survey Marker flush with pavement
9	113+15.93	127.49 Rt	330753.28	347403.03	Set 1 1/8" Copper Survey Marker flush with sidewalk
659	114+46.03	30.74 Rt	330850.58	347532.72	Found 5/8" Rebar 0.3' below grade
10	115+70.37	19.88 Rt	330861.97	347657.02	Set 1 1/8" Copper Survey Marker flush with sidewalk
658	116+07.15	101.70 Lt	330983.70	347693.28	Found 3 1/4" Aluminum Cap 0.2' below grade
11	118+49.77	132.30 Rt	330750.73	347936.89	Set 1 1/8" Copper Survey Marker flush with pavement
12	119+64.25	20.15 Rt	330863.36	348050.89	Set 1 1/8" Copper Survey Marker flush with sidewalk
13	--	--	330987.24	348113.29	Set 1 1/8" Copper Survey Marker flush with sidewalk

- LEGEND
- Existing Brass Cap
 - Existing Aluminum Cap
 - Existing Rebar or Iron Pipe
 - Control set by CRW
 - Control Point Number

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____ DATE: _____

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COMPANY: _____ DATE: _____

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DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

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TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC SCALE					
100	50	0	50	100	
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77	
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10	
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53	
ASBUILT		CONTRACTOR	INSPECTOR	BASIS OF THIS DATUM	
				GAAB 1972 ADJUST	
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM	
				REVISIONS	
				CONSULTANT	
				SEAL	



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

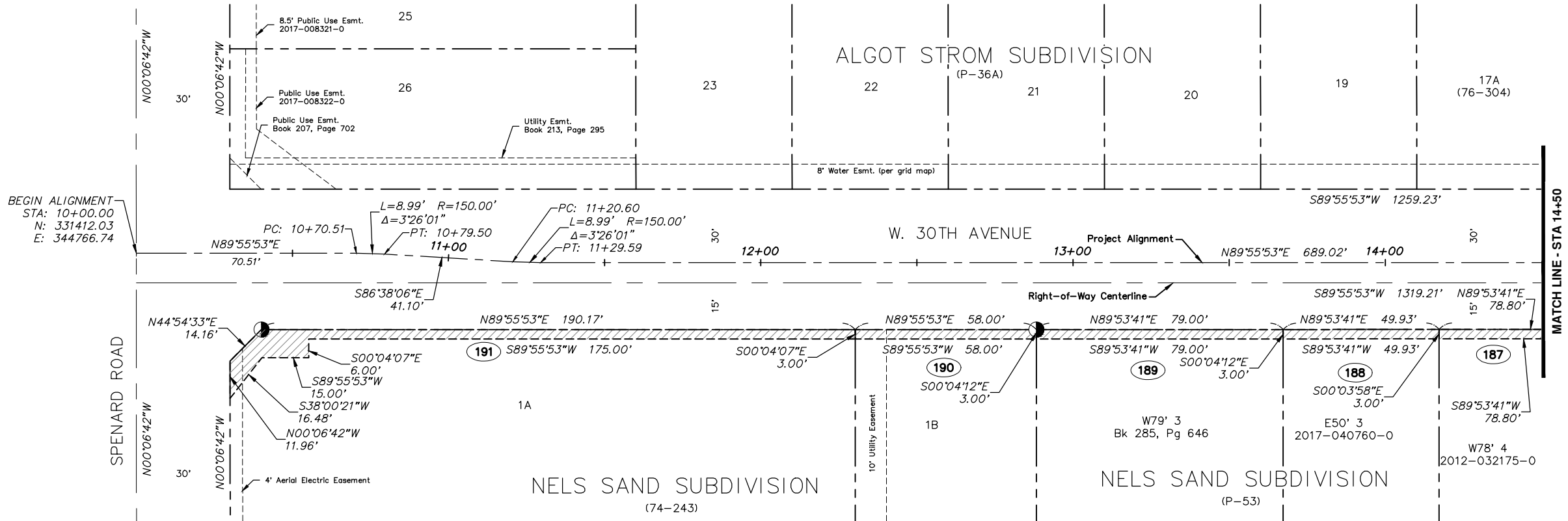
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY – PHASE 1 ALL

SURVEY CONTROL

W. 32ND AVENUE – STA 106+50 TO EOP

SCALE HOR. 1"=50' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V3 of V13

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey\02 ROW Base\10138.00 ROW Map - Phase 1.dwg



PARCEL INFORMATION						EASEMENT ACQUISITION INFORMATION	
PARCEL	LOT	BLOCK	SUBDIVISION	PLAT	OWNER	PUE (SF)	RECORDER'S SERIAL NUMBER
187	W78' Tract 4		Nels Sand	P-53	WILBER MICHELLE M	236	
188	E50' Tract 3		Nels Sand	P-53	VINNITSKY VICTOR A	150	
189	W79' Tract 3		Nels Sand	P-53	COLTER ALAN W	237	
190	1B		Nels Sand	74-243	JACKSON VERDELLA M	174	
191	1A		Nels Sand	74-243	BERGER REAL ESTATE LLC	767	

LEGEND

- 1 Parcel Number
- Public Use Easement (PUE)
- Drainage Easement (DE)
- Existing Brass Cap or Copperweld
- Existing Aluminum Cap
- Existing Rebar or Iron Pipe

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

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COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
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TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	GK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC SCALE					
40	20	0	20	40	
FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77	
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10	
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53	
ASBUILT		CONTRACTOR			
INSPECTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST			
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM	
				REVISIONS	
				CONSULTANT	
				SEAL	



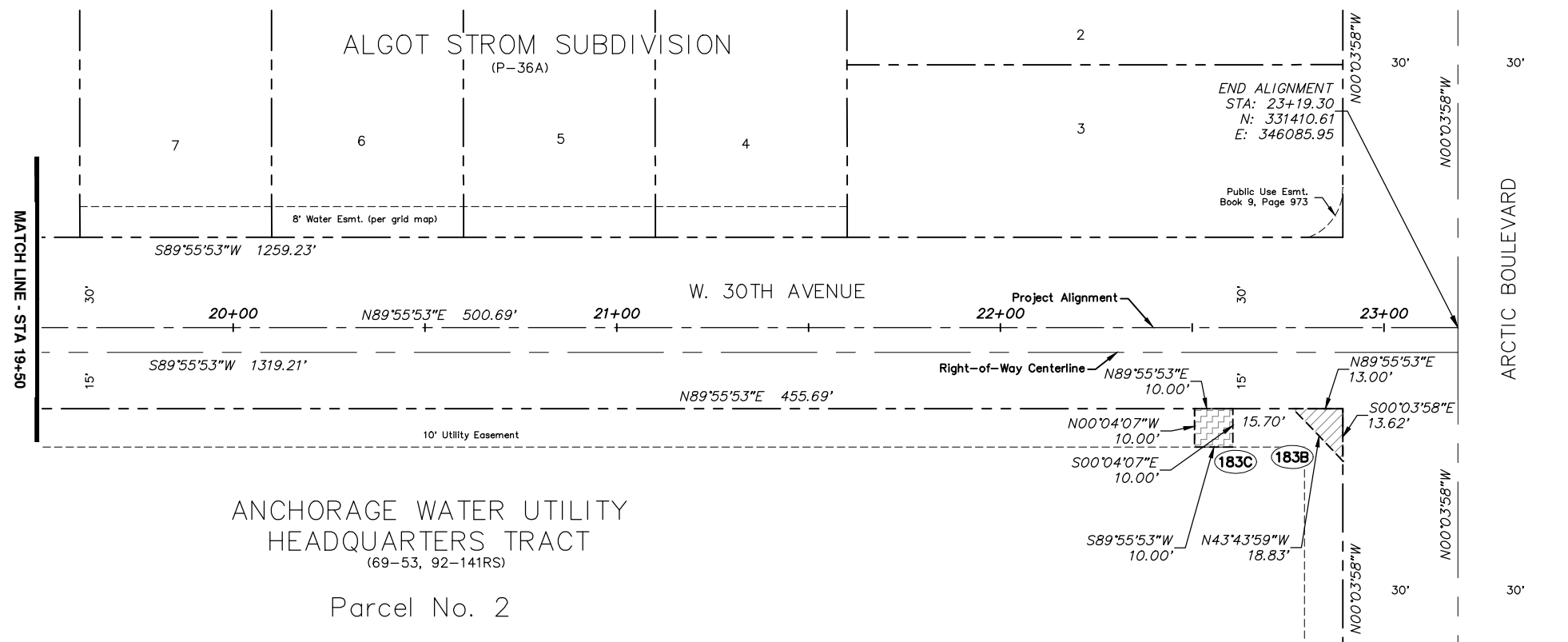
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

RIGHT OF WAY MAP







W. 30TH AVE - SPENARD ROAD TO STA 14+50

SCALE HOR. 1"=20' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V4 of V13



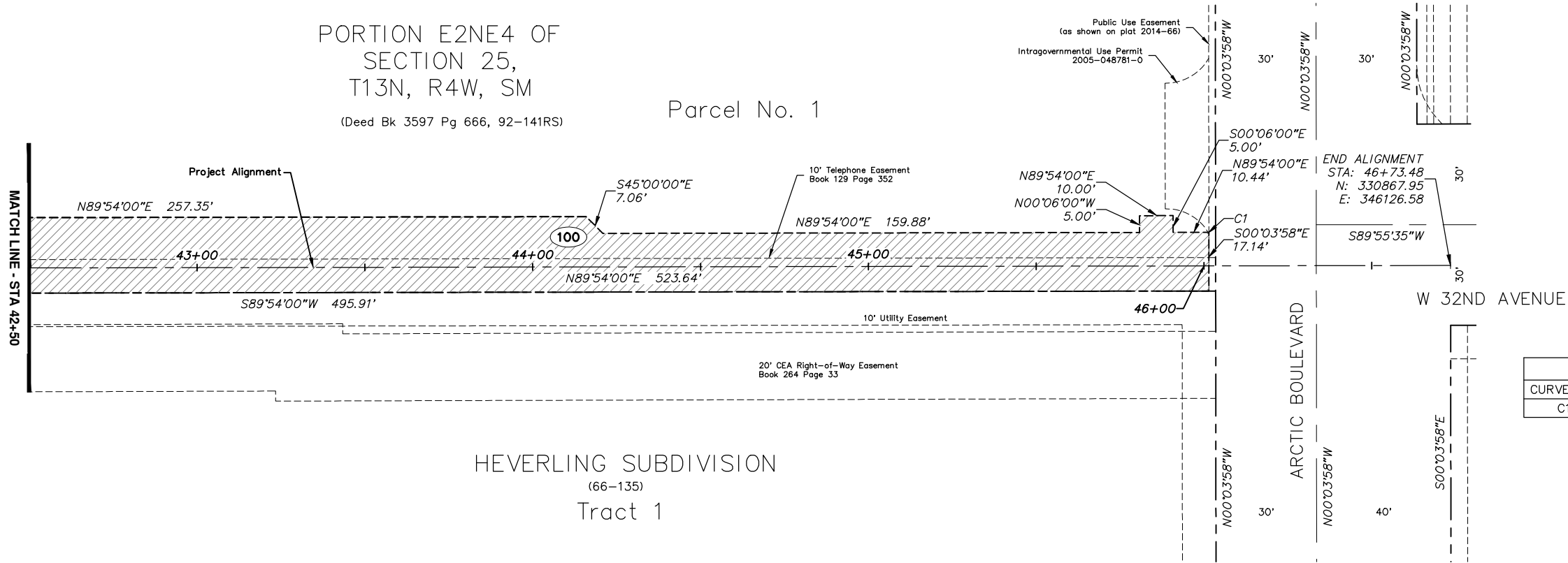
PARCEL INFORMATION						EASEMENT ACQUISITION INFORMATION		
PARCEL	LOT	BLOCK	SUBDIVISION	PLAT	OWNER	PUE (SF)	DE (SF)	RECORDER'S SERIAL NUMBER
183B			Anchorage Water Utility Headquarters Tract	69-53, 92-141RS	MOA AWWU	89		
183C			Anchorage Water Utility Headquarters Tract	69-53, 92-141RS	MOA AWWU		100	

LEGEND

- | | |
|---|----------------------------------|
|  | Parcel Number |
|  | Public Use Easement (PUE) |
|  | Drainage Easement (DE) |
|  | Existing Brass Cap or Copperweld |
|  | Existing Aluminum Cap |
|  | Existing Rebar or Iron Pipe |

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____		<table border="1"> <tr> <th>DATA</th> <th>DRAWN BY</th> <th>CHECKED BY</th> </tr> <tr> <td>BASE</td> <td>TS</td> <td>MJ</td> </tr> <tr> <td>TOPOGRAPHY</td> <td>BW</td> <td>BW</td> </tr> <tr> <td>PROFILE</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>STORM SEWER</td> <td>MV</td> <td>JH</td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>RK</td> <td>JK</td> </tr> <tr> <td>GAS</td> <td>RK</td> <td>JK</td> </tr> <tr> <td>TELEPHONE</td> <td>RK</td> <td>JK</td> </tr> <tr> <td>ELECTRIC</td> <td>JH</td> <td>TK</td> </tr> <tr> <td>DESIGN</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>QUANTITIES</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB</td> <td>JK</td> </tr> </table>		DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	BW	BW	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	RK	JK	GAS	RK	JK	TELEPHONE	RK	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<div style="text-align: center;"> 40 20 0 20 40 GRAPHIC SCALE </div> <table border="1"> <thead> <tr> <th colspan="2">FIELD BOOKS</th> <th>BM NO.</th> <th>LOCATION</th> <th>ELEV.</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> </thead> <tbody> <tr> <td>DESIGN CRW BOOK No. 149, 169,</td> <td>CB 7D</td> <td>See MOA Benchmark Book, Page D-56</td> <td>94.77</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>195 & 196</td> <td>CB 7C</td> <td>See MOA Benchmark Book, Page D-17</td> <td>106.10</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>STAKING</td> <td>GAAB 86</td> <td>See MOA Benchmark Book, Page D-18</td> <td>104.53</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </tbody> </table> <table border="1"> <thead> <tr> <th>ASBUILT</th> <th>CONTRACTOR</th> <th>INSPECTOR</th> </tr> </thead> <tbody> <tr> <td colspan="3">BASIS OF THIS DATUM: GAAB 1972 ADJUST</td> </tr> </tbody> </table>		FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77						195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10						STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53						ASBUILT	CONTRACTOR	INSPECTOR	BASIS OF THIS DATUM: GAAB 1972 ADJUST			<div style="display: flex; align-items: center;"> <div style="margin-left: 10px;"> <p>CRW ENGINEERING GROUP LLC</p> <p>3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC0882-AK</p> </div> </div>		<div style="display: flex; align-items: center;"> </div>		<div style="display: flex; align-items: center;"> </div>		<div style="text-align: center;"> PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 RIGHT OF WAY MAP W. 30TH AVE - STA 19+50 TO ARCTIC BLVD </div>			
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File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey\02 ROW Base\10138.00 ROW Map - Phase 1.dwg



PARCEL INFORMATION						EASEMENT ACQUISITION INFORMATION	
PARCEL	LOT	BLOCK	SUBDIVISION	PLAT	OWNER	PUE (SF)	RECORDER'S SERIAL NUMBER
100			T13N R4W SEC 25, S2S2NE4NE4,N2N2SE4NE4 PTN	92-141RS	MOA AWWU	16926	

LEGEND

- 1 Parcel Number
- Public Use Easement (PUE)
- Drainage Easement (DE)
- Existing Brass Cap or Copperweld
- Existing Aluminum Cap
- Existing Rebar or Iron Pipe

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

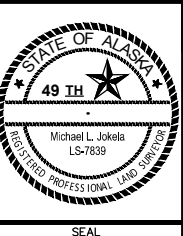
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COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
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TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
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QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS				BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196				CB 7D	See MOA Benchmark Book, Page D-56	94.77				
STAKING				CB 7C	See MOA Benchmark Book, Page D-17	106.10				
				GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT										
CONTRACTOR										
INSPECTOR										
PLAN CHECK										
CONSTRUCTION RECORD										
VERTICAL DATUM										
REVISIONS										
CONSULTANT										
SEAL										



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

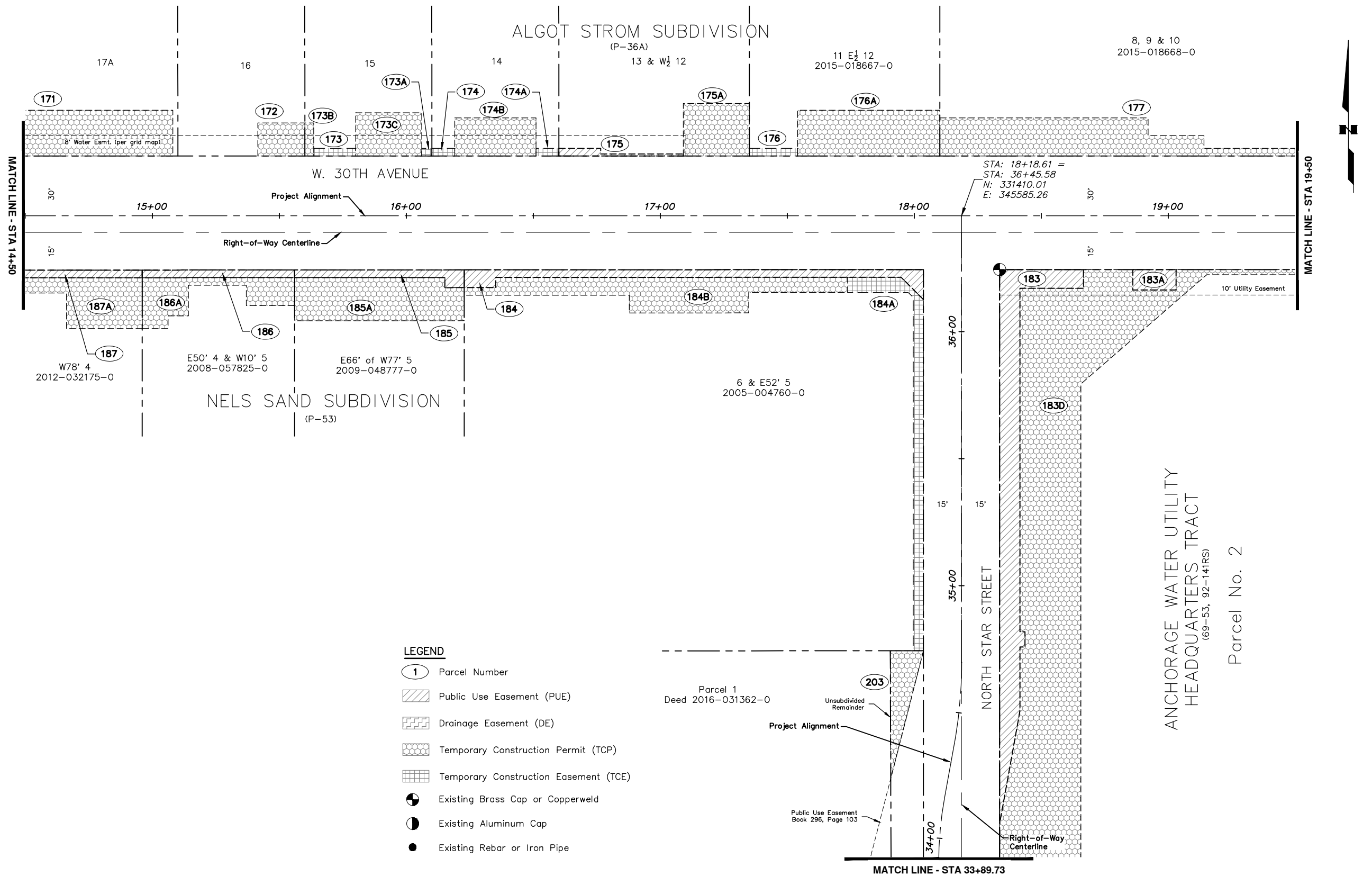
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

RIGHT OF WAY MAP

PATHWAY - STA 42+50 TO ARCTIC BLVD

SCALE HOR. 1"=20' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V8 of V13

File: s:\data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey\02 ROW Base\10138.00 Easmt-Permit Index Map - Phase 1.dwg



LEGEND

- 1 Parcel Number
- Public Use Easement (PUE)
- Drainage Easement (DE)
- Temporary Construction Permit (TCP)
- Temporary Construction Easement (TCE)
- Existing Brass Cap or Copperweld
- Existing Aluminum Cap
- Existing Rebar or Iron Pipe

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COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	JS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
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ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECCL882-AK

STATE OF ALASKA
49 TH
Michael L. Jokela
REGISTERED PROFESSIONAL LAND SURVEYOR
LS-7839



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

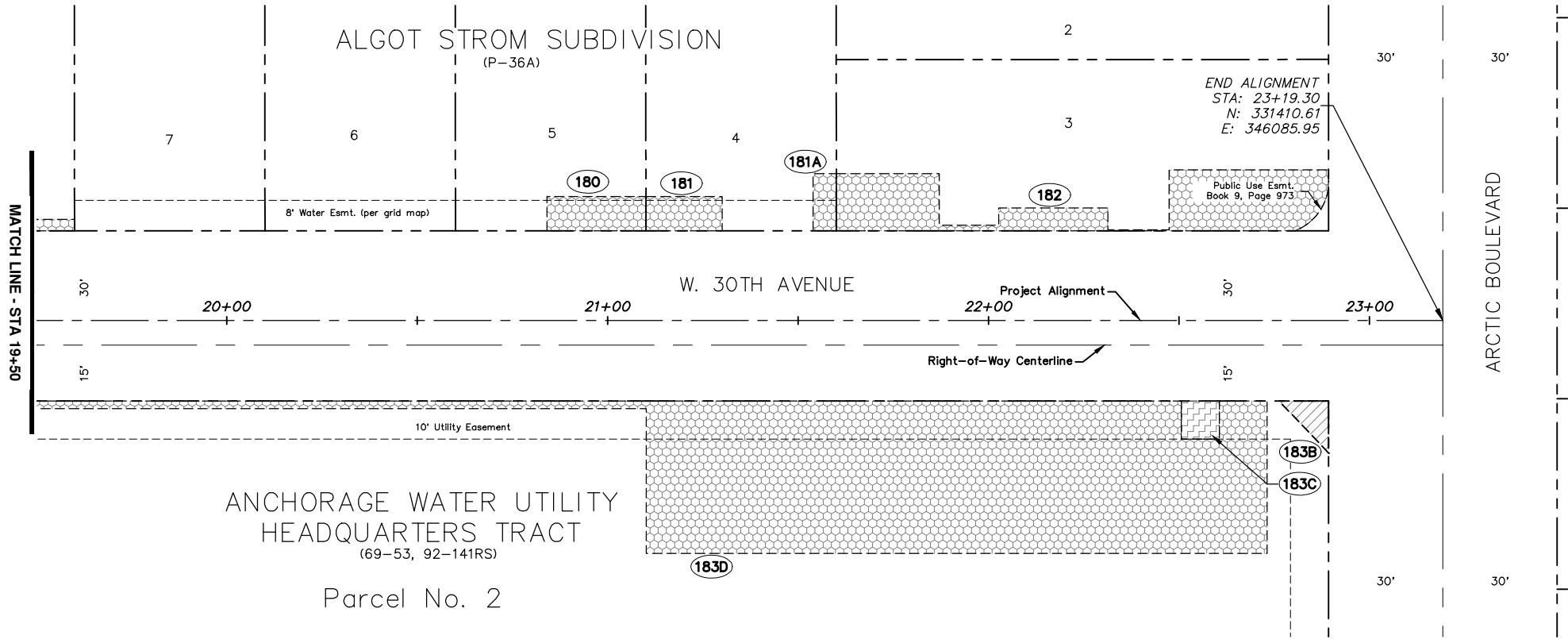
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

EASEMENT & PERMIT INDEX MAP

W. 30TH AVE - STA 14+50 TO STA 19+50
NORTH STAR ST - STA 33+89 TO STA 36+46

SCALE HOR. 1"=20' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V10 of V13

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey\02 ROW Base\10138.00 Easmt-Permit Index Map - Phase 1.dwg



LEGEND

- 1 Parcel Number
- Public Use Easement (PUE)
- Drainage Easement (DE)
- Temporary Construction Permit (TCP)
- Temporary Construction Easement (TCE)
- Existing Brass Cap or Copperweld
- Existing Aluminum Cap
- Existing Rebar or Iron Pipe

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____

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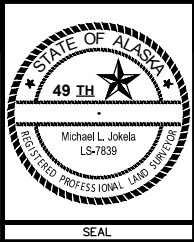
BY: _____

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TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
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FIELD BOOKS				BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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				GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT										
CONTRACTOR				BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR										

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLE882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

EASEMENT & PERMIT INDEX MAP

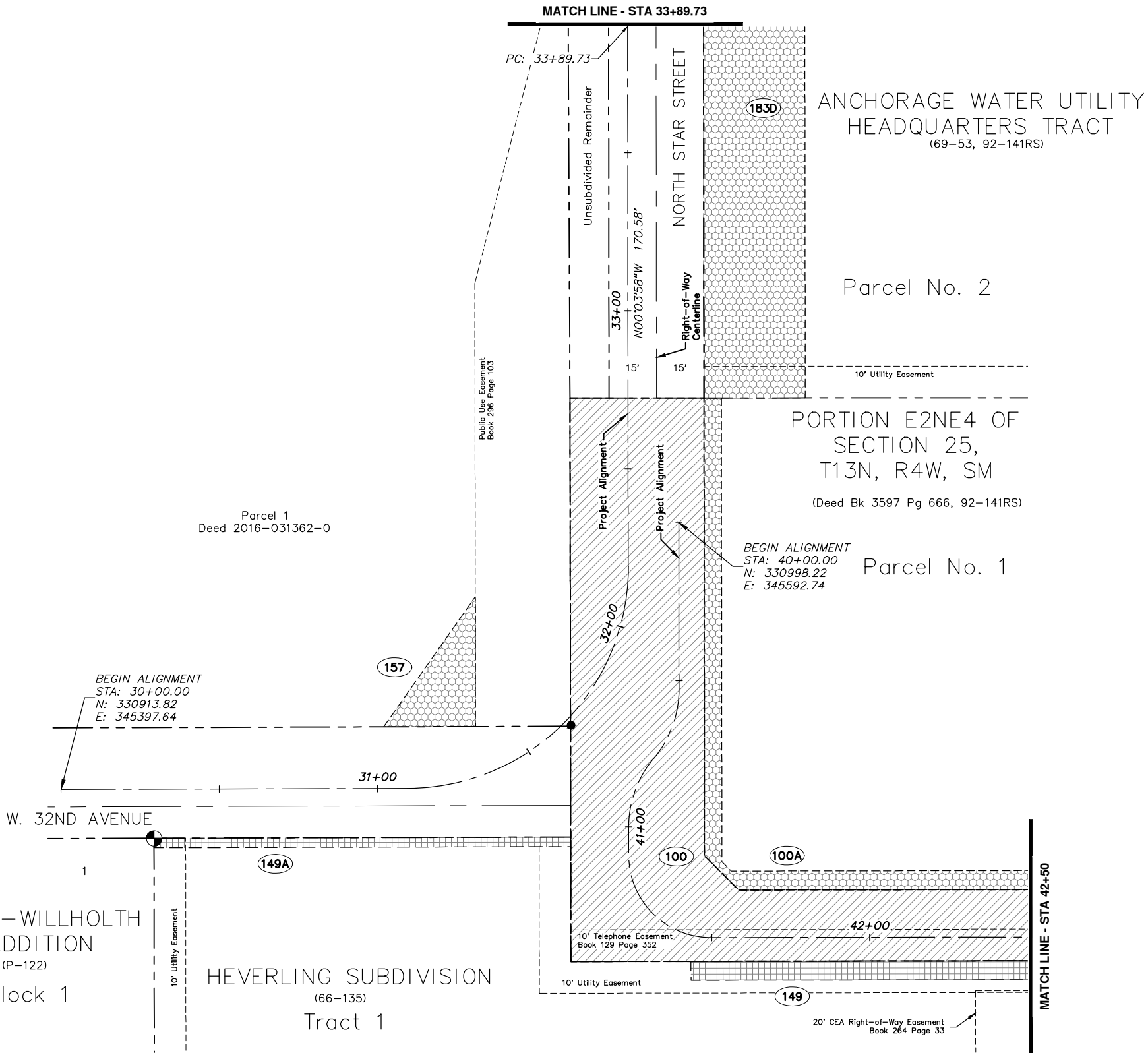
W. 30TH AVE - STA 19+50 TO ARCTIC BLVD

SCALE HOR. 1"=20' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET V11 of V13

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\02 Survey\02 ROW Base\10138.00 Easmt-Permit Index Map - Phase 1.dwg

LEGEND

- 1 Parcel Number
- Public Use Easement (PUE)
- Drainage Easement (DE)
- Temporary Construction Permit (TCP)
- Temporary Construction Easement (TCE)
- Existing Brass Cap or Copperweld
- Existing Aluminum Cap
- Existing Rebar or Iron Pipe



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL

EASEMENT & PERMIT INDEX MAP

NORTH STAR ST - STA 30+00 TO STA 33+89
PATHWAY - STA 40+00 TO STA 42+50

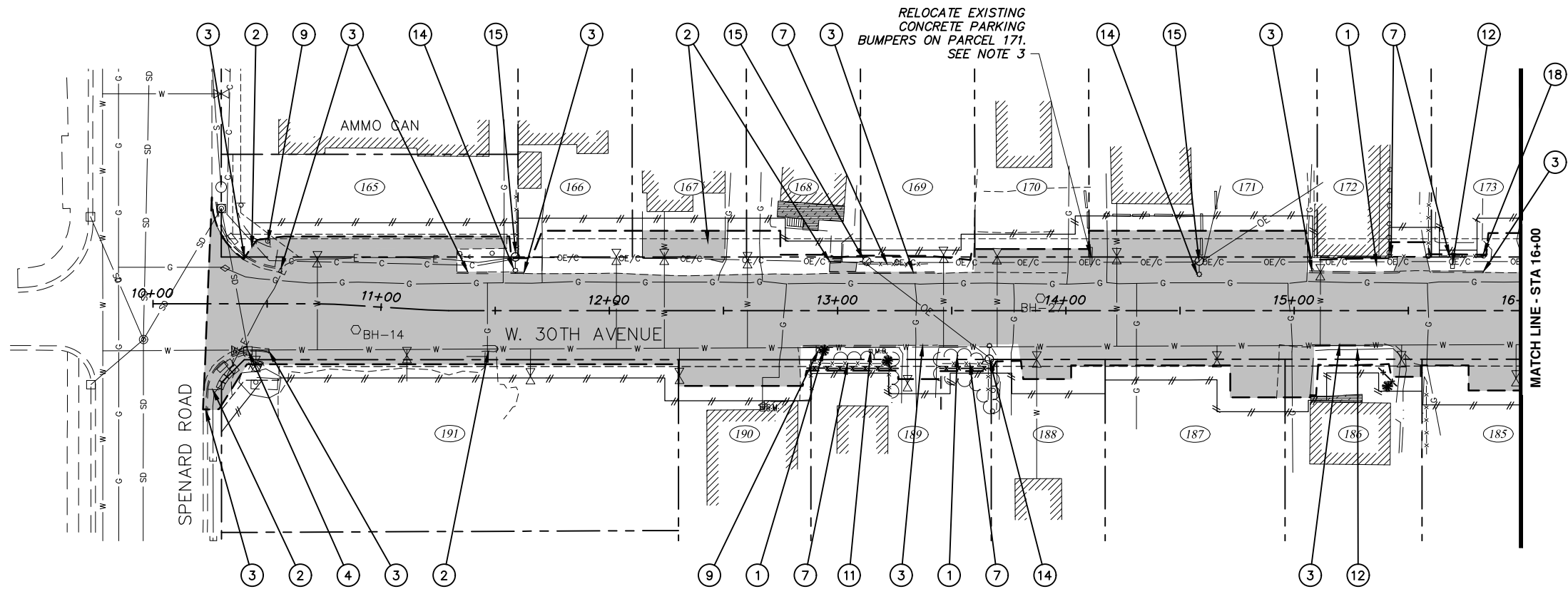
SCALE HOR. 1"=20'
VER. N/A

GRID SW1629, SW1630


DATE NOV 2019

STATUS 65%

SHEET V12 of V13



LEGEND

- ⑫ REMOVAL/DISPOSAL AND/OR SALVAGE OF OBSTRUCTIONS (SECTION 70.22).
 ⑭ REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
 ⑮ PROTECT IN PLACE.
 ⑱ REMOVE AND RELOCATE SIGNS (SECTION 70.10).
 REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
 — — APPROXIMATE LIMITS OF DISTURBANCE
 \ \ \ REMOVE PIPE
 *** TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE SHEET B3 FOR DETAIL.

1. SEE SUMMARY TABLE SHEETS B5 - B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.
3. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

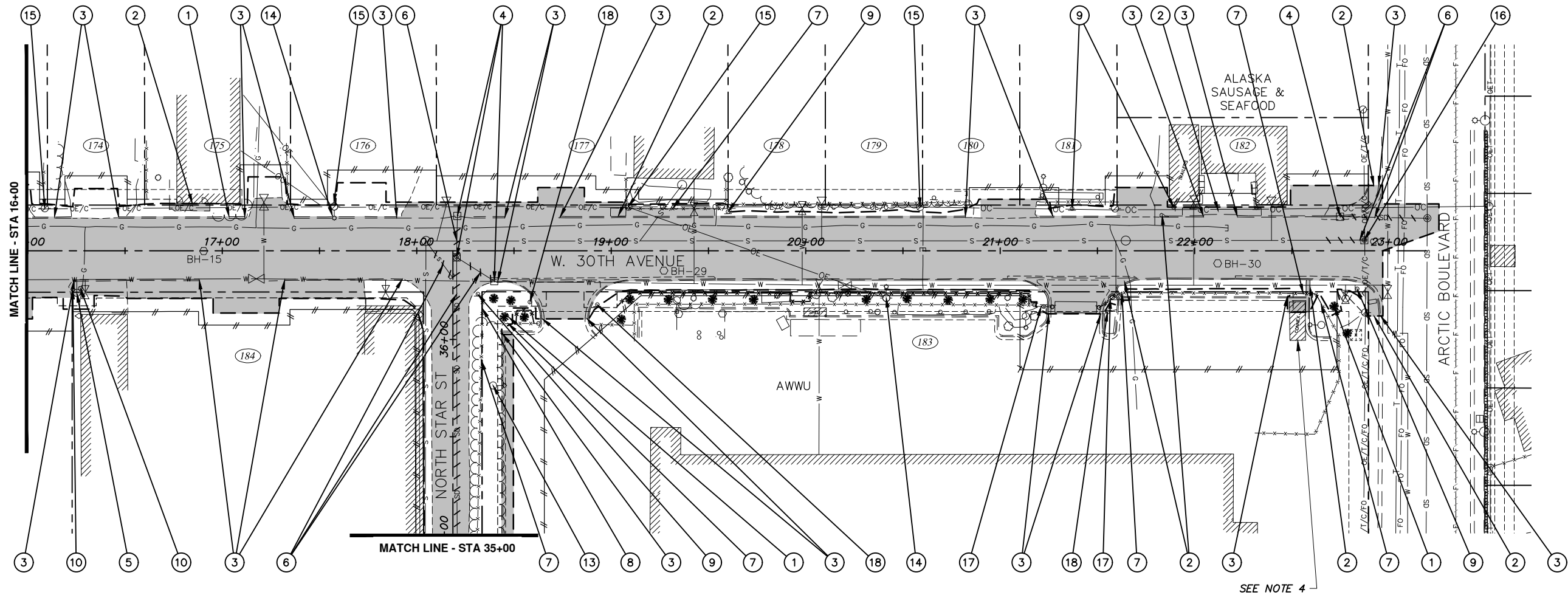
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TOPOGRAPHY	BW	BW
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DESIGN	RB	JK
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PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

CRW
ENGINEERING GROUP, LLC
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLEB82-AK

CONSULTANT



File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Demolition Plan.dwg



LEGEND

- 1 CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 2 REMOVE SIDEWALK OR CONCRETE APRON (SECTION 20.07).
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 5 DECOMMISSION FIRE HYDRANT ASSEMBLY (SINGLE PUMPER) (SECTION 60.08).
- 6 REMOVE PIPE (SECTION 70.07).
- 7 REMOVE AND RESET FENCE (SECTION 70.08).
- 8 REMOVE FENCE (SECTION 70.08).
- 9 REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 70.11).
- 10 REMOVE BOLLARD (SECTION 70.13).

- 13 REMOVE LUMINAIRE POLE (SECTION 80.28).
- 14 REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
- 15 PROTECT IN PLACE.
- 16 REMOVE EXISTING SANITARY SEWER CLEANOUT (SECTION 50.06).
- 17 REMOVE AND RESET GATE (SECTION 70.08).
- 18 REMOVE AND RELOCATE SIGNS (SECTION 70.10).
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- APPROXIMATE LIMITS OF DISTURBANCE
- \\ REMOVE PIPE
- *** TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE SHEET B3 FOR DETAIL.

NOTES:

1. SEE SUMMARY TABLE SHEETS B5 - B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.
3. CONTRACTOR SHALL REMOVE THE EXISTING CONNEX TO A LOCATION ON PARCEL 183 PROPERTY ACCEPTABLE TO PROPERTY OWNER AS REQUIRED TO CONSTRUCT PROPOSED IMPROVEMENTS ON PARCEL 183. ONCE PARCEL 183 PROPOSED IMPROVEMENTS ARE COMPLETE AND HAVE BEEN ACCEPTED BY ENGINEER, CONTRACTOR SHALL RE-INSTALL CONNEX TO EXISTING LOCATION AS SHOWN OR A LOCATION ON PARCEL 183 ACCEPTABLE TO PARCEL 183 PROPERTY OWNER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

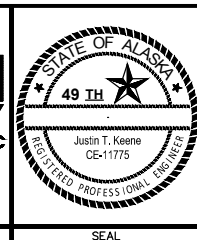
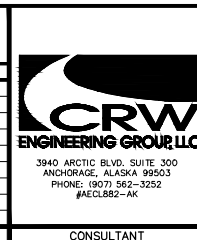
DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

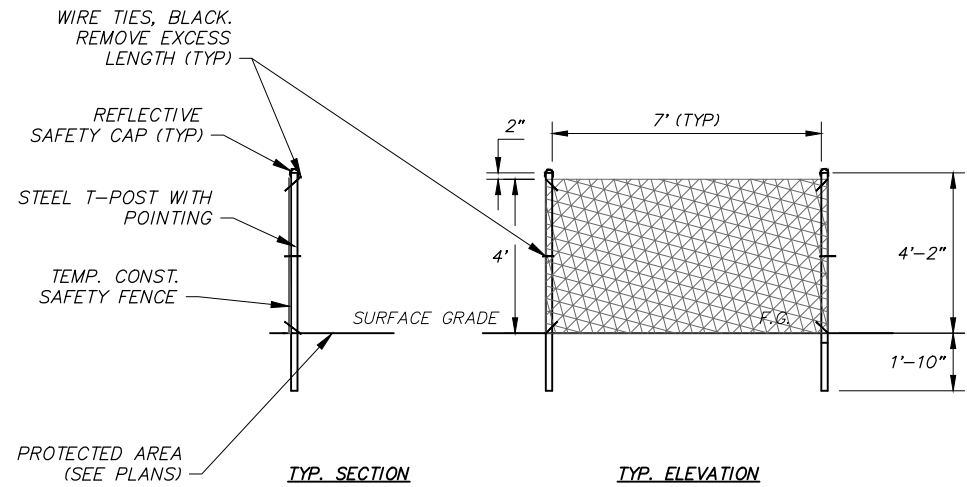
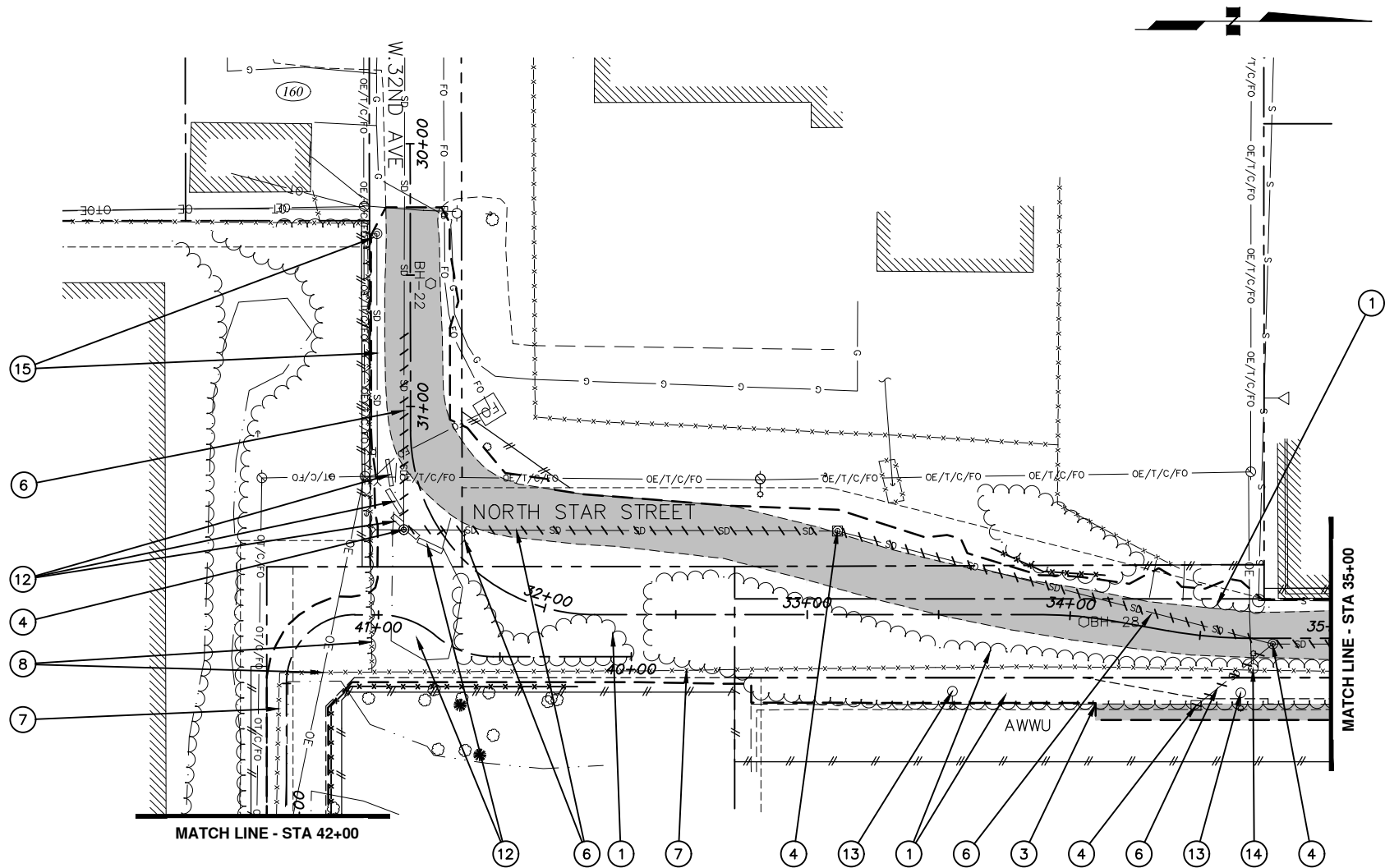
DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC										60	30	0	30	60	SCALE			
FIELD BOOKS		BM NO.		LOCATION		ELEV.		REV	DATE	DESCRIPTION				BY				
DESIGN CRW BOOK No. 149, 169, 195 & 196 STAKING		CB 7D		See MOA Benchmark Book, Page D-56		94.77												
		CB 7C		See MOA Benchmark Book, Page D-17		106.10												
		GAAB 86		See MOA Benchmark Book, Page D-18		104.53												
ASBUILT																		
CONTRACTOR																		
INSPECTOR				BASIS OF THIS DATUM GAAB 1972 ADJUST														
CONSTRUCTION RECORD				VERTICAL DATUM						REVISIONS								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT		
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	ALL
DEMOLITION PLAN		
W. 30TH AVE - STA 16+00 TO ARCTIC BLVD NORTH STAR ST - STA 35+00 TO 36+46		
SCALE HOR. 1"=30' VER. N/A	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65%	SHEET B2 of B7

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Demolition Plan.dwg



1 TEMPORARY TREE PROTECTION FENCE DETAIL

SCALE: NTS

LEGEND

- 1 CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 6 REMOVE PIPE (SECTION 70.07).
- 7 REMOVE AND RESET FENCE (SECTION 70.08).
- 8 REMOVE FENCE (SECTION 70.08).
- 12 REMOVAL/DISPOSAL AND/OR SALVAGE OF OBSTRUCTIONS (SECTION 70.22).
- 13 REMOVE LUMINAIRE POLE (SECTION 80.28).
- 14 REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
- 15 PROTECT IN PLACE.
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- APPROXIMATE LIMITS OF DISTURBANCE
- \\ \\ REMOVE PIPE
- *** TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE THIS SHEET FOR DETAIL.

NOTES:

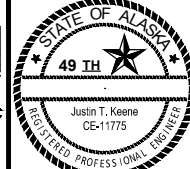
1. SEE SUMMARY TABLE SHEETS B5 - B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	
BY: _____	TITLE: _____
DATE: _____	
2. DATA TRANSFERRED BY: _____	
COMPANY: _____	DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____	TITLE: _____
COMPANY: _____	DATE: _____
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

60 30 0 30 60
GRAPHIC SCALE



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT		
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	ALL
DEMOLITION PLAN		
NORTH STAR ST - STA 30+24 TO STA 35+00 PATHWAY - STA 40+00 TO 42+00		
SCALE HOR. 1"=30' VER. N/A	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65%	SHEET B3 of B7

20.07

REMOVE SIDEWALK OR CONCRETE APRON	②
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SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	AREA (SY)	REMARKS
B1	10+28.9	38.0 RT	10+36.0	29.1 RT	9	W. 30TH AVE AND SPENARD ROAD
B1	10+45.4	27.3 LT	10+54.7	24.0 LT	11	W. 30TH AVE AND SPENARD ROAD
B1	11+43.3	17.6 RT	11+50.8	17.6 RT	2	W. 30TH AVE, PARCEL 191
B1	12+14.5	34.8 LT	12+50.8	34.8 LT	49	PARCEL 167 DWY
B1	12+96.7	21.4 LT	13+08.4	20.8 LT	6	W. 30TH AVE, PARCEL 168
B2	16+72.9	24.3 LT	16+93.5	24.2 LT	8	PARCEL 175
B2	18+99.7	22.0 LT	19+07.6	21.7 LT	20	W. 30TH AVE, PARCEL 177
B2	21+56.2	20.5 RT	21+67.7	15.1 RT	4	W. 30TH AVE, PARCEL 183
B2	21+82.1	18.8 LT	21+86.4	17.9 LT	1	W. 30TH AVE, PARCEL 183
B2	21+93.6	22.0 LT	22+34.2	21.9 LT	18	W. 30TH AVE, PARCEL 182
B2	22+57.3	21.7 RT	22+59.2	21.7 RT	1	PARCEL 183 SPILLWAY
B2	22+69.3	23.0 LT	22+89.6	33.5 LT	18	W. 30TH AVE AND ARCTIC BLVD
B2	22+86.1	21.8 RT	22+89.6	33.5 RT	8	W. 30TH AVE AND ARCTIC BLVD
B4	46+02.3	7.4 LT	46+06.4	42.1 RT	29	ARCTIC BLVD
B4	46+62.9	42.2 RT	46+63.4	26.2 RT	9	ARCTIC BLVD

20.08

REMOVE CURB AND GUTTER ③

SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	LENGTH (FT)	REMARKS
B1	10+24.1	37.5 RT	10+50.7	21.6 RT	35	W. 30TH AVE
B1	10+40.7	21.0 LT	10+56.5	15.6 LT	17	W. 30TH AVE
B1	11+33.5	16.8 LT	11+46.5	16.8 LT	13	W. 30TH AVE
B1	11+56.7	16.8 LT	11+62.5	16.8 LT	6	W. 30TH AVE
B1	12+84.9	16.0 RT	13+25.4	16.0 RT	41	W. 30TH AVE
B1	12+96.6	16.8 LT	13+42.4	17.0 LT	46	W. 30TH AVE
B1	13+44.3	16.1 RT	13+52.8	16.1 RT	8	W. 30TH AVE
B1	15+06.7	17.3 LT	15+54.7	17.2 LT	39	W. 30TH AVE
B1	15+07.8	15.5 RT	15+38.7	15.5 RT	31	W. 30TH AVE
B1	15+60.2	17.4 LT	15+82.7	17.2 LT	23	W. 30TH AVE
B2	16+02.4	17.3 LT	16+22.2	17.4 LT	20	W. 30TH AVE
B2	16+23.1	15.7 RT	16+88.2	15.4 RT	65	W. 30TH AVE
B2	16+46.7	17.4 LT	17+11.6	17.5 LT	65	W. 30TH AVE
B2	17+23.9	15.5 RT	17+92.8	15.0 RT	69	W. 30TH AVE
B2	17+36.9	17.4 LT	17+89.7	17.4 LT	53	W. 30TH AVE
B2	18+29.3	17.5 LT	18+45.2	17.6 LT	16	W. 30TH AVE
B2	18+38.7	16.5 RT	21+24.0	32.5 RT	376	W. 30TH AVE
B2	18+73.7	17.7 LT	20+82.1	17.9 LT	208	W. 30TH AVE
B2	21+20.3	17.8 LT	21+64.7	17.9 LT	41	W. 30TH AVE
B2	21+53.1	32.5 RT	21+60.1	28.0 RT	18	W. 30TH AVE
B2	21+66.4	23.4 RT	22+94.8	33.5 RT	156	W. 30TH AVE
B2	21+86.6	17.9 LT	22+04.5	18.0 LT	18	W. 30TH AVE
B2	22+21.7	18.2 LT	22+94.5	33.5 LT	81	W. 30TH AVE
B2	22+47.6	23.7 RT	22+59.0	31.6 RT	20	PARCEL 183
B2	35+00.0	26.9 RT	3+62.6	26.6 RT	103	PARCEL 183
B3	34+15.8	34.5 RT	35+00.0	26.9 RT	90	PARCEL 183
B4	46+06.4	12.7 LT	46+08.9	42.1 RT	55	ARCTIC BLVD
B4	46+57.9	42.2 RT	46+58.6	25.2 RT	17	ARCTIC BLVD

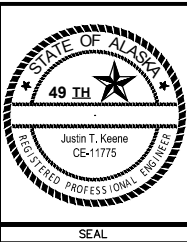
RECORD DRAWING	
1. DATA PROVIDED BY: _____	TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	
BY: _____	TITLE: _____
DATE: _____	
2. DATA TRANSFERRED BY: _____	TITLE: _____
COMPANY: _____	
DATE: _____	
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____	TITLE: _____
COMPANY: _____	
DATE: _____	
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST						
INSPECTOR							
CONSTRUCTION RECORD	VERTICAL DATUM			REVISIONS			

CRW
ENGINEERING GROUP LLC
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

CONSULTANT



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	ALL
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DEMOLITION SUMMARY TABLES

SCALE HOR. N/A
 VER. N/A

GRID SW1629, SW1630

DATE NOV 2019

STATUS 65%

SHEET B5 of B7

B5 of B7

20.09

[illegible]

SHEET	STATION TO STATION	OFFSET	AREA (SY)	REMARKS
B1	BOP TO 16+00	LT & RT	3,058	W. 30TH AVE, DRIVEWAYS
B2	16+00 TO EOP	LT & RT	3,428	W. 30TH AVE, NORTH STAR ST, ARCTIC BLVD, DWYS
B3	30+24 TO 35+00	LT & RT	1,099	NORTH STAR ST, PARKING AREA
B4	42+00 TO EOP	LT & RT	211	ARCTIC BLVD

NOTES: 1. SEE ROADWAY IMPROVEMENT (R) SHEETS FOR ROADWAY PAVEMENT REMOVAL LIMITS.
2. SEE DRIVEWAY RECONSTRUCTION TABLE ON (T) SHEETS FOR DRIVEWAY REMOVAL LIMITS.

50.06

REMOVE EXISTING SANITARY SEWER CLEANOUT (16)

SHEET	STATION	OFFSET (FT)	REMARKS
B2	22+87.2	5.3 LT	

55.11

REMOVE MANHOLE OR CATCH BASIN ④

SHEET	APPX STATION	APPX OFFSET (FT)	CATCH BASIN	MANHOLE	REMARKS
B1	10+41.6	20.6 RT	X		
B2	18+20.5	3.4 RT		X	
B2	18+20.9	18.0 LT	X		
B2	18+39.9	15.6 RT	X		
B2	22+74.7	17.6 LT	X		
B3	31+42.7	11.8 RT		X	
B3	33+11.5	31.8 LT		X	
B3	34+51.3	26.7 RT	X		
B3	34+51.3	26.7 RT	X		
B3	34+77.7	2.2 RT		X	
B4	45+77.83	0.9 LT	X		

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60.08			
DECOMMISSION FIRE HYDRANT ASSEMBLY			
SHEET	STATION	OFFSET (FT)	REMARKS
B1	16+25.2	21.7 RT	HYDRANT VALVE & LEG SHALL REMAIN IN PLACE

70.07							
REMOVE PIPE							
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	SIZE (INCH)	LENGTH (FT)	REMARKS
B2	18+04.7	5.0 LT	18+18.3	13.7 RT	8	23.2	SEWER
B2	18+20.5	3.4 RT	18+20.9	18.0 LT	8	21.4	STORM DRAIN
B2	18+20.5	3.4 RT	18+39.9	15.6 RT	10	22.9	STORM DRAIN
B2	22+68.5	5.4 LT	22+87.2	5.4 LT	8	18.7	SEWER
B2	22+74.7	17.6 LT	23+19.6	16.6 LT	12	44.8	STORM DRAIN
B2	35+00.0	2.1 RT	36+42.2	1.9 RT	12	142.2	STORM DRAIN
B3	30+69.3	2.4 RT	31+42.7	11.8 RT	15	77.6	STORM DRAIN
B3	31+42.7	11.8 RT	33+11.5	31.8 LT	15	165.0	STORM DRAIN
B3	31+54.8	7.8 LT	31+58.3	5.2 LT	12	4.0	STORM DRAIN
B3	33+11.5	31.8 LT	34+77.7	2.2 RT	12	171.0	STORM DRAIN
B3	34+51.3	26.7 RT	34+77.7	2.2 RT	12	37.3	STORM DRAIN
B3	34+77.7	2.2 RT	35+00.0	1.9 RT	12	22.3	STORM DRAIN
B4	45+77.8	0.9 LT	46+09.9	21.9 RT	12	39.4	STORM DRAIN




70.08						
REMOVE FENCE						
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	REMARKS
B3	41+40.6	14.3 LT	41+46.3	2.9 RT	17.7	CHAIN W/ BARBED WIRE
B3	41+46.3	2.9 RT	41+58.6	3.9 RT	12.7	CHAIN W/ BARBED WIRE

70.08										
REMOVE AND RESET FENCE										
SHEET	EXISTING LOCATION				PROPOSED LOCATION				LENGTH (FT)	REMARKS
	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)		
B1	12+88.4	23.0 RT	13+22.5	23.3 RT	12+88.3	24.5 RT	13+22.5	24.5 RT	34.2	WOOD
B1	13+09.2	20.4 LT	13+39.5	20.4 LT	13+10.1	23.5 LT	13+39.5	23.5 LT	29.4	CHAIN
B1	13+45.9	23.3 RT	13+66.0	23.6 RT	13+45.9	24.5 RT	13+66.0	24.5 RT	20.1	CHAIN
B1	15+37.3	23.7 LT	15+52.9	23.6 LT	15+37.4	23.9 LT	15+53.0	23.8 LT	15.6	CHAIN
B1	15+64.0	23.8 LT	15+82.7	23.7 LT	15+58.8	24.0 LT	15+82.8	24.0 LT	24.1	CHAIN
B2	19+07.6	23.5 LT	19+63.8	21.6 LT	19+07.7	23.5 LT	19+60.1	23.5 LT	52.4	CHAIN
B2	18+41.8	37.7 RT	18+49.2	37.6 RT	18+41.8	37.5 RT	18+49.2	37.5 RT	7.4	CHAIN
B2	21+61.8	21.8 RT	21+62.0	21.6 RT	21+62.0	21.6 RT	21+68.0	21.8 RT	6.0	CHAIN
B2	22+47.6	22.0 RT	22+62.1	22.3 RT	22+47.6	21.9 RT	22+62.2	22.1 RT	14.6	WOOD
B2	22+62.1	22.3 RT	22+64.5	26.6 RT	22+62.2	22.1 RT	22+64.6	26.5 RT	5.0	WOOD
B2	35+00.0	12.5 RT	36+21.9	15.2 RT	35+00.0	20.3 RT	36+14.4	20.3 RT	114.5	CHAIN W/BARBED WIRE
B3	31+64.4	59.6 RT	33+85.5	19.8 RT	31+65.2	61.1 RT	33+85.5	24.0 RT	221.1	CHAIN W/BARBED WIRE
B3	33+85.5	19.8 RT	33+89.0	19.8 RT	33+85.5	24.0 RT	33+89.0	20.5 RT	4.9	CHAIN W/BARBED WIRE
B3	33+89.0	19.8 RT	35+00.0	12.5 RT	33+89.0	20.5 RT	35+00.0	20.3 RT	113.9	CHAIN W/BARBED WIRE
B3	41+58.6	2.9 RT	42+00.0	3.9 RT	41+58.6	15.0 LT	42+00.0	15.0 LT	41.4	CHAIN W/BARBED WIRE
B3	41+10.2	21.8 LT	41+40.6	14.3 LT	41+11.2	24.0 LT	41+58.6	15.0 LT	15.3	CHAIN W/BARBED WIRE
B4	42+00.0	3.9 RT	44+15.4	9.3 RT	42+00.0	15.0 LT	44+16.0	15.0 LT	216.0	CHAIN W/BARBED WIRE
B4	44+15.4	9.3 RT	44+20.6	9.4 RT	44+16.0	15.0 LT	44+21.0	10.0 LT	7.1	CHAIN W/BARBED WIRE
B4	44+20.6	9.4 RT	45+35.3	11.9 RT	44+21.0	10.0 LT	+45.5	10.0 LT	133.3	CHAIN W/BARBED WIRE

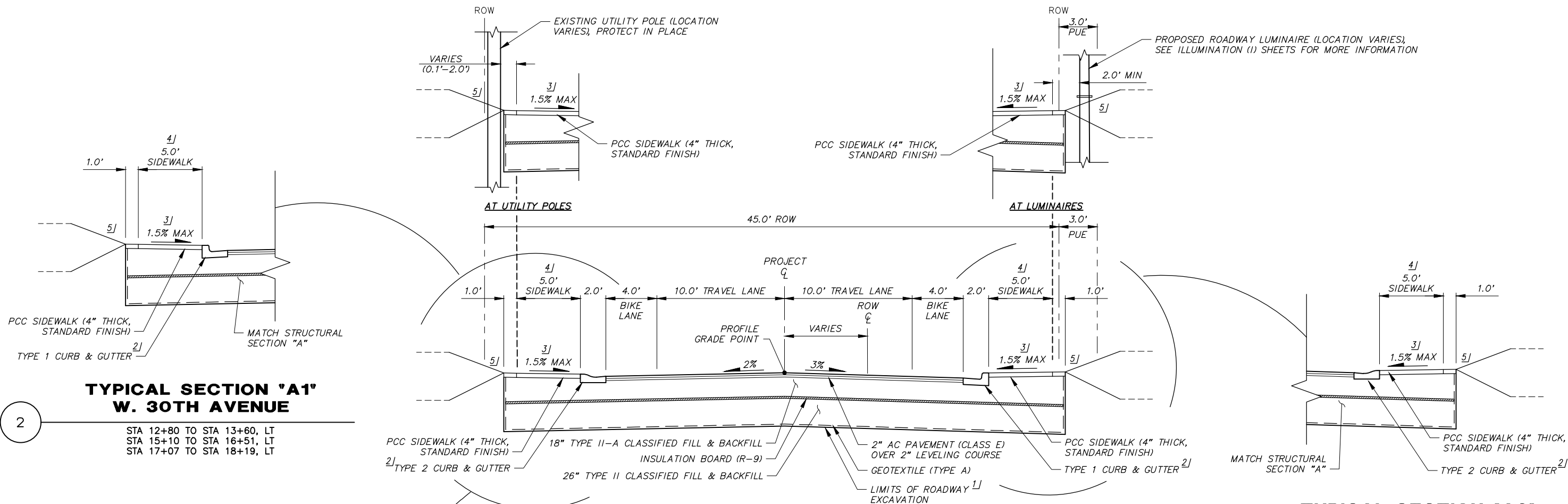
- NOTES:
- PROVIDE TEMPORARY FENCING PER SECTION 70.24 FOR ALL FENCES REMOVED OR AS DIRECTED BY THE ENGINEER.
 - STAKE RESET FENCE LAYOUT IN THE FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.08 PAY ITEM.

70.08										
REMOVE AND RESET GATE										
SHEET	EXISTING LOCATION				PROPOSED LOCATION				LENGTH (FT)	REMARKS
	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)		
B2	21+18.1	28.7 RT	21+26.6	29.1 RT	21+18.2	28.5 RT	21+26.7	28.9 RT	8.5	CHAIN, LT GATE SECTION
	21+53.1	27.6 RT	21+61.8	21.8 RT	21+50.8	29.2 RT	21+62.0	21.6 RT	13.5	CHAIN, RT GATE SECTION

- NOTE:
- STAKE RESET GATE LAYOUT IN THE FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.08 PAY ITEM.

<div>RECORD DRAWING</div> <div>1. DATA PROVIDED BY: _____ TITLE: _____</div> <div>THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.</div> <div>CONTRACTOR: _____</div> <div>BY: _____ TITLE: _____ DATE: _____</div> <div>2. DATA TRANSFERRED BY: _____ TITLE: _____</div> <div>COMPANY: _____ DATE: _____</div> <div>3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR--PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.</div> <div>DATA TRANSFER CHECKED BY: _____ TITLE: _____</div> <div>COMPANY: _____ DATE: _____</div> <div>BY: _____</div>	<table><tr><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>BW</td><td>BW</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>RK</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>RK</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>	DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	BW	BW	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	RK	JK	TELEPHONE	RK	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table><tr><td>FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td></tr><tr><td>DESIGN CRW BOOK No. 149, 169, 195 & 196</td><td>CB 7D</td><td>See MOA Benchmark Book, Page D--56</td><td>94.77</td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td>CB 7C</td><td>See MOA Benchmark Book, Page D--17</td><td>106.10</td><td></td><td></td><td></td><td></td></tr><tr><td></td><td>GAAB 86</td><td>See MOA Benchmark Book, Page D--18</td><td>104.53</td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR</td><td></td><td>BASIS OF THIS DATUM GAAB 1972 ADJUST</td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td></td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr></table>	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D--56	94.77					STAKING	CB 7C	See MOA Benchmark Book, Page D--17	106.10						GAAB 86	See MOA Benchmark Book, Page D--18	104.53																					ASBUILT								CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST						INSPECTOR																								<div><div>3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC1882-AK</div></div> <div></div> <div></div>	<div>PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</div> <div>16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 ALL</div> <div>DEMOLITION SUMMARY TABLES</div> <div>SCALE HOR. N/A VER. N/A</div> <div>GRID SW1629, SW1630 DATE NOV 2019</div> <div>STATUS 65%</div> <div>SHEET B6 of B7</div>
DATA	DRAWN BY	CHECKED BY																																																																																																																														
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File: E:\JobsData\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Civil\02 Design Phase 1\10138.00 Typical Sections - Phase 1.dwg



TYPICAL SECTION 'A1' W. 30TH AVENUE

STA 12+80 TO STA 13+60, LT
STA 15+10 TO STA 16+51, LT
STA 17+07 TO STA 18+19, LT

TYPICAL SECTION 'A' - W. 30TH AVENUE

BOP TO STA 18+19

TYPICAL SECTION 'A2' W. 30TH AVENUE

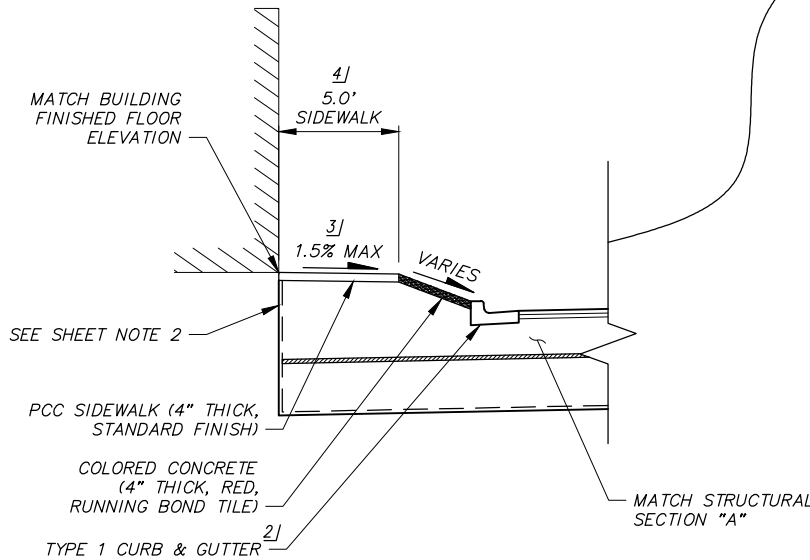
STA 12+30 TO 12+84, RT

SHEET NOTES:

- THE STATION RANGES IDENTIFIED FOR EACH TYPICAL SECTION ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.
- CAUTION!!! BUILDING FOUNDATION IS AT UNKNOWN LOCATION AND DEPTH. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION AND DEPTH OF EXISTING BUILDING FOUNDATION PRIOR TO CONSTRUCTION. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE. CONTRACTOR SHALL REPAIR BUILDING FOUNDATION THAT IS DAMAGED BY CONTRACTOR'S OPERATIONS AT NO COST TO THE OWNER.

#/ FOOT NOTES:

- PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- TOP AC PAVEMENT SHALL BE $\frac{1}{8}$ " - $\frac{1}{4}$ " ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 3, SHEET C5.
- THE MAXIMUM SIDEWALK CROSS SLOPE GRADE IS 2% AT DRIVEWAYS.
- INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
- USE APPROPRIATE MATCH EXISTING EDGE TREATMENT AS SHOWN ON DETAIL 2, SHEET C2.



TYPICAL SECTION 'A3' W. 30TH AVENUE

STA 16+51 TO 17+07, LT

RECORD DRAWING	
1. DATA PROVIDED BY: _____	TITLE: _____
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CONTRACTOR: _____	DATE: _____
BY: _____	TITLE: _____
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COMPANY: _____	DATE: _____
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DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

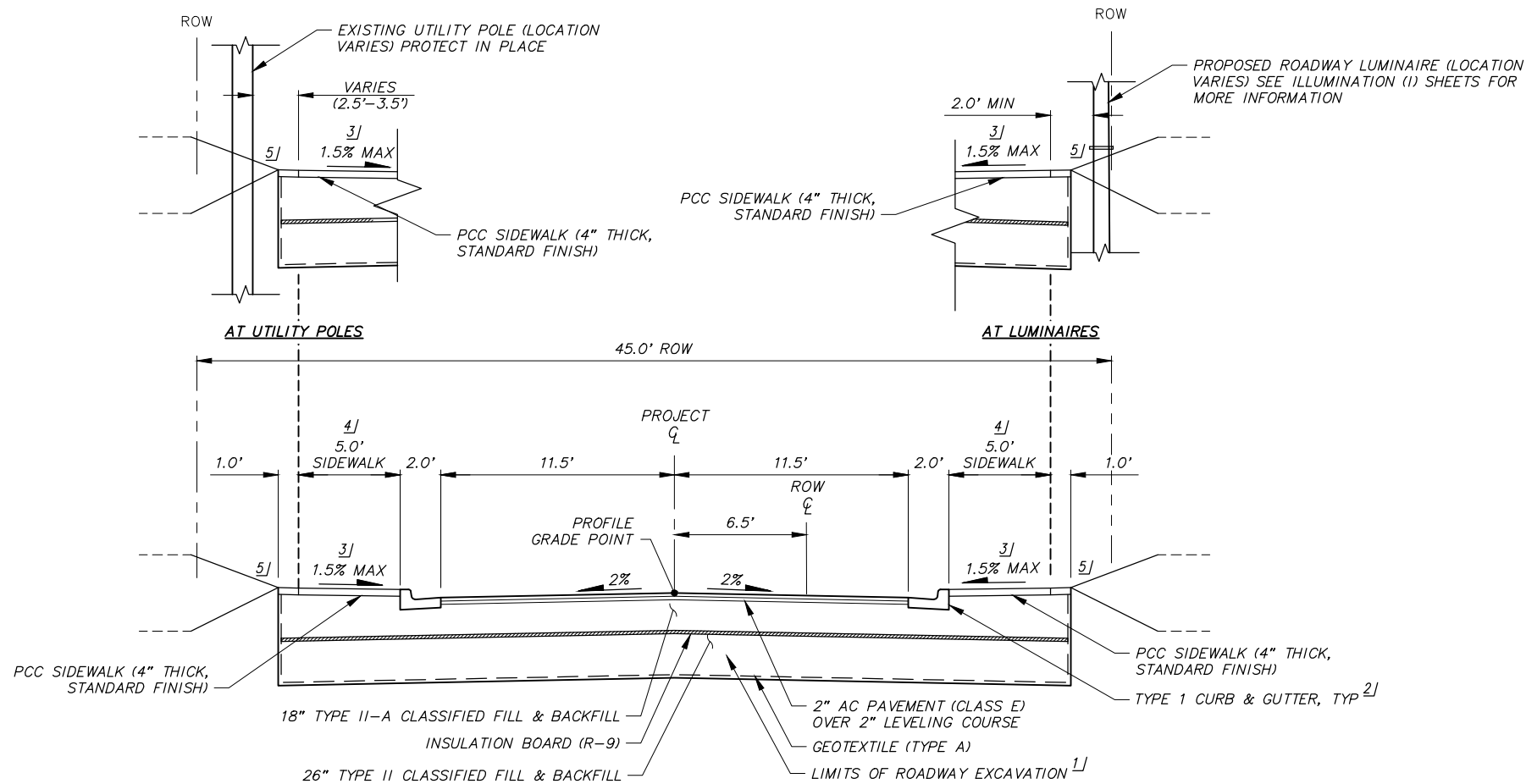
CRW ENGINEERING GROUP LLC
3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC1882-AK

STATE OF ALASKA 49 TH Robert W. Burdick CE-123959 REGISTERED PROFESSIONAL ENGINEER
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MUNICIPALITY OF ANCHORAGE

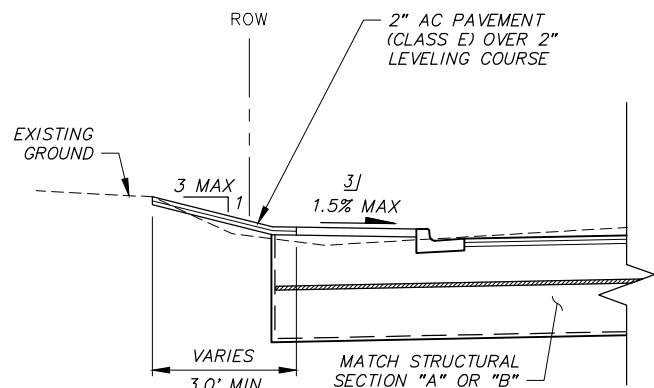
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1
TYPICAL SECTIONS	
W. 30TH AVENUE	
SCALE HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET C1 of C5

File: s:\data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Civil\02 Design Phase 1\10138.00 Typical Sections - Phase 1.dwg

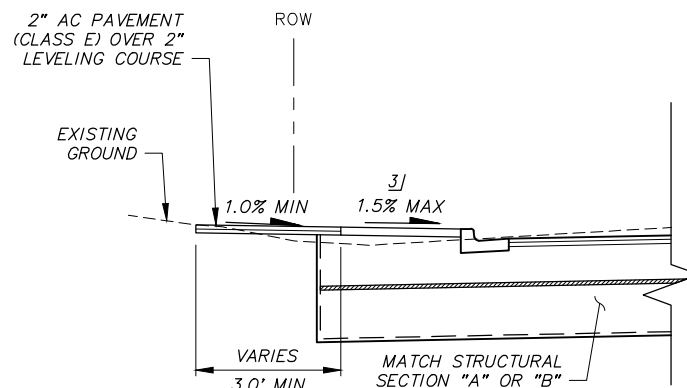


TYPICAL SECTION 'B' - W. 30TH AVENUE

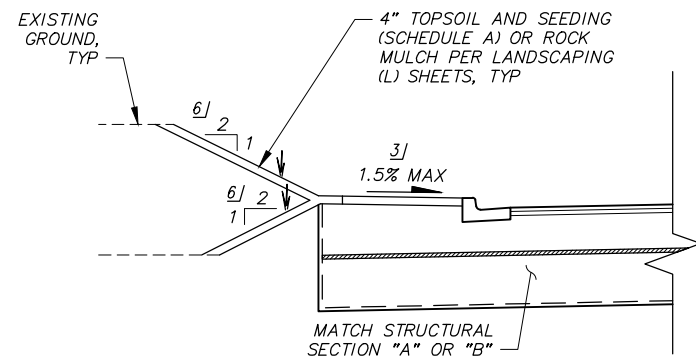
STA 18+19 TO EOP



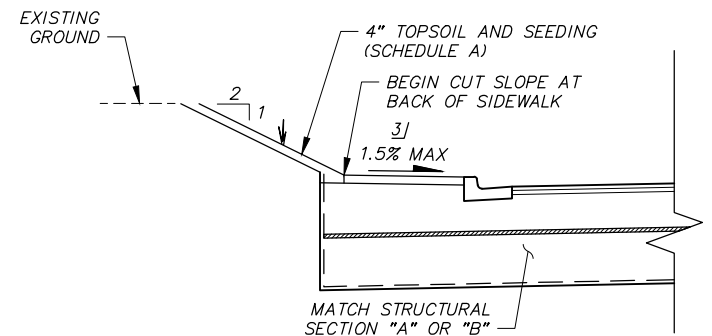
ADJACENT TO PAVED SURFACES - CUT CONDITION



ADJACENT TO PAVED SURFACES - FILL CONDITION



ADJACENT TO GRASSED/ROCK MULCH AREAS - CUT/FILL CONDITION



ADJACENT TO GRASSED AREAS WITH NO SHOULDER - CUT CONDITION
STA 15+05 TO STA 15+43, LT

W. 30TH AVENUE - TYPICAL MATCH EXISTING EDGE TREATMENT

2

SHEET NOTES:

1. THE STATION RANGES IDENTIFIED FOR EACH TYPICAL SECTION ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.

#1 FOOT NOTES:

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2. TOP AC PAVEMENT SHALL BE $\frac{1}{8}$ " - $\frac{1}{4}$ " ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 3, SHEET C5.
3. THE MAXIMUM SIDEWALK CROSS SLOPE GRADE IS 2% AT DRIVEWAYS.
4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
5. USE APPROPRIATE MATCH EXISTING EDGE TREATMENT AS SHOWN ON DETAIL 2, THIS SHEET.
6. THE TYPICAL AND MAXIMUM CUT/FILL SLOPES ARE 2 (HORIZONTAL): 1 (VERTICAL). FILL SLOPES MAY VARY ALONG ROADWAY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. SEE DETAIL 2, SHEET C5. SEE ROADWAY SHEETS FOR LOCATIONS. THE ENGINEER MAY ADJUST THE TYPICAL SLOPES IN THE FIELD.

MATCH EXISTING EDGE TREATMENT NOTE:

1. MIRROR FOR RIGHT SIDE.

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FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
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	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

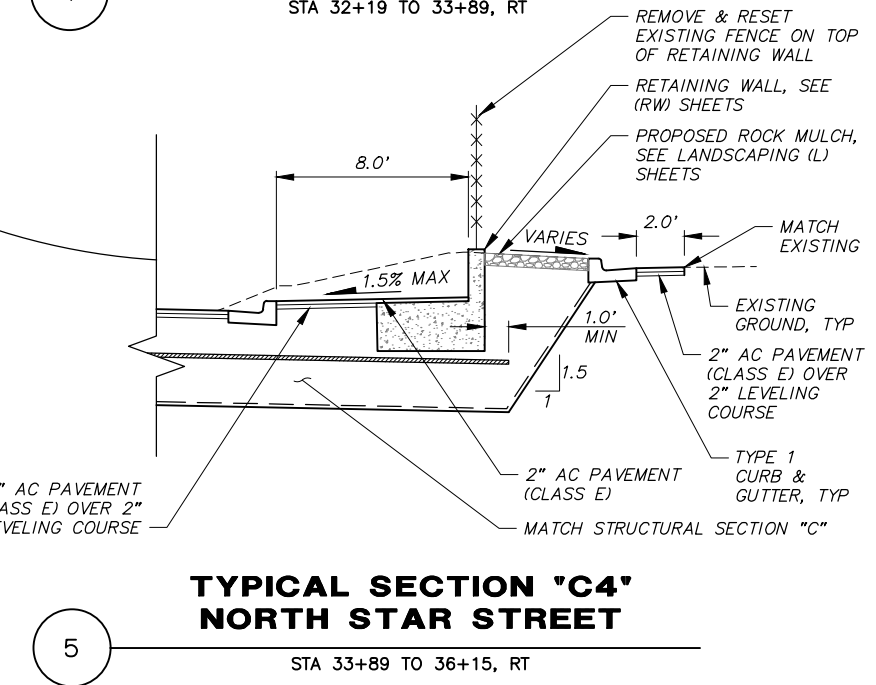
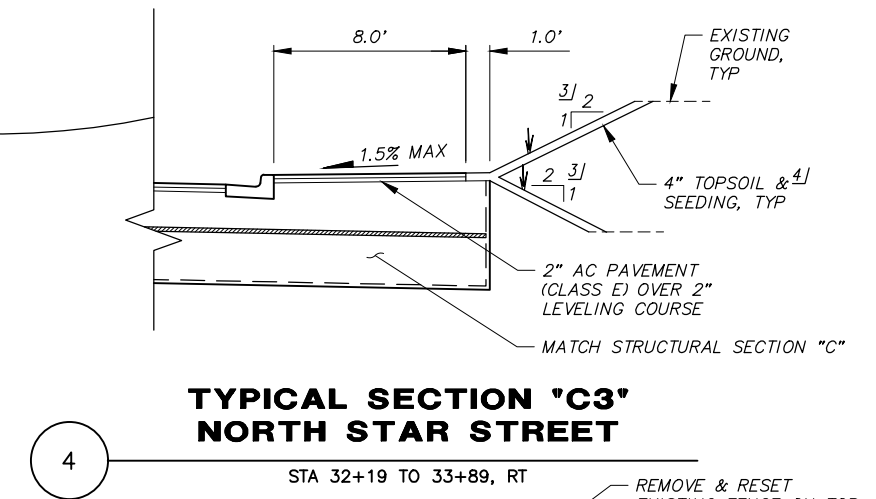
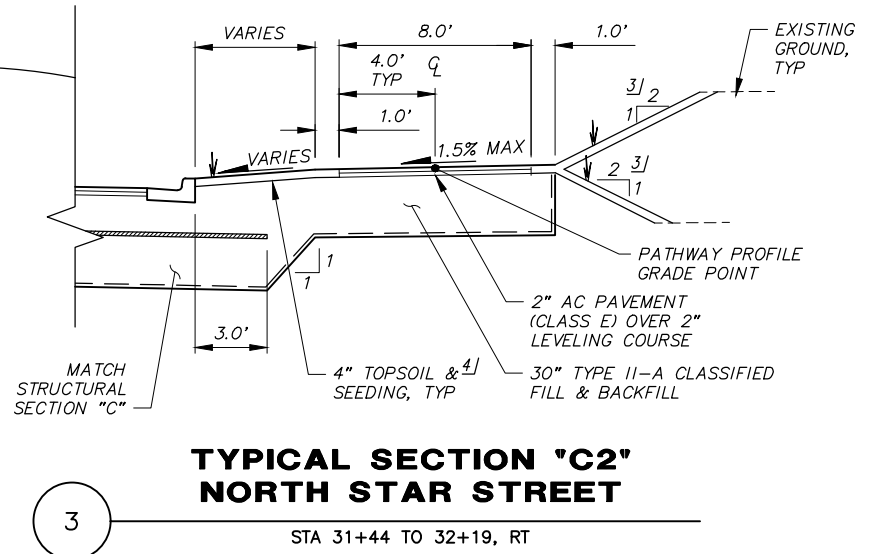
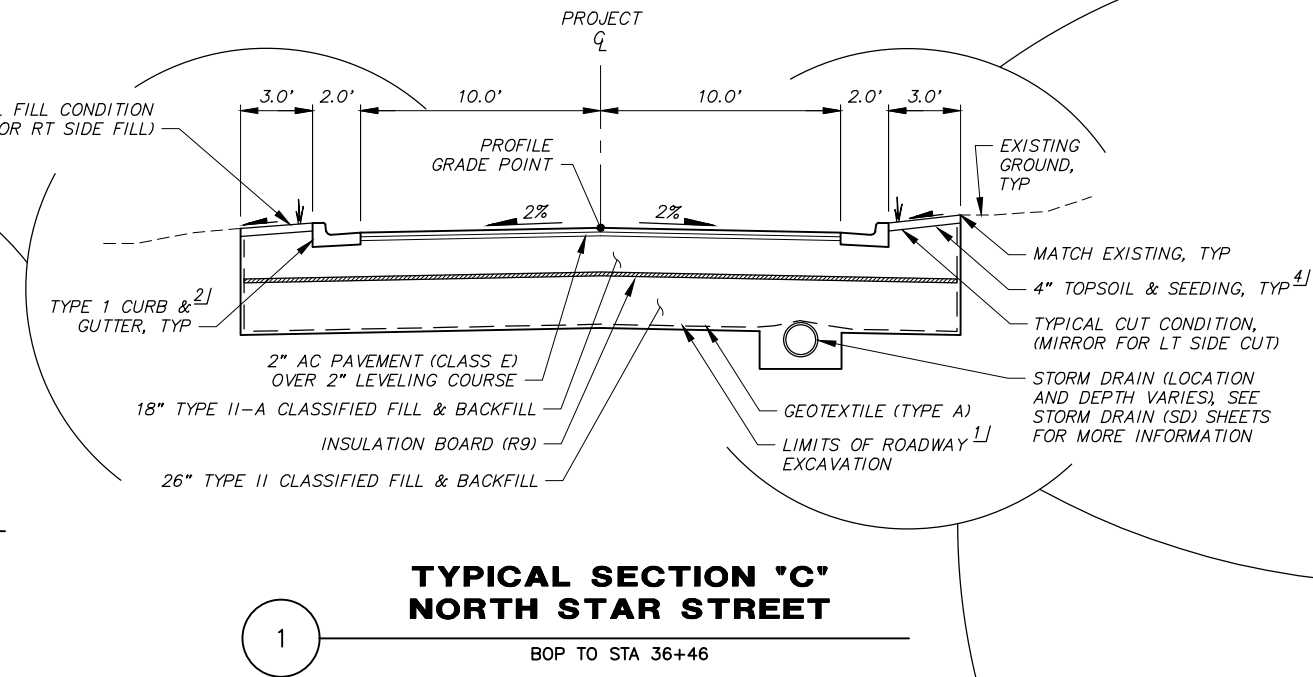
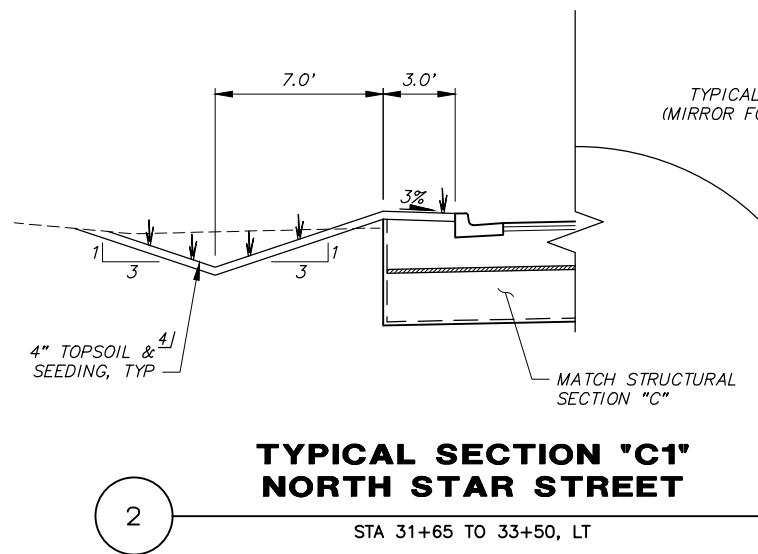
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STATE OF ALASKA 49 TH Robert W. Burdick CE-123959 REGISTERED PROFESSIONAL ENGINEER
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MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1
TYPICAL SECTIONS	
W. 30TH AVENUE	
SCALE HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET C2 of C5

File: I:\JobData\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Typical Sections - Phase 1.dwg



SHEET NOTES:

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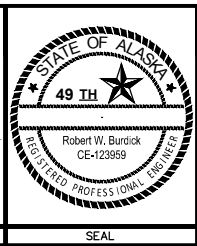
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4. PLACE 4" OF TOPSOIL AND SEEDING PER LANDSCAPING (L) SHEETS ON ALL DISTURBED AREAS.

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ASBUILT							
CONTRACTOR							
INSPECTOR							
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
TYPICAL SECTIONS			
NORTH STAR STREET			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET C3 of C5



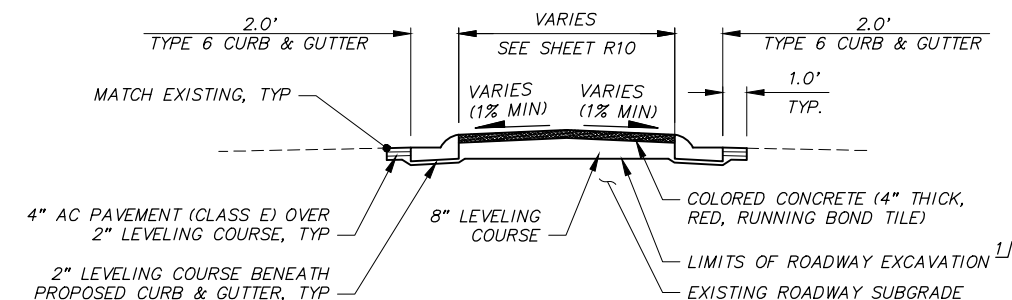
STA 44+11 TO 45+60. LT



BOP TO EOP

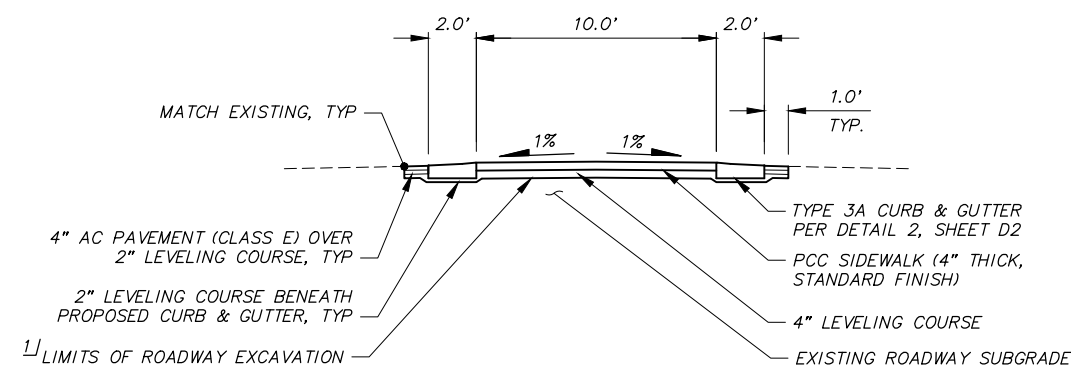


STA 45+60 TO EOP, LT



TYPICAL RAISED MEDIAN SECTION

4



TYPICAL PEDESTRIAN REFUGE ISLAND SECTION




5

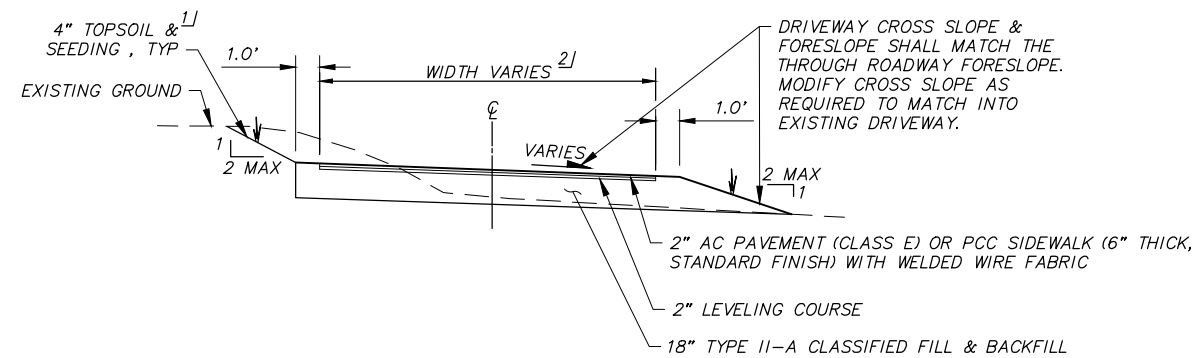
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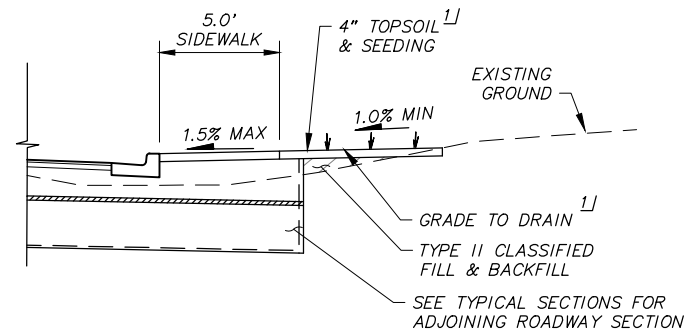
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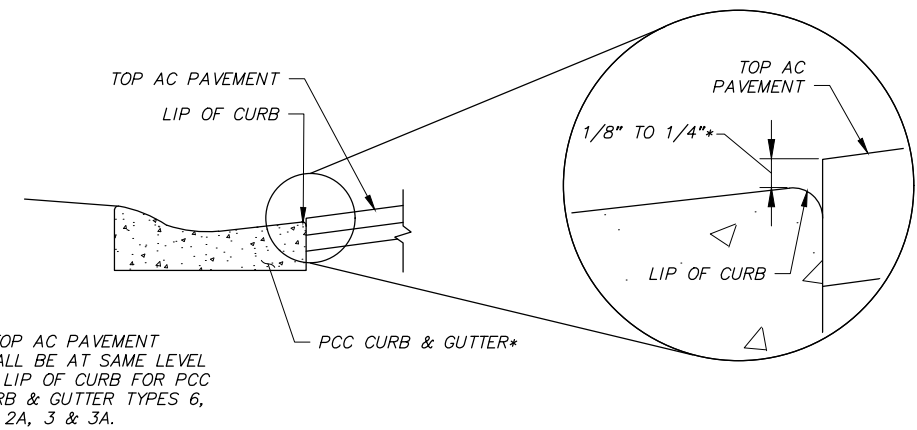
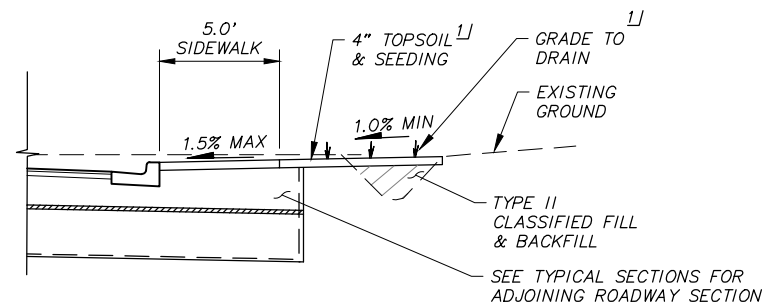
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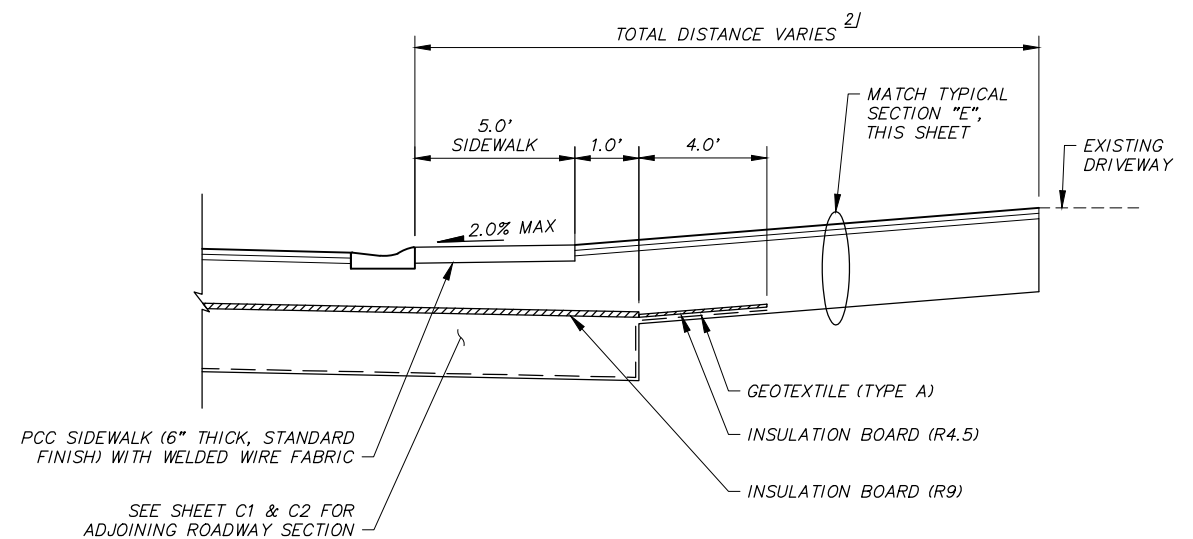
**TYPICAL SECTION "E" DRIVEWAY ^{3/}
PAVED OR CONCRETE**



SPECIAL FILL GRADING DETAILS



CURB AND GUTTER & AC PAVEMENT EDGE DETAIL






TYPICAL DRIVEWAY CONNECTION SECTION

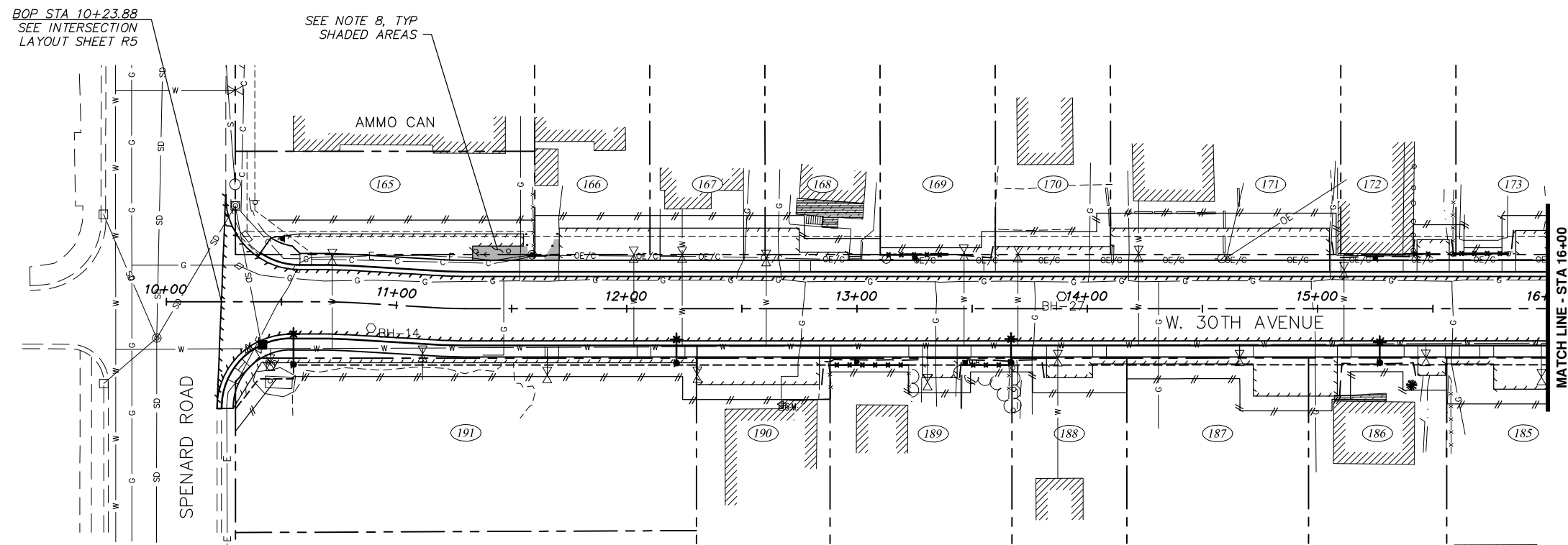
SHEET NOTES:

1. SEE SHEETS C1-C2 FOR ADJOINING ROADWAY SECTION.

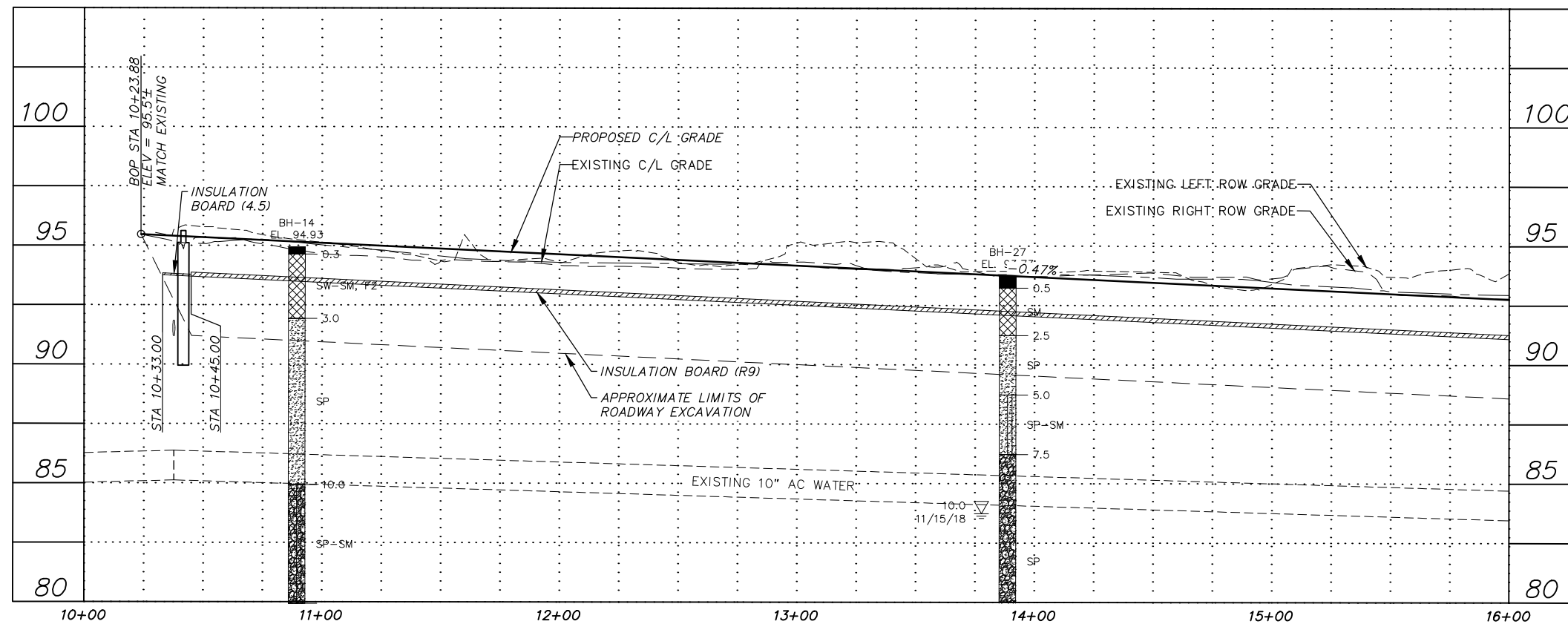
#/ FOOT NOTES:

1. PLACE 4" OF TOPSOIL AND SEEDING PER LANDSCAPING (L) SHEETS ON ALL DISTURBED AREAS.
2. SEE RECONSTRUCT DRIVEWAY SUMMARY TABLE ON THE ROADWAY SUMMARY TABLE (T) SHEETS, DRIVEWAY RECONSTRUCTION PLANS & DRIVEWAY DETAILS FOR DRIVEWAY RECONSTRUCTION INFORMATION.
3. INSTALL INSULATION ADJACENT TO DRIVEWAY AND TRANSITION TO DRIVEWAY SECTION PER DETAIL 4, THIS SHEET

RECORD DRAWING		<table><tr><td>DATA</td><td>DRAWN BY</td><td>CHECKED BY</td></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>BW</td><td>BW</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>RK</td><td>JK</td></tr><tr><td>GAS</td><td>RK</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>RK</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>		DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	BW	BW	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	RK	JK	GAS	RK	JK	TELEPHONE	RK	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table><tr><td colspan="2">FIELD BOOKS</td><td>BM NO.</td><td>LOCATION</td><td>ELEV.</td><td>REV</td><td>DATE</td><td>DESCRIPTION</td><td>BY</td></tr><tr><td colspan="2">DESIGN CRW BOOK No. 149, 169,</td><td>CB 7D</td><td>See MOA Benchmark Book, Page D-56</td><td>94.77</td><td></td><td></td><td></td><td></td></tr><tr><td colspan="2">195 & 196</td><td>CB 7C</td><td>See MOA Benchmark Book, Page D-17</td><td>106.10</td><td></td><td></td><td></td><td></td></tr><tr><td colspan="2"></td><td>GAAB 86</td><td>See MOA Benchmark Book, Page D-18</td><td>104.53</td><td></td><td></td><td></td><td></td></tr><tr><td colspan="2">ASBUILT</td><td colspan="7">BASIS OF THIS DATUM GAAB 1972 ADJUST</td></tr><tr><td colspan="2">CONTRACTOR</td><td colspan="7"></td></tr><tr><td colspan="2">INSPECTOR</td><td colspan="7"></td></tr></table>										FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77					195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10							GAAB 86	See MOA Benchmark Book, Page D-18	104.53					ASBUILT		BASIS OF THIS DATUM GAAB 1972 ADJUST							CONTRACTOR									INSPECTOR									 <p>3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC0882-AK</p>						<table><tr><td colspan="10">PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</td></tr><tr><td colspan="10">16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1</td></tr><tr><td colspan="10">TYPICAL SECTIONS</td></tr><tr><td colspan="10">DRIVEWAYS & MISC DETAILS</td></tr></table>						PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT										16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1										TYPICAL SECTIONS										DRIVEWAYS & MISC DETAILS									
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 8. GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 2, SHEET C5. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.0% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.



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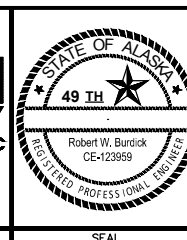
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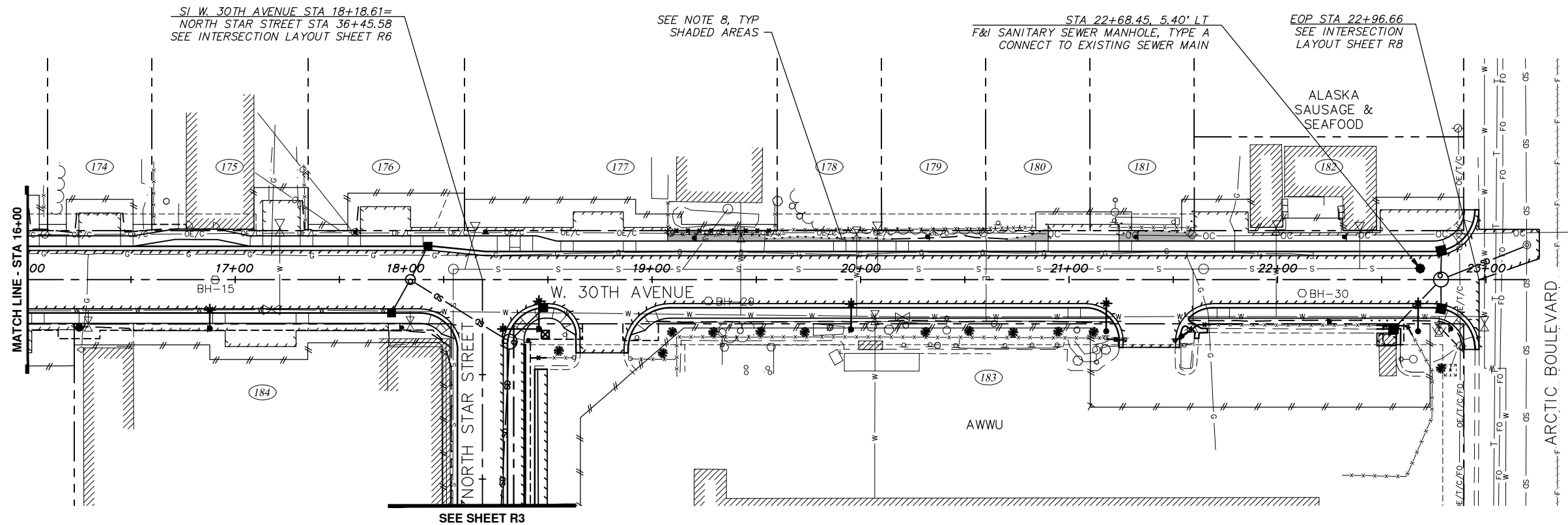
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PROFILE	RB	JH								
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TELEPHONE	RK	JK	STAKING	GAAB B6	See MOA Benchmark Book, Page D-18	104.53				
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
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BY AM. CHECK			CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS			



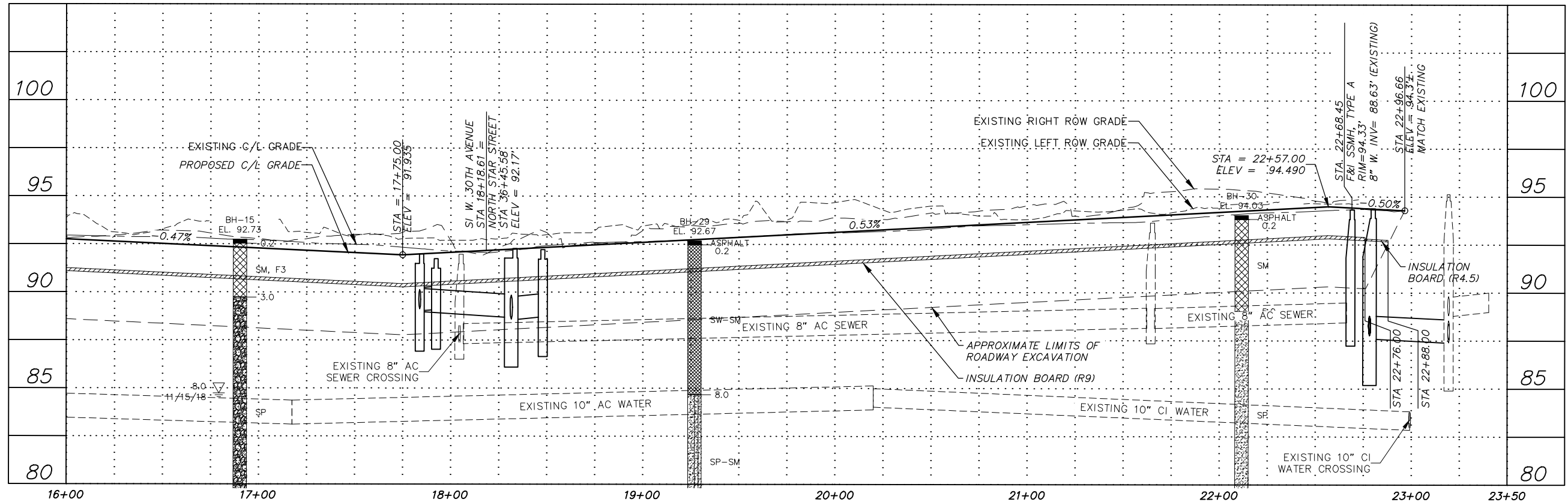
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16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	SCHED A	
ROADWAY PLAN & PROFILE			
W. 30TH AVENUE BOP TO STA 16+00			
SCALE	HOR. 1"=30' VER. 1"=3'	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65%	SHEET R1 of R11

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan And Profile - Phase 1.dwg



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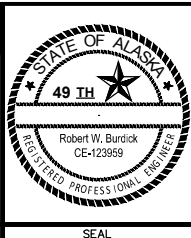
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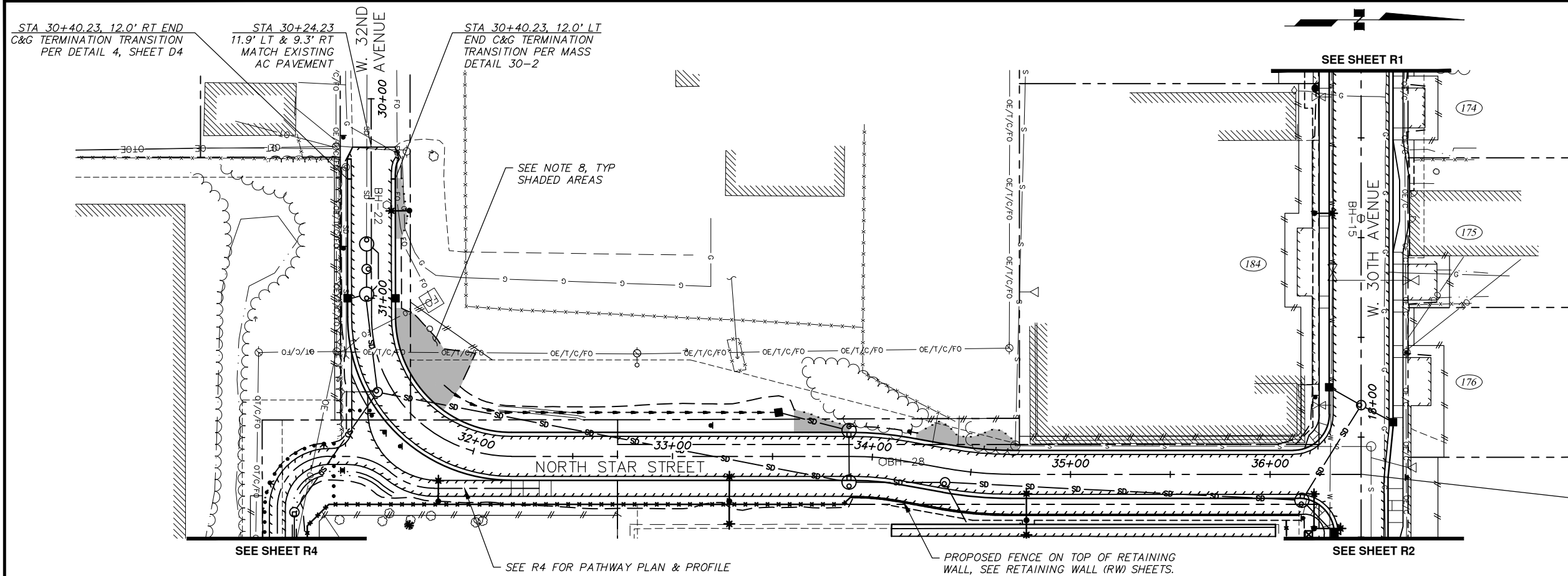
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CONSULTANT							
SEAL							



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W. 30TH AVENUE STA 16+00 TO ARCTIC BOULEVARD			
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SHEET R2 of R11			

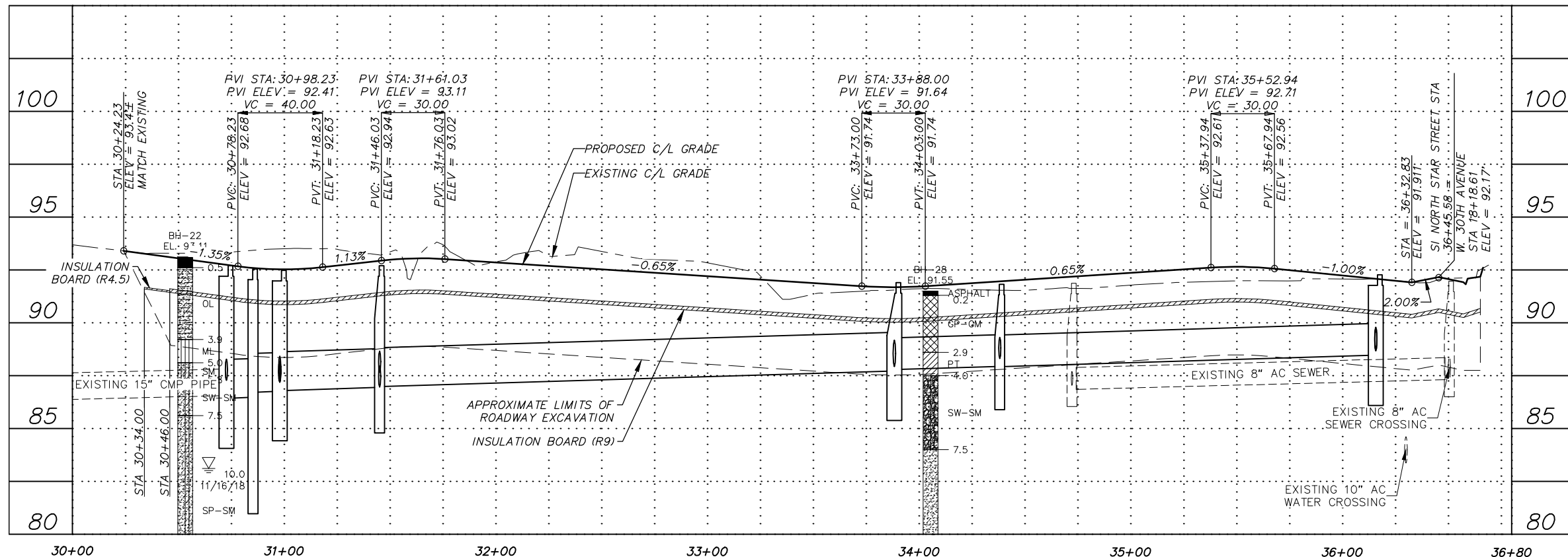
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6. SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
7. THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
8. GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 2, SHEET C5. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.0% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

SI NORTH STAR STREET STA 36+45.58=
W. 30TH AVENUE STA 18+18.61
SEE INTERSECTION LAYOUT SHEET R6



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
STAKING	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC0882-AK

STATE OF ALASKA

49 TH

Robert W. Burdick
CE-123959

REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

ROADWAY PLAN & PROFILE

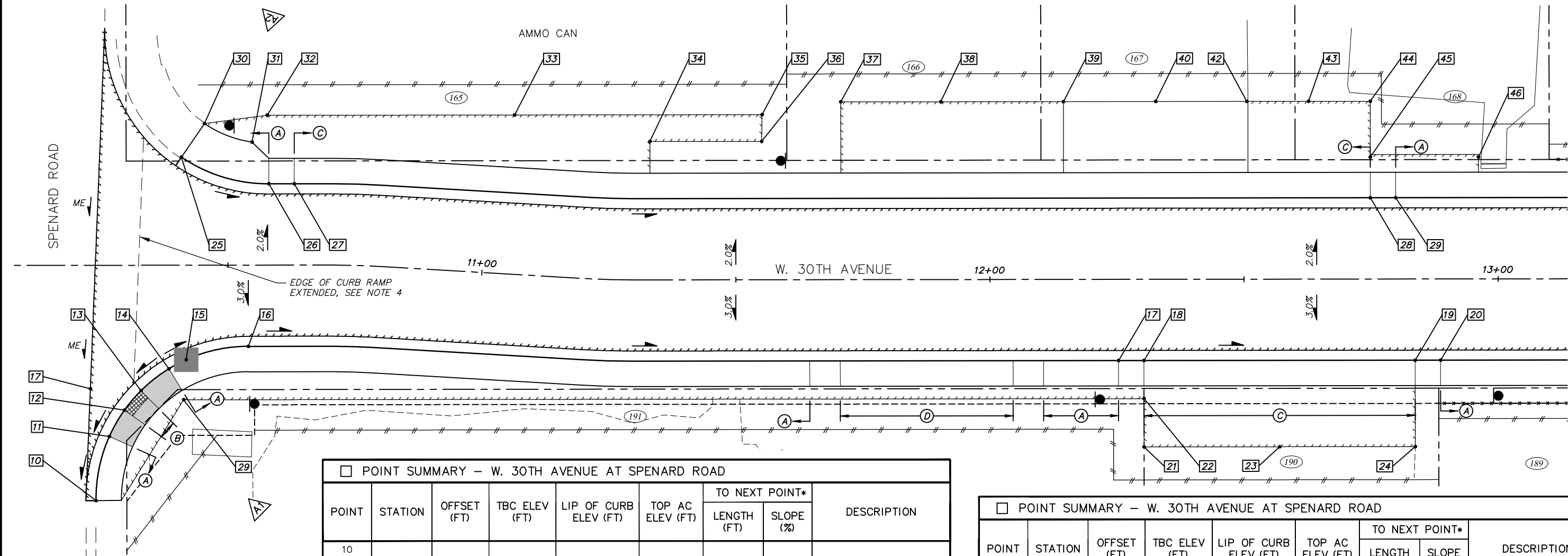
NORTH STAR STREET
W. 32ND AVENUE TO W. 30TH AVENUE

SCALE HOR. 1"=30'
VER. 1"=3'

GRID SW1629, SW1630

DATE NOV 2019 STATUS 65% SHEET R3 of R11

File: s:\data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Intersection Layout - Phase 1.dwg



DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(B)	TYPE 1A CURB
(C)	TYPE 2 CURB
(D)	TYPE 4 CURB

NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
- SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
- THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
- LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- ➔ APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▨ DETECTABLE WARNING PANEL
- PCC CURB RAMP

POINT SUMMARY – W. 30TH AVENUE AT SPENARD ROAD

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
10								
11								
12								
13								
14								
15								
16								
17								
18								
19								
20								
21								
22								
23								
24								
25								
26								
27								
28								
29								
30								
31								
32								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

POINT SUMMARY – W. 30TH AVENUE AT SPENARD ROAD

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
33								
34								
35								
36								
37								
38								
39								
40								
41								
42								
43								
44								
45								
46								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

CURB RADIUS TABLE

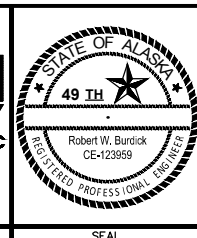
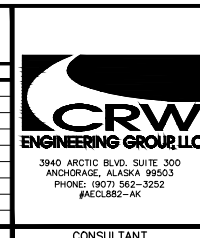
POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1				SPENARD ROAD
A2				SPENARD ROAD

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
- THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
- CONTRACTOR: _____ DATE: _____
- BY: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____
- COMPANY: _____ DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
- DATA TRANSFER CHECKED BY: _____ TITLE: _____
- COMPANY: _____ DATE: _____
- BY: _____

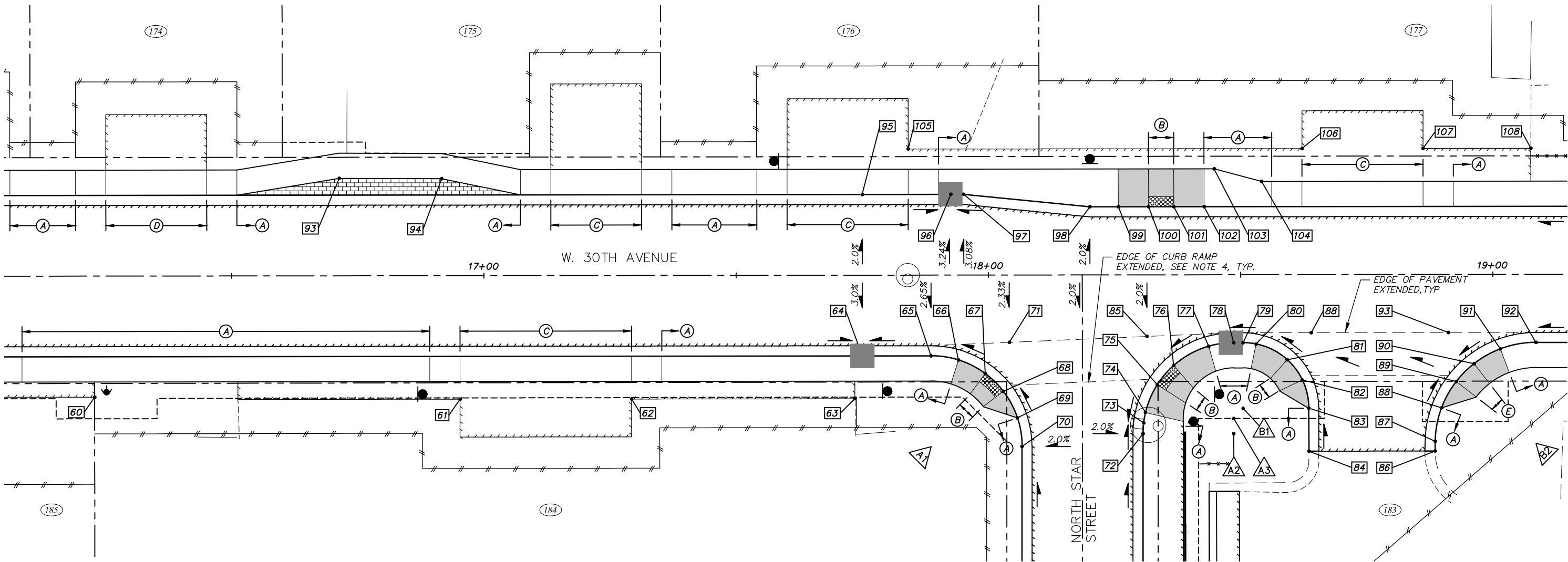
DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM		GAAB 1972 ADJUST						
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS		



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	SCHED A	
INTERSECTION LAYOUT PLAN			
SPENARD ROAD			
SCALE	HOR. 1"=10'	GRID SW1629, SW1630	
	VER. N/A	DATE NOV 2019	STATUS 65%
SHEET		R5 of R11	

File: s:\data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Intersection Layout - Phase 1.dwg



DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(B)	TYPE 1A CURB
(C)	TYPE 2 CURB
(D)	TYPE 4 CURB
(E)	TYPE 3A CURB

NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
- SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
- THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
- LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

□ POINT SUMMARY – W. 30TH AVENUE AT NORTH STAR STREET

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
60								
61								
62								
63								
64								
65								
66								
67								
68								
69								
70								
71								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

△ CURB RADIUS TABLE

POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1				NORTH STAR STREET
A2				NORTH STAR STREET
A3				NORTH STAR STREET BOS
B1				PARCEL 183
B2				PARCEL 183

LEGEND

- APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- DETECTABLE WARNING PANEL
- PCC CURB RAMP
- COLORED CONCRETE (4" THICK, RED, BRICK PATTERN)

RECORD DRAWING

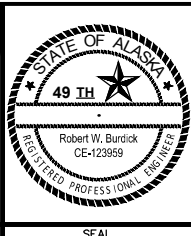
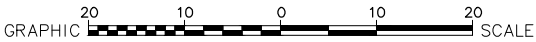
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM		GAAB 1972 ADJUST						



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

INTERSECTION LAYOUT PLAN

NORTH STAR STREET

SCALE HOR. 1"=10'	GRID SW1629, SW1630	DATE NOV 2019	STATUS 65%	SHEET R6 of R11
-------------------	---------------------	---------------	------------	-----------------

File: k:\jobData\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Intersection Layout - Phase 1.dwg

☐ POINT SUMMARY – W. 30TH AVENUE AT NORTH STAR STREET								
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
74								
75								
76								
77								
78								
79								
80								
81								
82								
83								
84								
85								
86								
87								
88								
89								
90								
91								
92								
93								
94								
95								
96								
97								
98								
99								
100								
101								
102								
103								
104								
105								
106								
107								
108								

TO BE COMPLETED FOR 95%

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

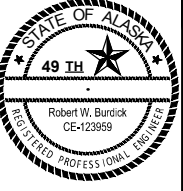
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D--56	94.77				
195 & 196		CB 7C	See MOA Benchmark Book, Page D--17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D--18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								
VERTICAL DATUM								
REVISIONS								



3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLE882-AK





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SCHED A

INTERSECTION LAYOUT TABLE

SCALE

HOR. N/A
VER. N/A

GRID SW1629, SW1630

DATE NOV 2019

STATUS 65%

SHEET

R7 of R11

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Intersection Layout - Phase 1.dwg

△ CURB RADIUS TABLE				
POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1				PATHWAY
A2				PATHWAY

□ POINT SUMMARY – PATHWAY AT NORTH STAR STREET								
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
200								
201								
202								
203								
204								
205								
206								
207								
208								
209								
210								
211								
212								
213								
214								
215								
216								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

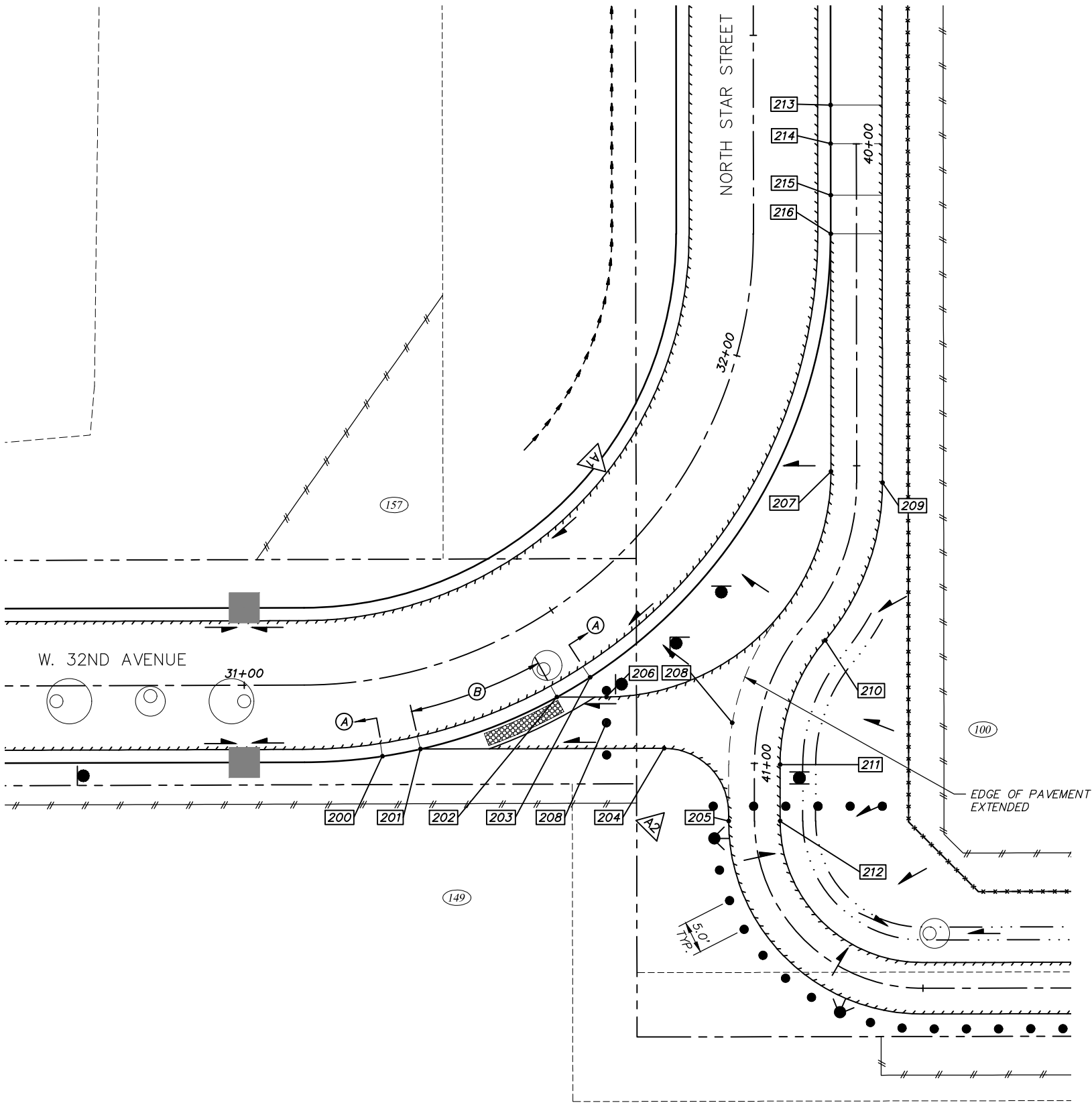
NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
- SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
- LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▨ DETECTABLE WARNING PANEL
- PCC CURB RAMP

DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(B)	TYPE 1A CURB



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ DATE: _____

BY: _____ TITLE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

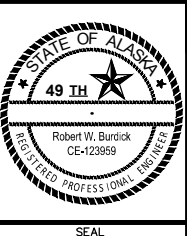
DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
PLAN CHECK								
CONSTRUCTION RECORD								
VERTICAL DATUM								
REVISIONS								
CONSULTANT								
SEAL								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

INTERSECTION LAYOUT PLAN

PATHWAY AT NORTH STAR STREET

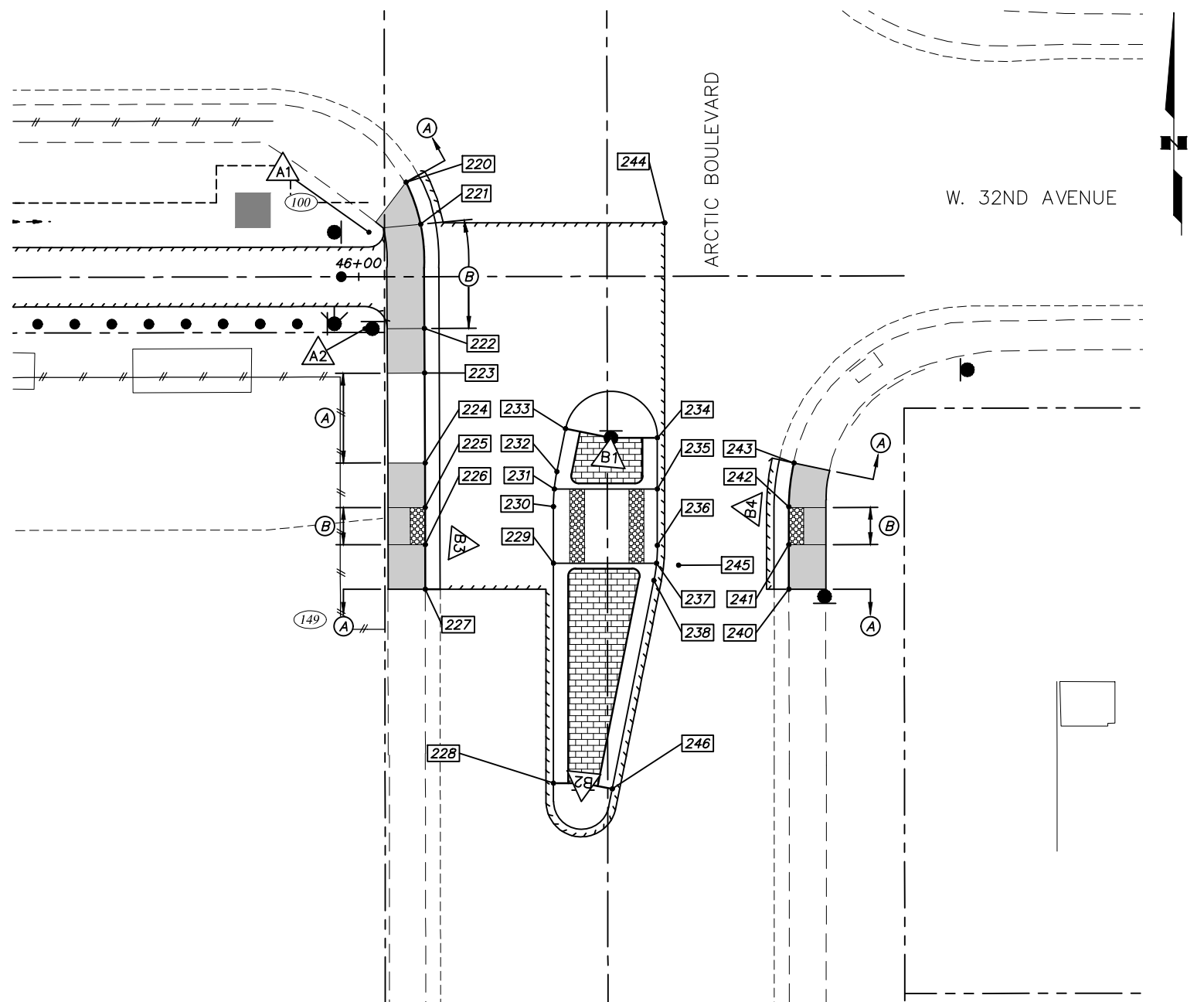
SCALE HOR. 1"=10'
VER. N/A

GRID SW629, SW630
DATE NOV 2019 STATUS 65% SHEET R9 of R11

<input type="checkbox"/> POINT SUMMARY – PATHWAY AT ARCTIC BOULEVARD								
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
220								
221								
222								
223								
224								
225								
226								
227								
228								
229								
230								
231								
232								
233								
234								
235								
236								
237								
238								
239								
240								
241								
242								
243								
244								
245								
246								
247								
248								
249								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

△ CURB RADIUS TABLE			
POINT	TBC RADIUS POINT		RADIUS (FT)
	STATION	OFFSET (FT)	
A1			95% PATHWAY
A2			PATHWAY
B1			REFUGE ISLAND NOSE
B2			REFUGE ISLAND NOSE
B3			REFUGE ISLAND
B4			REFUGE ISLAND



LEGEND

- | | |
|---|---|
|  | DETECTABLE WARNING PANEL |
|  | PCC CURB RAMP |
|  | COLORÉD CONCRÉTÉ (4" THICK, RED, RUNNING BOND TILÉ) |

DESIGNATION | CURB TYPE

- | | |
|-----|--------------|
| (A) | TYPE 1 CURB |
| (B) | TYPE 1A CURB |

NOTES

1. SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
2. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
3. SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
4. PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
5. SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
6. LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.


DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

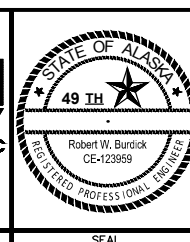
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FIELD BOOKS		BM NO.	LOCATION		ELEV.	REV	DATE	DESCRIPTION		BY			
DESIGN CRW BOOK No. 149, 169,		CB 7D	See MOA Benchmark Book, Page D-56		94.77								
195 & 196		CB 7C	See MOA Benchmark Book, Page D-17		106.10								
STAKING		GAAB 86	See MOA Benchmark Book, Page D-18		104.53								
ASBUILT													
CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST											
INSPECTOR													
CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS									



CRW
ENGINEERING GROUP, LLC

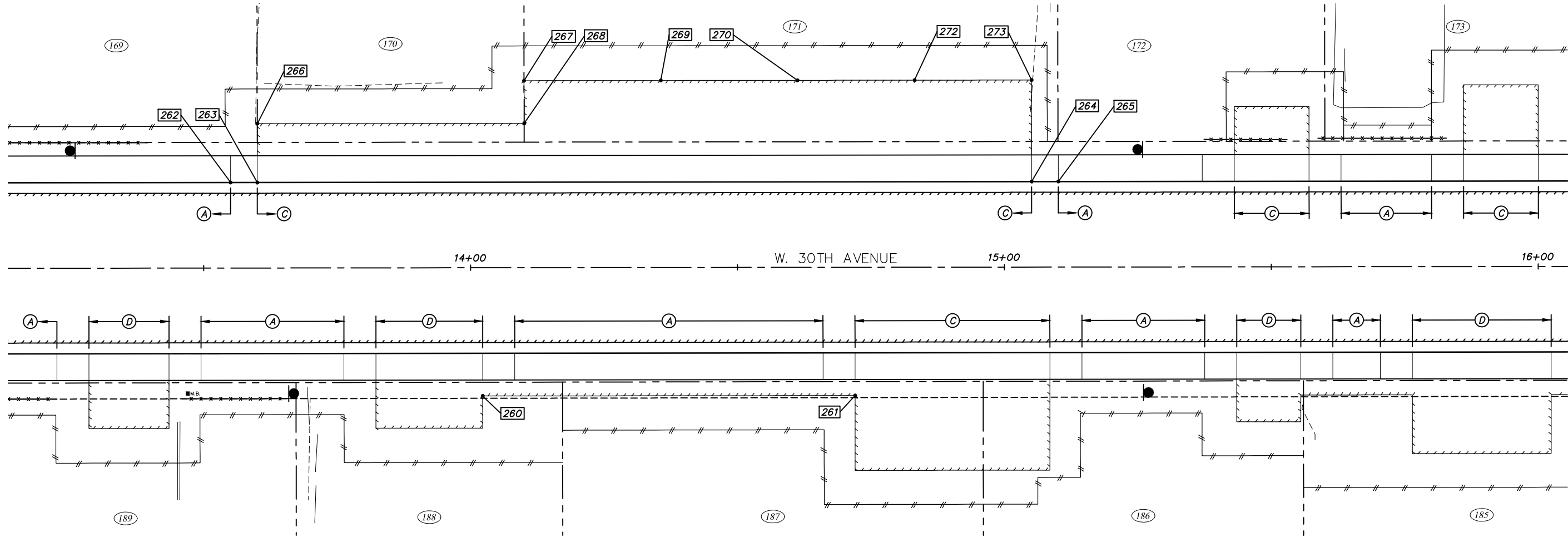
3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

CONSULTANT



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
INTERSECTION LAYOUT PLAN			
PATHWAY AT ARCTIC BOULEVARD			
SCALE	HOR. 1"=10' VER. N/A	GRID SW1629, SW1630 DATE NOV 2019 STATUS 65%	R10 of R11 SHEET

File: s:\data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Interaction Layout - Phase 1.dwg



DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(B)	TYPE 1A CURB
(C)	TYPE 2 CURB
(D)	TYPE 4 CURB

NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
- SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
- SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
- THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
- LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

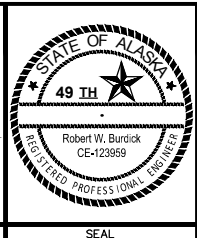
POINT SUMMARY – 30TH AVENUE DRIVEWAYS								
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
						LENGTH (FT)	SLOPE (%)	
260								
261								
262								
263								
264								
265								
266								
267								
268								
269								
270								
271								
272								
273								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____ DATE: _____	
BY: _____ TITLE: _____	
2. DATA TRANSFERRED BY: _____ TITLE: _____	
COMPANY: _____ DATE: _____	
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____ TITLE: _____	
COMPANY: _____ DATE: _____	
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56		94.77				
	CB 7C	See MOA Benchmark Book, Page D-17		106.10				
	GAAB 86	See MOA Benchmark Book, Page D-18		104.53				
STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								
PLAN CHECK								
CONSTRUCTION RECORD								
VERTICAL DATUM								
REVISIONS								
CONSULTANT								
SEAL								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY – PHASE 1		SCHED A
DRIVEWAY RECONSTRUCTION PLAN			
SCALE	HOR. 1"=10'	GRID SW1629, SW1630	
	VER. N/A	DATE NOV 2019	STATUS 65%
SHEET			R11 of R11

SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	6" THICK, AREA (SY)	REMARKS
R1	10+24.0	46.3 RT	10+26.6	33.7 RT	7		
R1	10+38.3	20.4 RT	11+64.5	16.0 RT	70		
R1	10+40.9	21.3 LT	10+57.9	16.0 LT	14		
R1	10+57.9	16.0 LT	12+79.9	16.0 LT		180	PARCEL 165-167 DRIVEWAY
R1	11+64.5	16.0 RT	12+10.5	16.0 RT		26	PARCEL 191 DRIVEWAY
R1	12+10.5	16.0 RT	12+25.3	16.0 RT	8		
R1	12+25.3	16.0 RT	12+88.7	16.0 RT		35	PARCEL 190 DRIVEWAY
R1	12+79.9	16.0 LT	13+55.1	16.0 LT	42		
R1	12+88.7	16.0 RT	13+22.5	16.0 RT	19		
R1	13+22.5	16.0 RT	13+49.5	16.0 RT		15	PARCEL 189 DRIVEWAY
R1	13+49.5	16.0 RT	13+76.3	16.0 RT	15		
R1	13+55.1	16.0 LT	15+10.1	16.0 LT		86	PARCEL 170 & 171 DRIVEWAY
R1	13+76.3	16.0 RT	14+08.3	16.0 RT		18	PARCEL 188 DRIVEWAY
R1	14+08.3	16.0 RT	14+66.0	16.0 RT	32		
R1	14+66.0	16.0 RT	15+14.5	16.0 RT		27	PARCEL 187 DRIVEWAY
R1	15+10.1	16.0 LT	15+37.1	16.0 LT	15		
R1	15+14.5	16.0 RT	15+37.5	16.0 RT	13		
R1	15+37.1	16.0 LT	15+63.1	16.0 LT		14	PARCEL 172 DRIVEWAY
R1	15+37.5	16.0 RT	15+61.5	16.0 RT		13	PARCEL 186 DRIVEWAY
R1	15+61.5	16.0 RT	15+70.4	16.0 RT	5		
R1	15+63.1	16.0 LT	15+80.1	16.0 LT	9		
R1/R2	15+70.4	16.0 RT	16+08.4	16.0 RT		21	PARCEL 185 DRIVEWAY
R1/R2	15+80.1	16.0 LT	16+06.1	16.0 LT		14	PARCEL 173 DRIVEWAY
R2	16+06.1	16.0 LT	16+19.1	16.0 LT	7		
R2	16+08.4	16.0 RT	16+89.3	16.0 RT	45		
R2	16+19.1	16.0 LT	16+51.1	16.0 LT		18	PARCEL 174 DRIVEWAY
R2	16+51.1	16.0 LT	17+07.3	16.0 LT	31		
R2	16+89.3	16.0 RT	17+35.3	16.0 RT		26	PARCEL 184 DRIVEWAY
R2	17+07.3	16.0 LT	17+37.3	16.0 LT		17	PARCEL 175 DRIVEWAY
R2	17+35.3	16.0 RT	17+94.0	16.8 RT	32		
R2	17+37.3	16.0 LT	17+54.1	16.0 LT	9		
R2	17+54.1	16.0 LT	17+90.1	16.0 LT		20	PARCEL 176 DRIVEWAY
R2	17+90.1	16.0 LT	18+25.8	13.5 LT	25		
R2	18+42.8	13.5 LT	18+56.2	13.5 LT	9		
R2	18+43.7	14.2 RT	18+53.0	14.8 RT	5		
R2	18+56.2	13.5 LT	18+92.2	13.5 LT		20	PARCEL 177 DRIVEWAY
R2	18+92.2	13.5 LT	19+87.0	13.5 LT	53		
R2	19+01.5	14.8 RT	20+99.5	13.5 RT	110		
R2	19+87.0	13.5 LT	20+23.0	13.5 LT		20	PARCEL 178 DRIVEWAY
R2	20+23.0	13.5 LT	20+84.0	13.5 LT	34		
R2	20+84.0	13.5 LT	21+30.0	13.5 LT		26	PARCEL 180 DRIVEWAY
R2	21+30.0	13.5 LT	22+80.8	14.7 LT	83		
R2	21+77.0	13.5 RT	22+81.8	14.8 RT	58		
R2	22+93.7	27.4 LT	22+94.6	33.5 LT	3		
R2	22+93.8	27.4 RT	22+94.7	33.5 RT	3		
R10	46+03.9	13.0 RT	46+03.9	25.1 RT	7		
R10	46+06.4	12.7 LT	16+08.9	13.0 RT		13	BIKE RAMP
R10	46+26.3	28.6 RT	16+40.2	28.6 RT	15		PEDESTRIAN REFUGE ISLAND

30.10

SHEET	APPX STATION	OFFSET (FT)	CURB RAMP AREA (SY)	DETECTABLE WARNING AREA (SF)	CURB RAMP TYPE	REMARKS
R1	10+32	27.2 RT	9	10	PARALLEL	SPENARD ROAD
R2	18+01	21.9 RT	7	9	PARALLEL	NORTH STAR STREET
R2	18+34	14.5 LT	14	10	PARALLEL	W 30TH AVENUE
R2	18+36	20.4 RT	11	9	PARALLEL	NORTH STAR STREET
R2	18+61	18.7 RT	7		PARALLEL	PARCEL 183 WEST
R2	18+94	19.4 RT	7		PARALLEL	PARCEL 183 WEST
R2	21+14	14.8 RT	9		UNIDIRECTIONAL	PARCEL 183 EAST
R2	21+63	14.8 RT	9	0	UNIDIRECTIONAL	PARCEL 183 EAST
R2	22+88	20.1 LT	9	10	PARALLEL	ARCTIC BOULEVARD
R2	22+88	20.1 RT	9	10	PARALLEL	ARCTIC BOULEVARD
R10	46+08	33.6 RT	9	10	PARALLEL	ARCTIC BOULEVARD
R10	46+29	33.6 RT	N/A	20	N/A	PEDESTRIAN REFUGE ISLAND
R10	46+37	33.6 RT	N/A	20	N/A	PEDESTRIAN REFUGE ISLAND
R10	46+59	33.7 RT	9	10	PARALLEL	ARCTIC BOULEVARD

1. SEE INTERSECTION LAYOUT SHEETS R5-R11 FOR FOR LOCATIONS OF CURB RAMPS AND DETECTABLE WARNINGS.

COLORED CONCRETE (4" THICK, RED, RUNNING BOND TILE PATTERN)						
SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	REMARKS
R2	16+51.1	16.0 LT	17+07.3	16.0 LT	14	
R10	46+30.1	68.3 RT	46+33.0	39.4 RT	6	REFUGE ISLAND
R10	46+33.5	27.9 RT	46+33.5	21.8 RT	22	REFUGE ISLAND

RECORD DRAWING		DATA		DRAWN BY	CHECKED BY																				
1. DATA PROVIDED BY: _____ TITLE: _____						BASE TS MJ																			
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.						TOPOGRAPHY BW BJW																			
CONTRACTOR: _____						PROFILE RB JKH																			
BY: _____ TITLE: _____ DATE: _____						STORM SEWER MV JKH																			
2. DATA TRANSFERRED BY: _____ TITLE: _____ DATE: _____						WATER/SANITARY SEWER RK JK																			
COMPANY: _____						GAS RK JK																			
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.						TELEPHONE RK JK																			
DATA TRANSFER CHECKED BY: _____ TITLE: _____						ELECTRIC JH TK																			
COMPANY: _____ DATE: _____						DESIGN RB JK																			
BY: _____						QUANTITIES RB JK																			
						PRELIMINARY/FINAL RB JK																			
						MUNICIPAL/STATE RB JK																			
						PLAN CHECK		CONSTRUCTION RECORD				VERTICAL DATUM				REVISIONS				CONSULTANT		SEAL			
						FIELD BOOKS		BM NO.		LOCATION		ELEV.	REV	DATE	DESCRIPTION		BY								
						DESIGN CRW BOOK No. 149, 169,		CB 70		See MOA Benchmark Book, Page D-56		94.77													
						195 & 196		CB 7C		See MOA Benchmark Book, Page D-17		106.10													
						STAKING		GAAB 86		See MOA Benchmark Book, Page D-18		104.53													
						ASBUILT		BASIS OF THIS DATUM GAAB 1972 ADJUST																	
						CONTRACTOR																			
						INSPECTOR																			
						CRW ENGINEERING GROUP LLC																			
						3940 ARCTIC BLVD. SUITE 300																			
						ANCHORAGE, ALASKA 99503																			
						PHONE: (907) 562-3252																			
						#AECL882-AK																			
						PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT																			
						16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A																			
						SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1																			
						ROADWAY SUMMARY TABLES																			
						SCALE HOR. N/A VER. N/A		GRID SW1629, SW1630																	
						DATE NOV 2019		STATUS 65%																	
						SHEET T ² of T ³																			

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50.03

CONSTRUCT SANITARY SEWER MANHOLE (TYPE A)

SHEET	STATION	OFFSET (FT)	REMARKS
R2	22+68.45	5.4 LT	

SANITARY SEWER MANHOLE NOTES:

1. SEE MASS DETAIL 50-01.

50.06

REMOVE AND REPLACE MANHOLE CONE SECTION OR MANHOLE COVER AND FRAME

SHEET	STATION	OFFSET (FT)	CONE SECTION	COVER AND FRAME	REMARKS
R2	18+05	5.0 LT		X	
R2	21+65	5.2 LT	X		
R3	34+72	14.3 LT		X	

SANITARY SEWER MANHOLE CONE/RING NOTES:

1. SEE MASS DETAILS 50-25 AND 50-26.

2. COORDINATE W/ ENGINEER IN FIELD TO VERIFY WHETHER CONE SECTION OR MANHOLE COVER AND FRAME REPLACEMENT IS REQUIRED.

55.07 & 55.08

ADJUST STORM DRAIN MANHOLE CONE OR RING

SHEET	STATION	OFFSET (FT)	CONE	RING	REMARKS
R2	23+20	16.6 LT		X	
R4	30+34	12.7 RT		X	
R4	46+10	21.9 RT		X	
R4	46+33	20.3 RT	X		

ADJUST STORM DRAIN MANHOLE RING NOTES:

1. SEE MASS DETAIL 55-17 AND 55-18.

60.04

FURNISH AND INSTALL FIRE HYDRANT ASSEMBLY SINGLE PUMPER

SHEET	STATION	OFFSET (FT)	REMARKS
B1	16+25	23.0 RT	CONNECT TO EXISTING HYDRANT LEG

70.18

CHAIN LINK FENCE (6', 9 GAUGE)

SHEET	PROPOSED LOCATION				LENGTH (FT)	REMARKS
	APPROX BEGIN	APPROX BEGIN	APPROX END	APPROX END		
	STATION	OFFSET (FT)	STATION	OFFSET (FT)		
R4	45+00.0	10.0 LT	45+54.4	10.0 LT	54.4	INSTALL BARBED WIRE PER MASS WITH VINYL SLATS TO MATCH COLOR OF RESET FENCE

60.03 & 60.05

REMOVE AND REPLACE VALVE BOX TOP SECTION OR ADJUST KEY BOX

SHEET	STATION	OFFSET (FT)	KEY BOX	VALVE BOX TOP SECTION	REMARKS
R1	10+38	20.4 RT		X	
R1	10+45	26.5 RT		X	
R1	10+72	19.5 LT	X		
R1	11+13	20.3 RT	X		
R1	12+03	23.5 RT	X		
R1	12+24	23.5 RT	X		
R1	12+31	28.7 RT	X		
R1	12+61	22.1 LT	X		
R1	13+31	31.9 RT	X		
R1	13+46	24.4 LT	X		
R1	13+70	23.8 LT	X		
R1	13+87	21.4 RT	X		
R1	14+22	23.8 LT	X		
R1	14+66	21.4 LT	X		
R1	15+11	16.7 LT	X		
R1	15+47	21.3 RT	X		
R1	15+97	29.7 RT	X		
R2	16+30	21.3 RT	X		
R2	17+18	14.4 RT		X	
R2	17+21	24.9 LT	X		
R2	17+84	21.0 RT	X		
R2	19+43	21.4 LT	X		
R2	20+07	18.2 RT	X		
R2	20+20	17.8 RT		X	
R4	46+23	16.7 RT		X	
R4	46+25	38.6 RT		X	
R4	46+26	28.2 RT		X	

SPECIAL FILL GRADING TABLE

SHEET	APPROX BEGIN STATION	APPROX END STATION	OFFSET	REMARKS
R1	11+33	11+57	21.0 LT	
R1	11+64	11+71	21.0 LT	
R2	19+08	20+15	18.5 LT	
R2	20+70	20+90	18.5 LT	
R2	21+24	21+60	18.5 LT	
R3	30+40	30+70	12.0 LT	
R3	31+05	31+75	12.0 LT	
R3	33+61	33+88	12.0 LT	
R3	34+20	34+74	12.0 LT	

SPECIAL FILL GRADING NOTES:

- SPECIAL FILL GRADING SHALL BE PER DETAIL 2, SHEET C5.
- LOCATIONS ARE APPROXIMATE, CONTRACTOR SHALL MODIFY LOCATIONS IN THE FIELD PER THE DIRECTION OF THE ENGINEER OR AS NECESSARY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

RECORD DRAWING

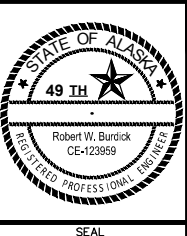
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST					
INSPECTOR							

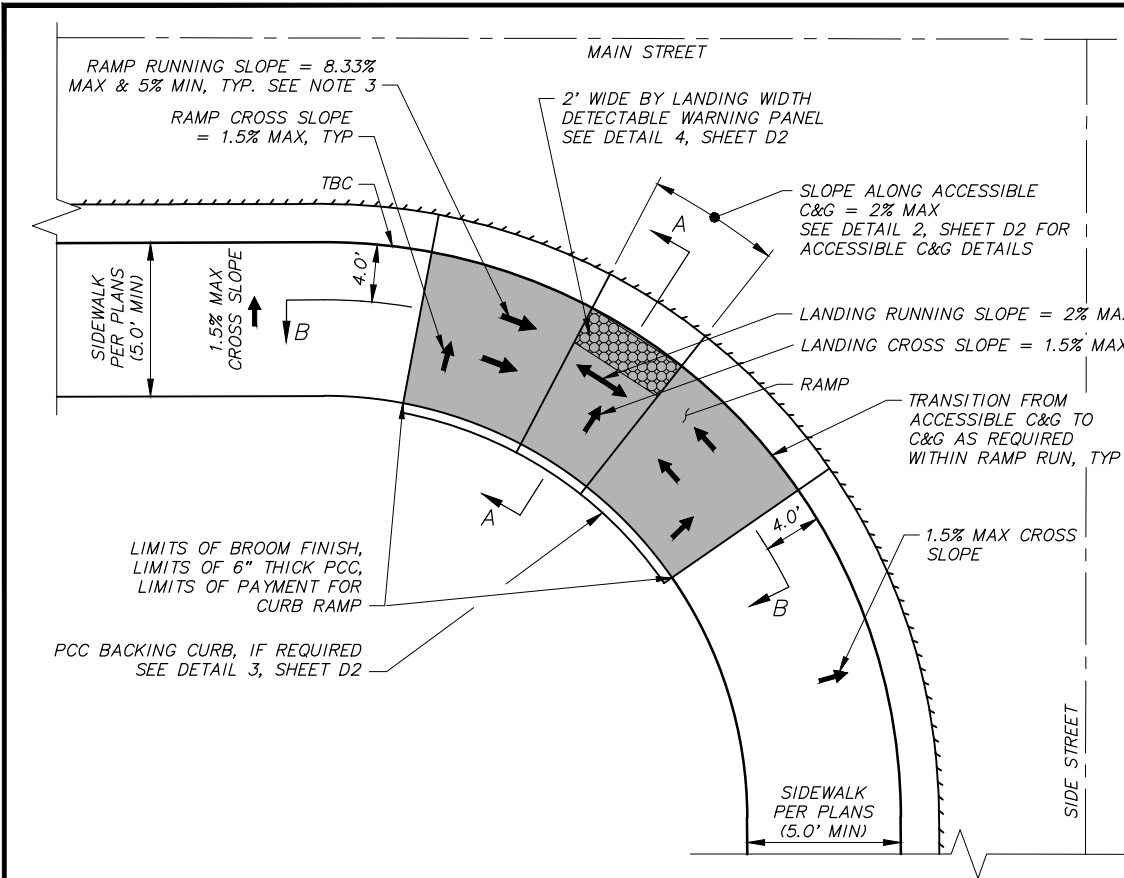


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

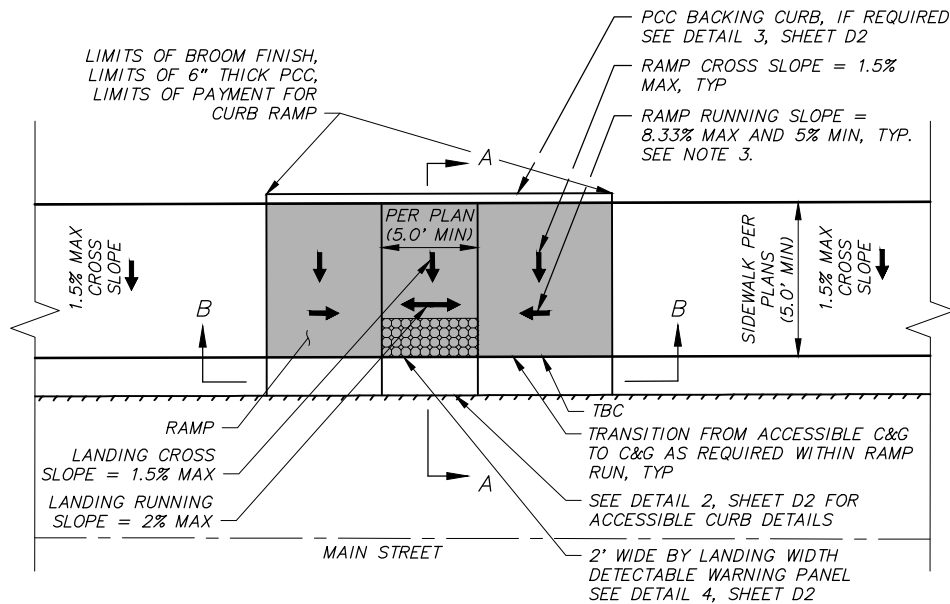
ROADWAY SUMMARY TABLES

SCALE HOR. N/A VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET T3 of T3



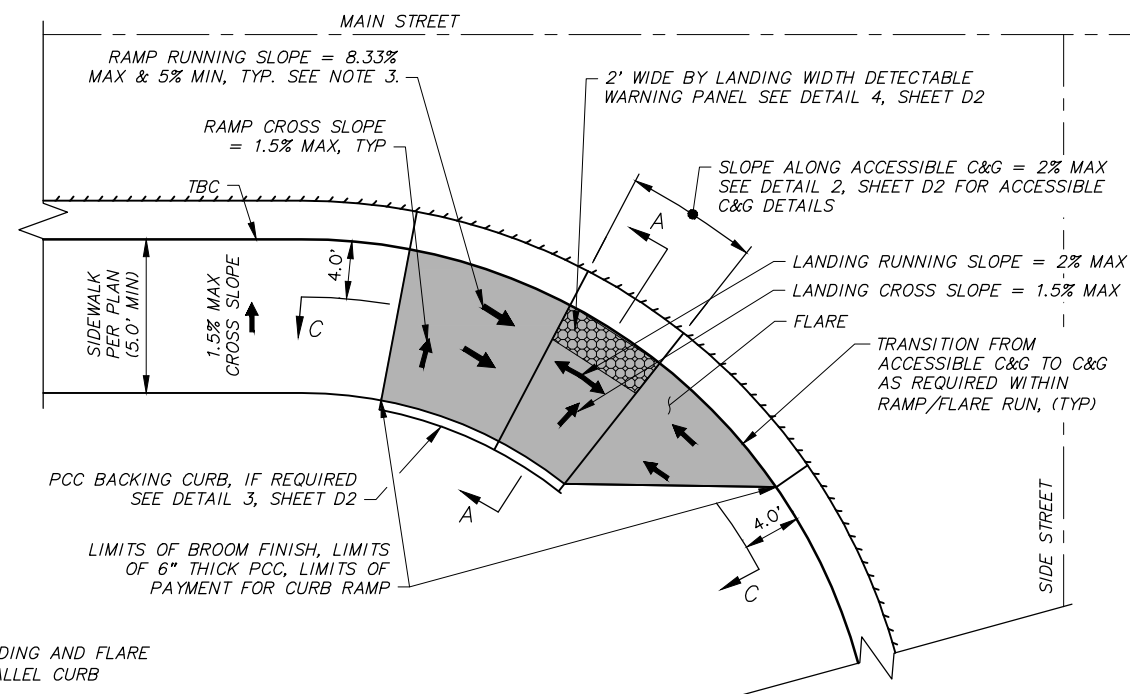
**TYPICAL PARALLEL CURB RAMP AT
CORNER LOCATION WITH CONNECTING
SIDE STREET SIDEWALK - PLAN VIEW**

SCALE: NTS



TYPICAL PARALLEL CURB RAMP AT NON-CORNER LOCATION - PLAN VIEW

SCALE: NTS

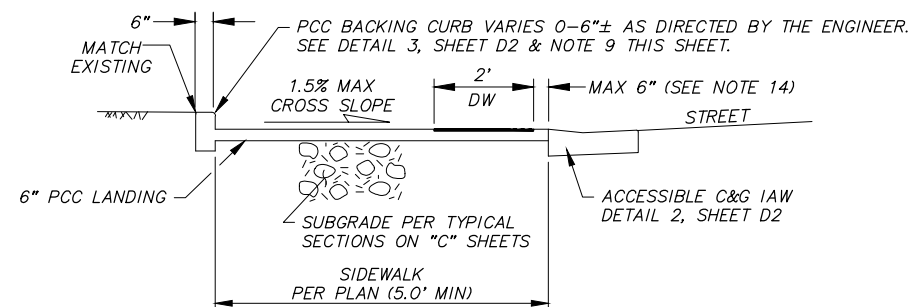


**TYPICAL PARALLEL CURB RAMP AT
CORNER LOCATION WITHOUT CONNECTING
SIDE STREET SIDEWALK - PLAN VIEW**

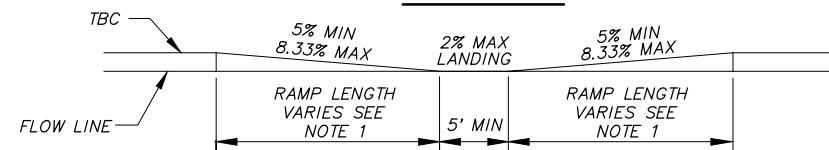
SCALE: NTS

SHEET NOTES:

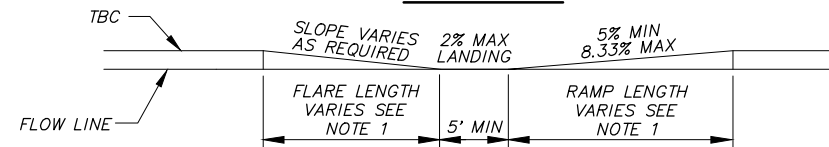
1. SEE SHEETS R5-R10 FOR CURB RAMP TYPES, LOCATIONS, RAMP, LANDING AND FLARE LENGTHS AND ELEVATIONS. RAMP/FLARE/LANDING LENGTH FOR PARALLEL CURB RAMPS SHALL BE AS MEASURED 4' OFF BACK OF CURB.
2. NOTIFY ENGINEER PRIOR TO INSTALLATION OF CONCRETE IF MAXIMUM/MINIMUM SLOPES CANNOT BE MAINTAINED.
3. FOR PARALLEL CURB RAMPS, RAMPS SHALL BE 15 FEET MAXIMUM. RAMPS SHALL HAVE THE OUTSIDE EDGES AND JOINTS TRIMMED WITH A 1/4-INCH RADIUS EDGING TOOL.
4. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
5. MINIMUM FLOWLINE SLOPE IN CURB RETURN IS 0.5%, UNLESS OTHERWISE NOTED.
6. PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
7. CONSTRUCT SIDEWALK ADJACENT TO CURB RAMP PER THE TYPICAL SECTIONS SHOWN ON THE "C" SHEETS.
8. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "P.C.C. CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
9. FORM BACKING CURB AS DIRECTED BY THE ENGINEER TO MATCH EXISTING GROUND. PAYMENT FOR THIS CURB SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE. IF EXISTING GROUND BEHIND SIDEWALK IS GRAVEL OR GRASS, GRADE TO MATCH EXISTING GROUND. PAYMENT FOR GRADING SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE. 4" TOPSOIL AND SEEDING SHALL BE PLACED ON DISTURBED GRASS AREAS PER THE LANDSCAPING PLANS.
10. CONSTRUCT RAMPS AND LANDINGS WITH A BROOM FINISH RUNNING PERPENDICULAR TO THE DIRECTION OF TRAVEL.
11. INSTALL YELLOW ADA APPROVED DETECTABLE WARNINGS (DW) PANELS UNLESS OTHERWISE NOTED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND THESE DRAWINGS. SET DETECTABLE WARNINGS SO THAT THE FIELD AREA AT THE BASE OF THE DOMES IS FLUSH WITH THE SURROUNDING CONCRETE. THERE SHALL BE NO LIP AT THE EDGE OF THE DETECTABLE CURB WARNINGS. SEE DETAIL 4, SHEET D2.
12. DETECTABLE WARNINGS DOMES AT PARALLEL CURB RAMPS SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINATE DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
13. RAMP LOCATIONS MAY BE ADJUSTED TO ENSURE MINIMUM 48" CLEARANCE AROUND APPURTENANCES SUCH AS SIGNAL POLES, POWER POLES, LIGHT POLES, J-BOXES, SIGNS, CATCH BASINS AND MANHOLES. PRIOR TO PLACEMENT OF CONCRETE AND APPURTENANCES, THE RAMP LAYOUT AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
14. GAP BETWEEN DETECTABLE WARNING PANELS AND BACK OF CURB ONLY ALLOWABLE AT CENTER OF CURB RAMPS. CORNERS OF DETECTABLE WARNINGS SHALL BE FLUSH WITH BACK OF CURB. IF REQUIRED BY THE ENGINEER CONTRACTOR SHALL CUT DETECTABLE WARNING PANELS PER THE MANUFACTURER'S RECOMMENDATIONS. CUTTING DW PANELS SHALL BE INCIDENTAL TO 30.04 DETECTABLE WARNINGS PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.



SECTION A-A



SECTION B-B



SECTION C-C

TYPICAL CURB RAMP SECTIONS

SCALE: NTS

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION		BY
DESIGN CRW BOOK No. 149, 169, 195 & 196		CB 7D	See MOA Benchmark Book, Page D-56	94.77					
STAKING		CB 7C	See MOA Benchmark Book, Page D-17	106.10					
		GAAB 86	See MOA Benchmark Book, Page D-18	104.55					
ASBUILT									
CONTRACTOR INSPECTOR		BASIS OF THIS DATUM GAAB 1972 ADJUST							
CONSTRUCTION RECORD		VERTICAL DATUM					REVISIONS		

CRW
ENGINEERING GROUP, LLC
3940 ARCTIC BLVD, SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLEB2-AK

CONSULTANT

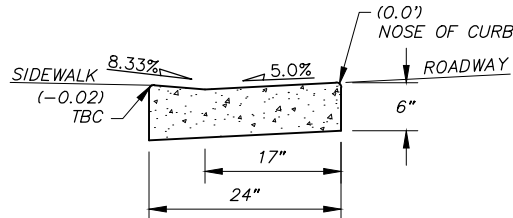
SEAL



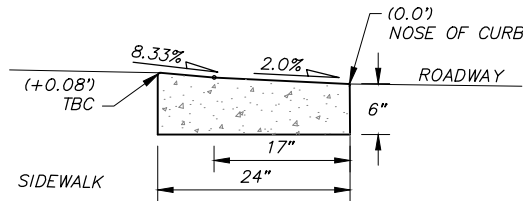
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
ROADWAY DETAILS			
CURB RAMPS			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	D1 of D5 SHEET
		STATUS 65%	

1. SEE SHEET NOTES ON SHEET D1.

SCALE: NTS



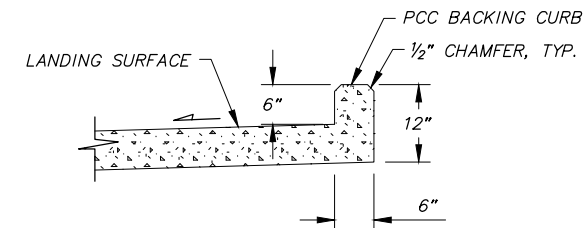
PCC CURB AND GUTTER TYPE 1A
FOR USE IN CURB RAMPS WITH TYPE 1 C&G.



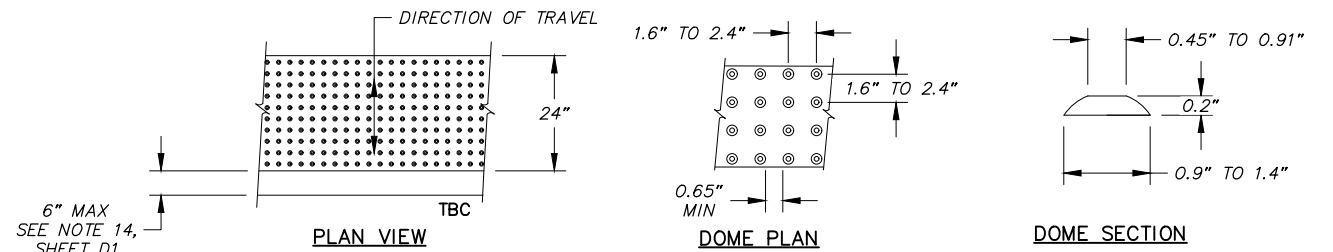
PCC CURB AND GUTTER TYPE 3A
FOR USE IN CURB RAMPS WITH TYPE 3 C&G.

2. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.

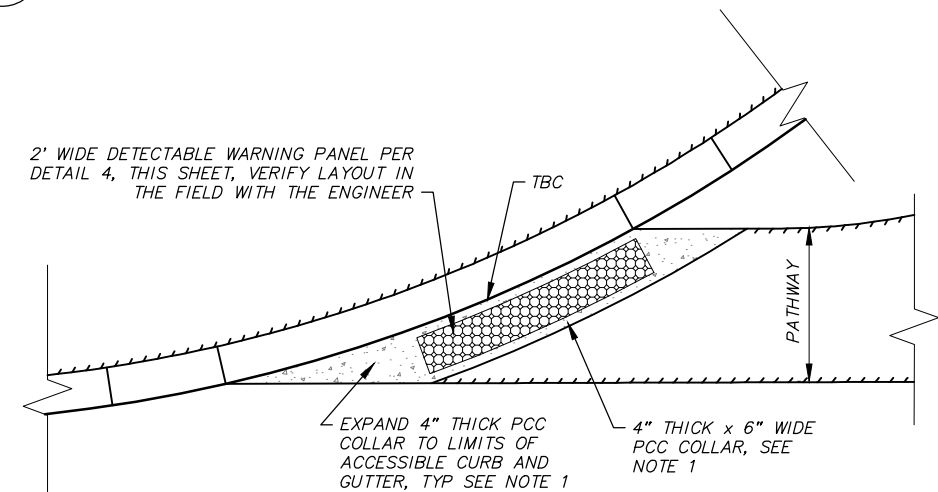
SCALE: NTS



SCALE: NTS






SCALE: NTS



1. ALL WORK & MATERIALS REQUIRED FOR INSTALLING DETECTABLE WARNING PANEL SHALL BE INCIDENTAL TO ITEM 30.04 DETECTABLE WARNINGS PAY ITEM AND NO SEPERATE PAYMENT SHALL BE MADE.

SCALE: NTS

RECORD DRAWING										<div> CRW ENGINEERING GROUP LLC 3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC0882-AK</div>		<div> 49 TH Justin T. Keene CE-11776 REGISTERED PROFESSIONAL ENGINEER</div>		<div> MUNICIPALITY OF ANCHORAGE</div>		PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT									
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1																									
ROADWAY DETAILS																									
CURB RAMPS																									
SCALE		HOR. N/A VER. N/A		GRID SW1623; SW1630 DATE NOV 2019		STATUS 65%		SHEET								D2 of D5									
1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ TITLE: _____ DATE: _____ BY: _____																DATA		DRAWN BY		CHECKED BY					
2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____																BASE		TS		M.J					
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____																TOPOGRAPHY		B.W		B.W					
																PROFILE		R.B		J.K					
																STORM SEWER		M.V		J.H					
										WATER/SANITARY SEWER		R.K		J.K											
										GAS		R.K		J.K											
										TELEPHONE		R.K		J.K											
										ELECTRIC		J.H		T.K											
										DESIGN		R.B		J.K											
										QUANTITIES		R.B		J.K											
										PRELIMINARY/FINAL		R.B		J.K											
										MUNICIPAL/STATE		R.B		J.K											
										FIELD BOOKS		BM NO.		LOCATION		ELEV.		REV		DATE		DESCRIPTION		BY	
										DESIGN CRW BOOK No. 149, 169, 195 & 196		CB 7C		See MOA Benchmark Book, Page D-17		106.10									
										STAKING		GAAB 86		See MOA Benchmark Book, Page D-18		104.53									
										ASBUILT															
										CONTRACTOR															
										INSPECTOR															
										BASIS OF THIS DATUM		GAAB 1972 ADJUST													
										PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS		CONSULTANT		SEAL					



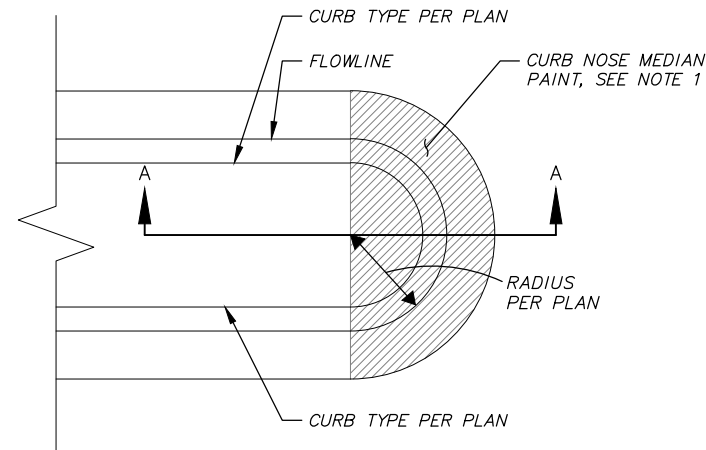
1. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
2. PAYMENT FOR PCC CURB & GUTTER (ALL TYPES) AND TRANSITION C&G SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER, (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
3. CENTER THE PROPOSED DRIVEWAY ENTRANCES ON DRIVEWAY CENTERLINE REFERENCE POINT AS SHOWN IN THE 20.28 RECONSTRUCT DRIVEWAY SUMMARY TABLES PROVIDED ON THE ROADWAY SUMMARY TABLE "T" SHEETS OR AS SHOWN ON THE DRIVEWAY RECONSTRUCTION PLANS.
4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS LANDINGS AND RAMP TRANSITIONS AND ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
5. SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLES ON THE ROADWAY SUMMARY TABLE "T" SHEETS AND DRIVEWAY RECONSTRUCTION PLANS, FOR INDIVIDUAL DRIVEWAY SPECIFICS.
6. WHERE INSULATION IS INSTALLED IN ROADWAY, INSTALL INSULATION UNDER DRIVEWAY PER DETAIL 4, SHEET C5.



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29		W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	
ROADWAY DETAILS			
DRIVEWAYS			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET D3 of D5

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
<h2 style="margin: 0;">ROADWAY DETAILS</h2>			
MISCELLANEOUS			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET D4 of D5

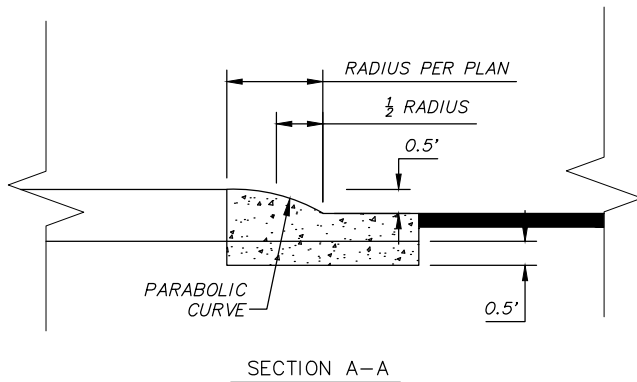
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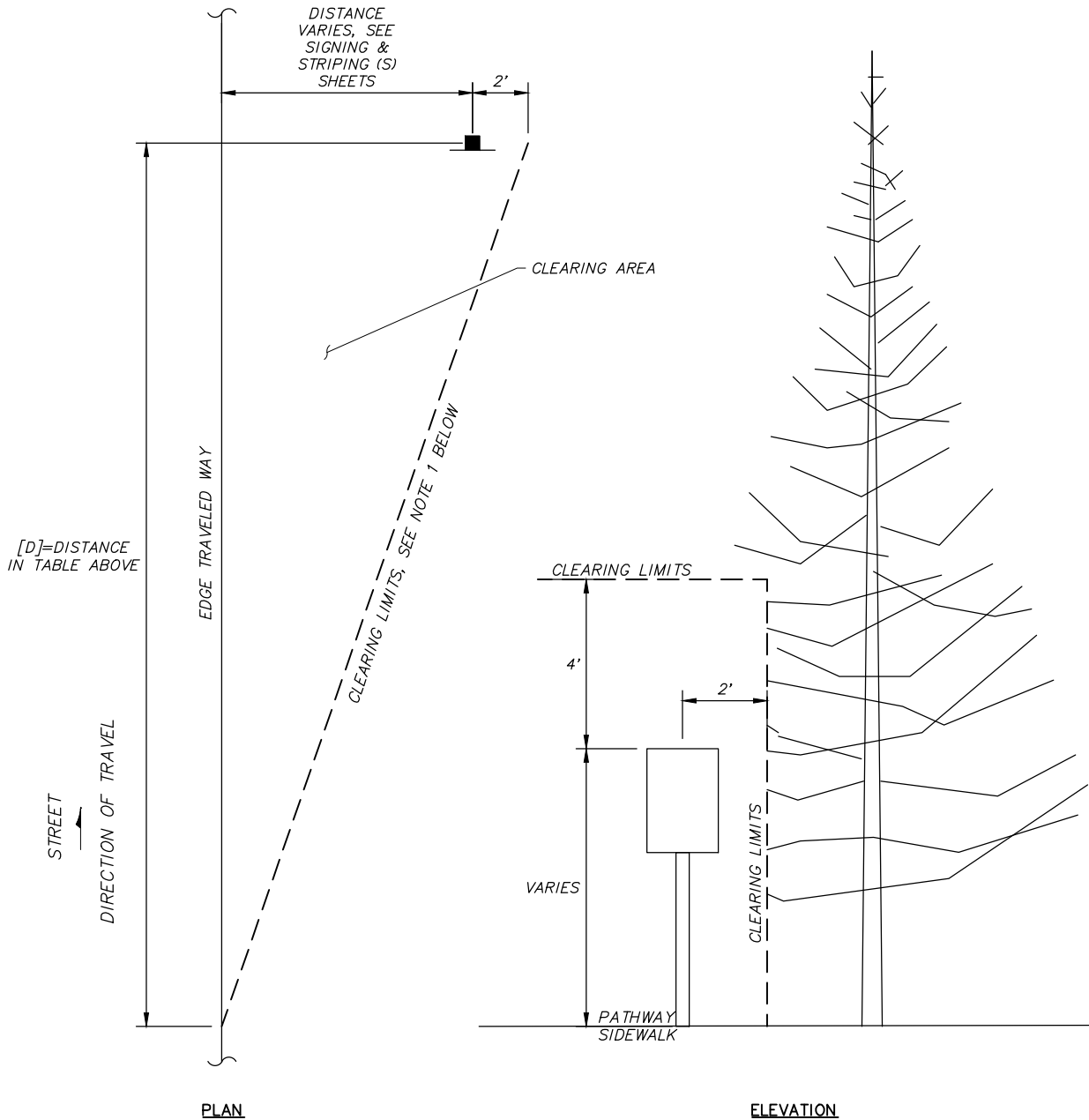
SCALE: NTS

1. SEE "RELOCATE MAILBOX" TABLE, DEMOLITION SHEETS & ROADWAY SHEETS FOR LOCATING MAILBOXES ALONG ROADWAY. LOCATIONS ARE APPROXIMATE, VERIFY LOCATION WITH ENGINEER PRIOR TO INSTALLATION.
2. MAILBOXES AND SUPPORTS SHALL CONFORM WITH U.S. POSTAL SERVICE REGULATIONS.
3. NEWSPAPER RECEPTACLES SHALL CONFORM TO THE SAME SETBACK AND SUPPORT REGULATIONS AS MAILBOXES. WHERE NEWSPAPER RECEPTACLES AND MAILBOXES ARE TO BE MOUNTED TOGETHER, THE NEWSPAPER RECEPTACLE SHALL BE MOUNTED BELOW THE BOTTOM SURFACE OF THE MAILBOX.
4. CONTRACTOR SHALL COORDINATE WITH THE MOA AND ENGINEER IN THE FIELD REGARDING MAILBOX SUBSTITUTIONS OR MAILBOX SIZING, PRIOR TO ORDERING MATERIALS.
5. CONTRACTOR SHALL INSTALL MAILBOX ADDRESS LABELS TO MATCH EXISTING LABELS. ADDRESS LABELS SHALL BE A MINIMUM OF 1" IN HEIGHT AND INSTALLED ON THE SIDE OF THE MAILBOX VISIBLE FROM ON COMING TRAFFIC. ADDRESS LABELS SHOULD BE CENTERED BOTH VERTICAL AND HORIZONTAL ON MAILBOX.
6. ALL WOOD SHALL BE PRESSURE TREATED WOOD SEALED WITH A SEMI-TRANSPARENT OIL BASED STAIN BROWN IN COLOR. SUBMIT COLOR SAMPLE FOR APPROVAL.

1. CURB NOSE MEDIAN SHALL BE PAINTED WITH YELLOW REFLECTIVE TRAFFIC PAINT. PAINTING & MATERIALS SHALL BE INCIDENTAL TO CURB NOSE MEDIAN PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.



CLEARING DISTANCE	
SPEED LIMIT (MPH)	DISTANCE[D] (FT)
20	100
25	125
30	150
35	175



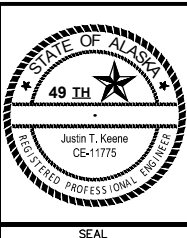
1. MAINTAIN CLEARING LIMITS WITHIN AVAILABLE RIGHT-OF-WAY.

SCALE: NTS

RECORD DRAWING		
1. DATA PROVIDED BY: _____	TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.		
CONTRACTOR: _____		
BY: _____	TITLE: _____	DATE: _____
2. DATA TRANSFERRED BY: _____	TITLE: _____	
COMPANY: _____	DATE: _____	
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.		
DATA TRANSFER CHECKED BY: _____	TITLE: _____	
COMPANY: _____	DATE: _____	
BY: _____		

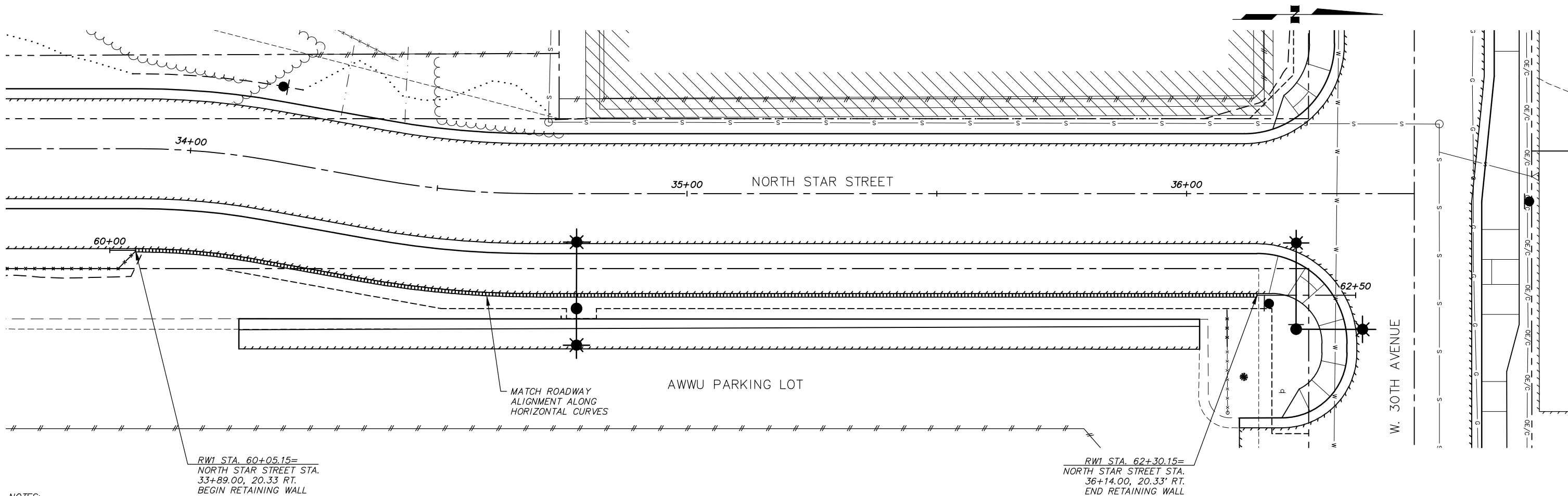
DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK
PLAN CHECK		

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 70	See MOA Benchmark Book, Page D-56	94.77					
	CB 7C	See MOA Benchmark Book, Page D-17	106.10					
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
ASBUILT								
CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST							
INSPECTOR								
CONSTRUCTION RECORD	VERTICAL DATUM			REVISIONS				



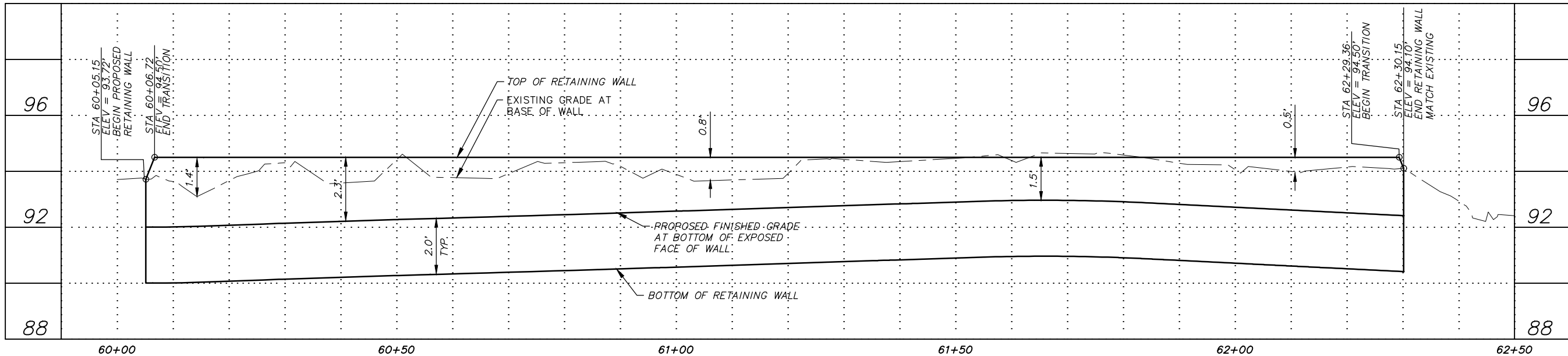
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	SCHED A	
ROADWAY DETAILS			
MISCELLANEOUS			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV. 2019 STATUS 65%	D5 of D5 SHEET

File: s:\jobdata\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Retaining Wall Plan & Profile - Phase 1.dwg



NOTES:

- SEE TYPICAL RETAINING WALL SECTIONS, SHEET RW2
- RETAINING WALL PROFILE IS APPROXIMATE. CONTRACTOR SHALL INSTALL WALL AS NECESSARY TO MATCH GRADE.



RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	
BY: _____	TITLE: _____ DATE: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	JS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							

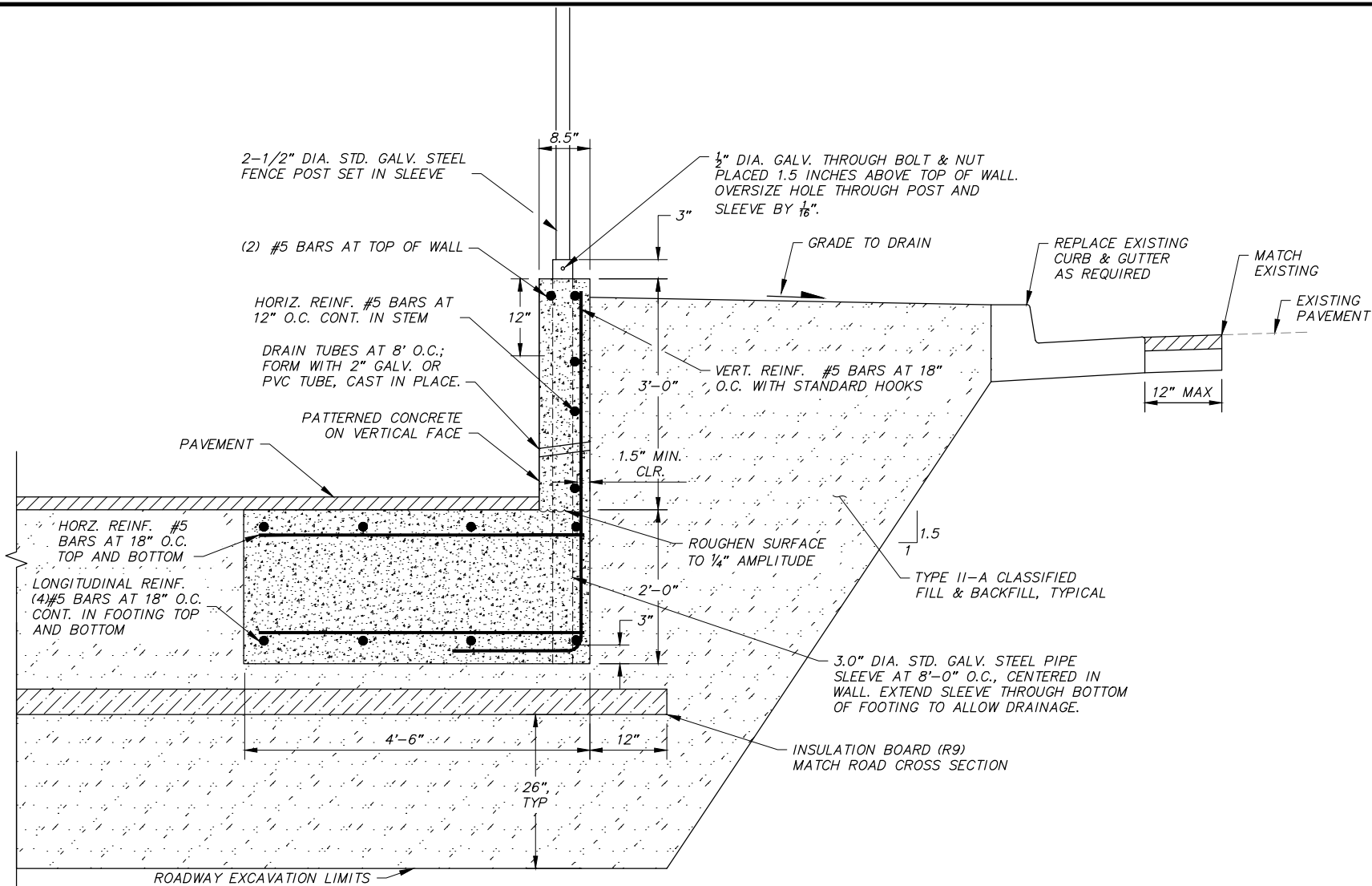
20 10 0 10 20
GRAPHIC SCALE

CRW ENGINEERING GROUP LLC
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC1882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29		W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A	
		SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	
RETAINING WALL PLAN & PROFILE			
SCALE HOR. 1"=10' VER. 1"=2'		GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET RW1 of RW2	

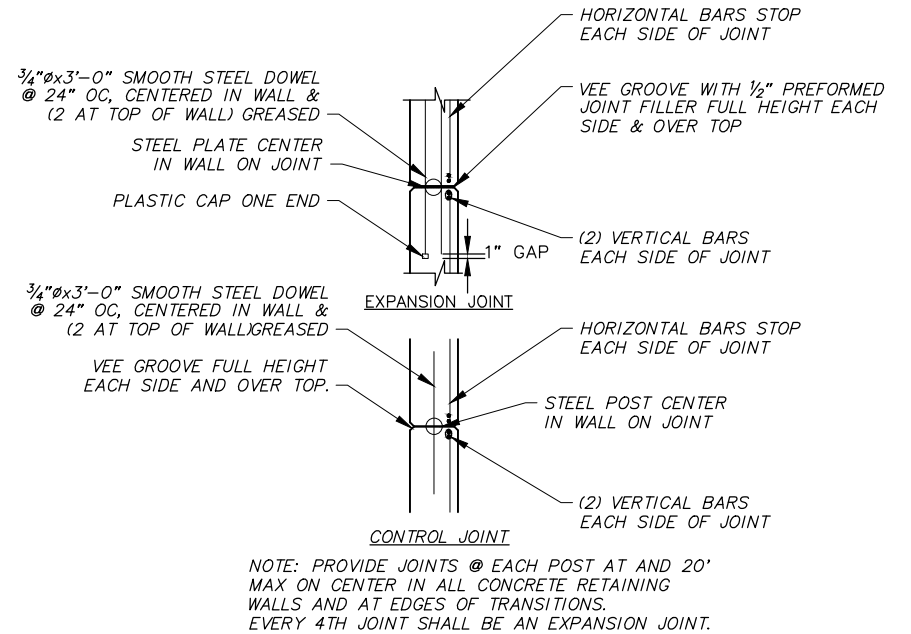
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1 RETAINING WALL SECTION

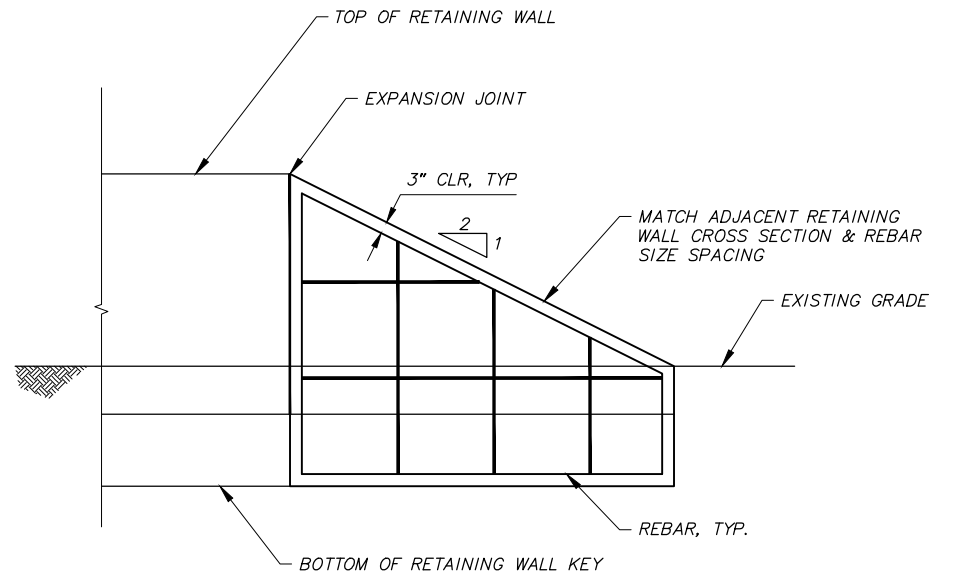
SCALE: NTS

LAP LENGTH (INCHES)		
BAR SIZE	NORMAL	TOP
#4	24	31
#5	30	38
#6	35	46
#7	51	67
#8	59	76
#9	66	86
#10	74	96



2 VERTICAL JOINTS PLAN VIEW

SCALE: NTS



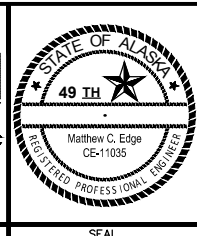
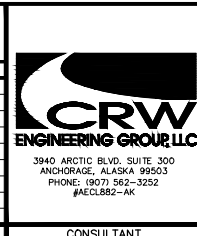
3 TRANSITION DETAIL

SCALE: NTS




RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	
BY: _____	TITLE: _____ DATE: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	
DATA TRANSFER CHECKED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

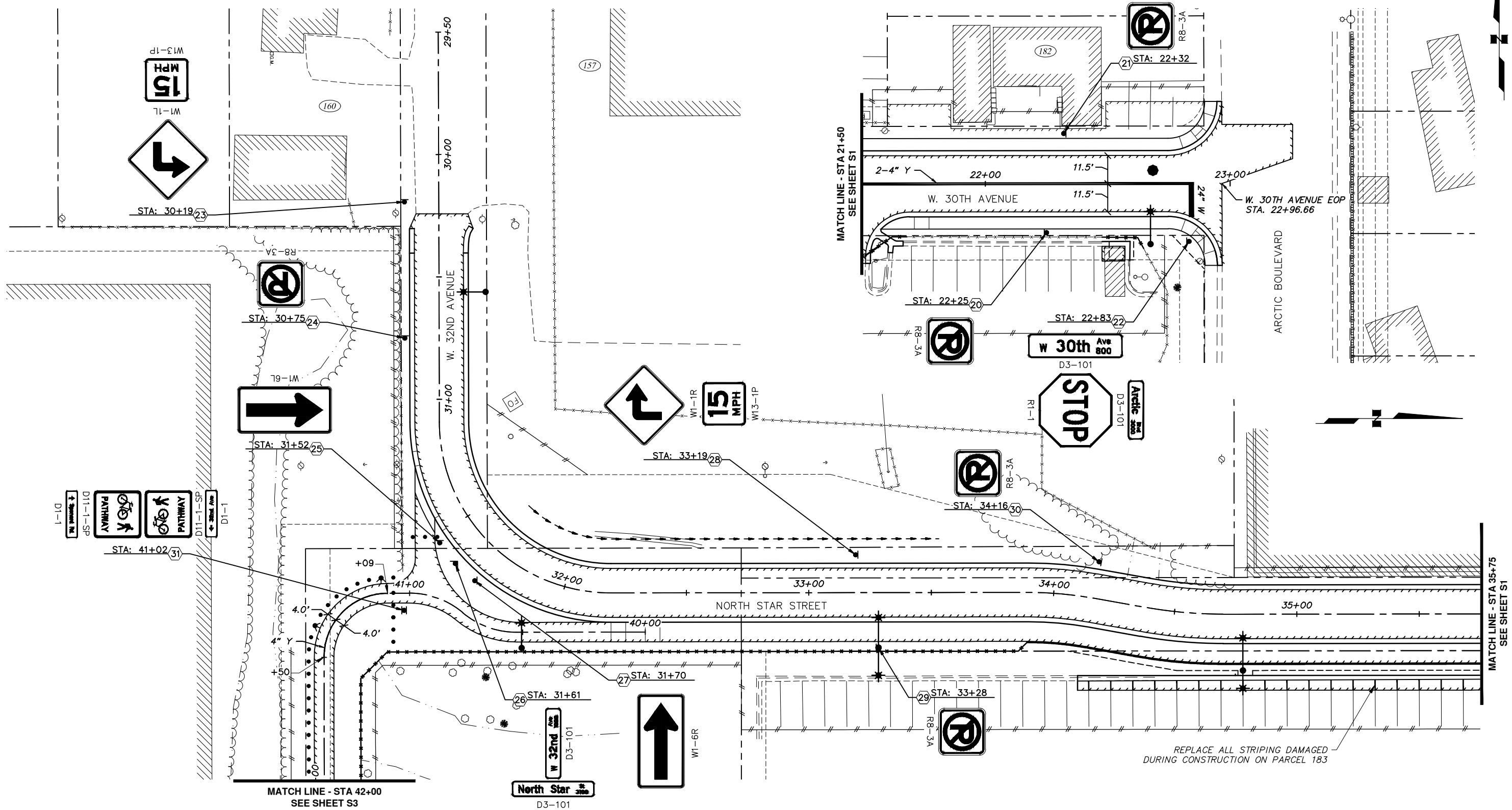
FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
PLAN CHECK							
CONSTRUCTION RECORD							
VERTICAL DATUM							
REVISIONS							
CONSULTANT							
SEAL							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
RETAINING WALL DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65%
SHEET			RW2 of RW2

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____ 2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____			<table><tr><th>DATA</th><th>DRAWN BY</th><th>CHECKED BY</th></tr><tr><td>BASE</td><td>TS</td><td>MJ</td></tr><tr><td>TOPOGRAPHY</td><td>BW</td><td>BW</td></tr><tr><td>PROFILE</td><td>RB</td><td>JK</td></tr><tr><td>STORM SEWER</td><td>MV</td><td>JH</td></tr><tr><td>WATER/SANITARY SEWER</td><td>RK</td><td>JK</td></tr><tr><td>GAS</td><td>RK</td><td>JK</td></tr><tr><td>TELEPHONE</td><td>RK</td><td>JK</td></tr><tr><td>ELECTRIC</td><td>JH</td><td>TK</td></tr><tr><td>DESIGN</td><td>RB</td><td>JK</td></tr><tr><td>QUANTITIES</td><td>RB</td><td>JK</td></tr><tr><td>PRELIMINARY/FINAL</td><td>RB</td><td>JK</td></tr><tr><td>MUNICIPAL/STATE</td><td>RB</td><td>JK</td></tr></table>			DATA	DRAWN BY	CHECKED BY	BASE	TS	MJ	TOPOGRAPHY	BW	BW	PROFILE	RB	JK	STORM SEWER	MV	JH	WATER/SANITARY SEWER	RK	JK	GAS	RK	JK	TELEPHONE	RK	JK	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<div><div>200204060</div><div>GRAPHIC SCALE</div></div> <table><tr><th>FIELD BOOKS</th><th>BM NO.</th><th>LOCATION</th><th>ELEV.</th><th>REV</th><th>DATE</th><th>DESCRIPTION</th><th>BY</th></tr><tr><td>DESIGN CRW BOOK No. 149, 169, 195 & 196</td><td>CB 7D</td><td>See MOA Benchmark Book, Page D—56</td><td>94.77</td><td></td><td></td><td></td><td></td></tr><tr><td>STAKING</td><td>CB 7C</td><td>See MOA Benchmark Book, Page D—17</td><td>106.10</td><td></td><td></td><td></td><td></td></tr><tr><td></td><td>GAAB 86</td><td>See MOA Benchmark Book, Page D—18</td><td>104.53</td><td></td><td></td><td></td><td></td></tr><tr><td>ASBUILT</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>CONTRACTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td>INSPECTOR</td><td></td><td></td><td></td><td></td><td></td><td></td><td></td></tr><tr><td colspan="8">BASIS OF THIS DATUM GAAB 1972 ADJUST</td></tr></table>			FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D—56	94.77					STAKING	CB 7C	See MOA Benchmark Book, Page D—17	106.10						GAAB 86	See MOA Benchmark Book, Page D—18	104.53					ASBUILT								CONTRACTOR								INSPECTOR								BASIS OF THIS DATUM GAAB 1972 ADJUST								<div><div><div>3940 ARCTIC BLVD, SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECCL882-AK</div></div><div><div>STATE OF ALASKA 49 TH Colin M. Singleton CE-124620 REGISTERED PROFESSIONAL ENGINEER</div></div></div>			<div><div><div>PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT</div><div>16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY – PHASE 1</div><div>SCHED A</div><div>SIGNING & STRIPING PLAN</div><div>W. 30TH AVENUE BOP TO STA. 21+50 NORTH STAR STREET STA. 35+75 TO EOP</div></div></div>		
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File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan Over Plan - S365 - Phase 1.dwg



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
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FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
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ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							
REVISIONS							

CRW ENGINEERING GROUP LLC

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC1882-AK

STATE OF ALASKA
49 TH
Cait M. Singleton
CE-124820
REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

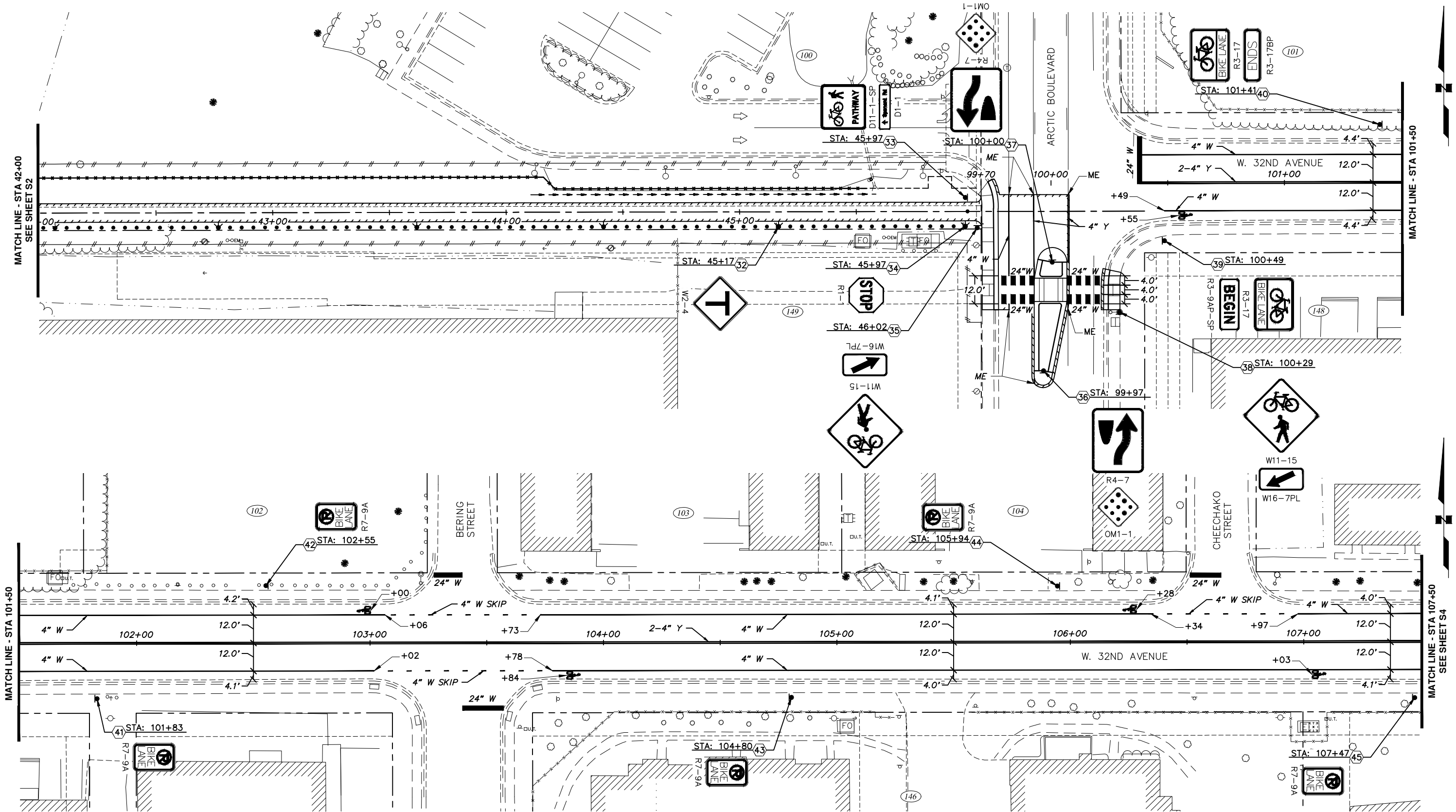
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SIGNING & STRIPING PLAN

W. 30TH AVENUE STA. 21+50 TO EOP
NORTH STAR STREET BOP TO STA. 35+75 / PATHWAY BOP TO STA. 42+00

SCALE HOR. 1"=20' VER. N/A GRID SW1629, SW1630 DATE NOV 2019 STATUS 65% SHEET S2 of S7

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan Over Plan - S365 - Phase 1.dwg



RECORD DRAWING	
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ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								

CRW
ENGINEERING GROUP LLC

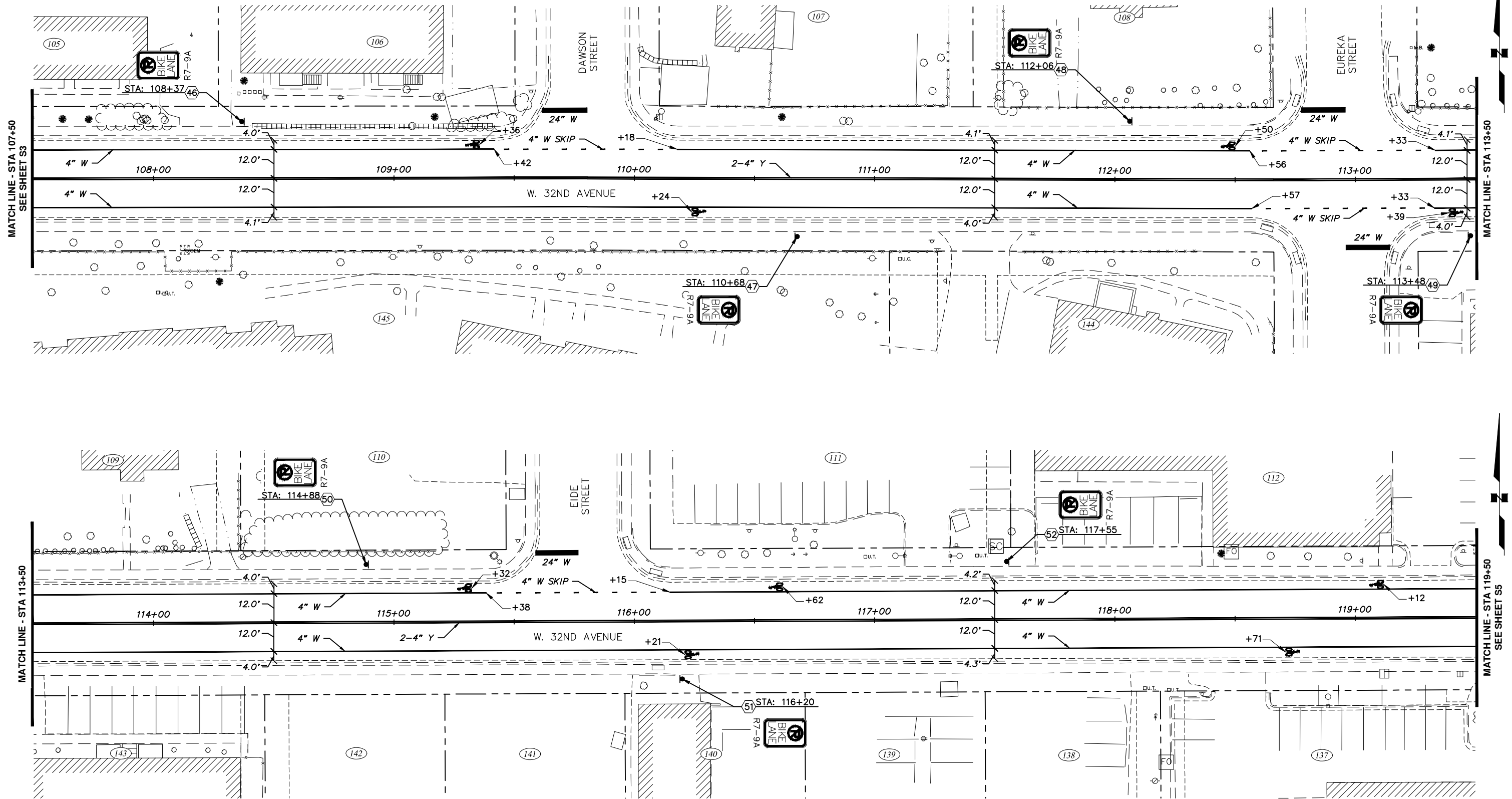
3940 ARCTIC BLVD, SUITE 300
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STATE OF ALASKA
49 TH
Colin M. Singleton
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A		SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1	
SIGNING & STRIPING PLAN			
PATHWAY STA. 42+00 TO EOP			
W. 32ND AVENUE BOP TO STA. 107+50			
SCALE HOR. 1"=20'	GRID SW629, SW630	DATE NOV 2019	STATUS 65%
VER. N/A			SHEET S3 of S7

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan Over Plan - S4S - Phase 1.dwg



RECORD DRAWING

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STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								
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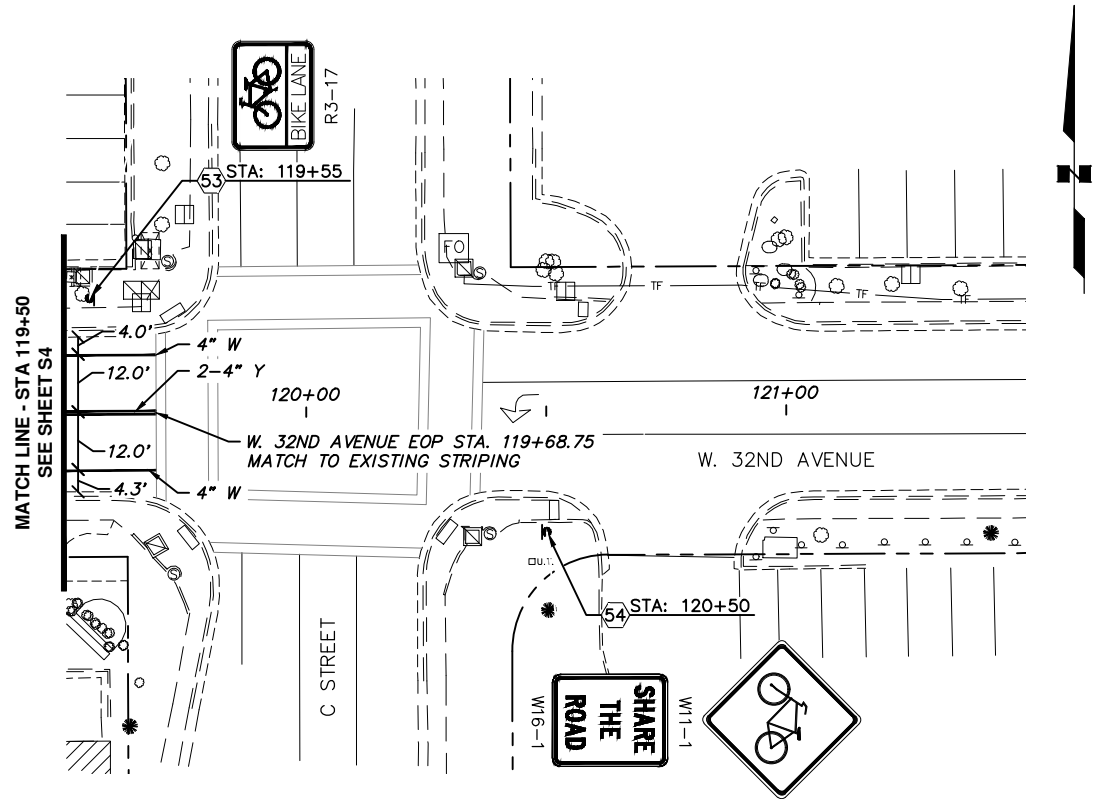
CRW ENGINEERING GROUP LLC
3940 ARCTIC BLVD. SUITE 300
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PHONE: (907) 562-3252
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49 TH
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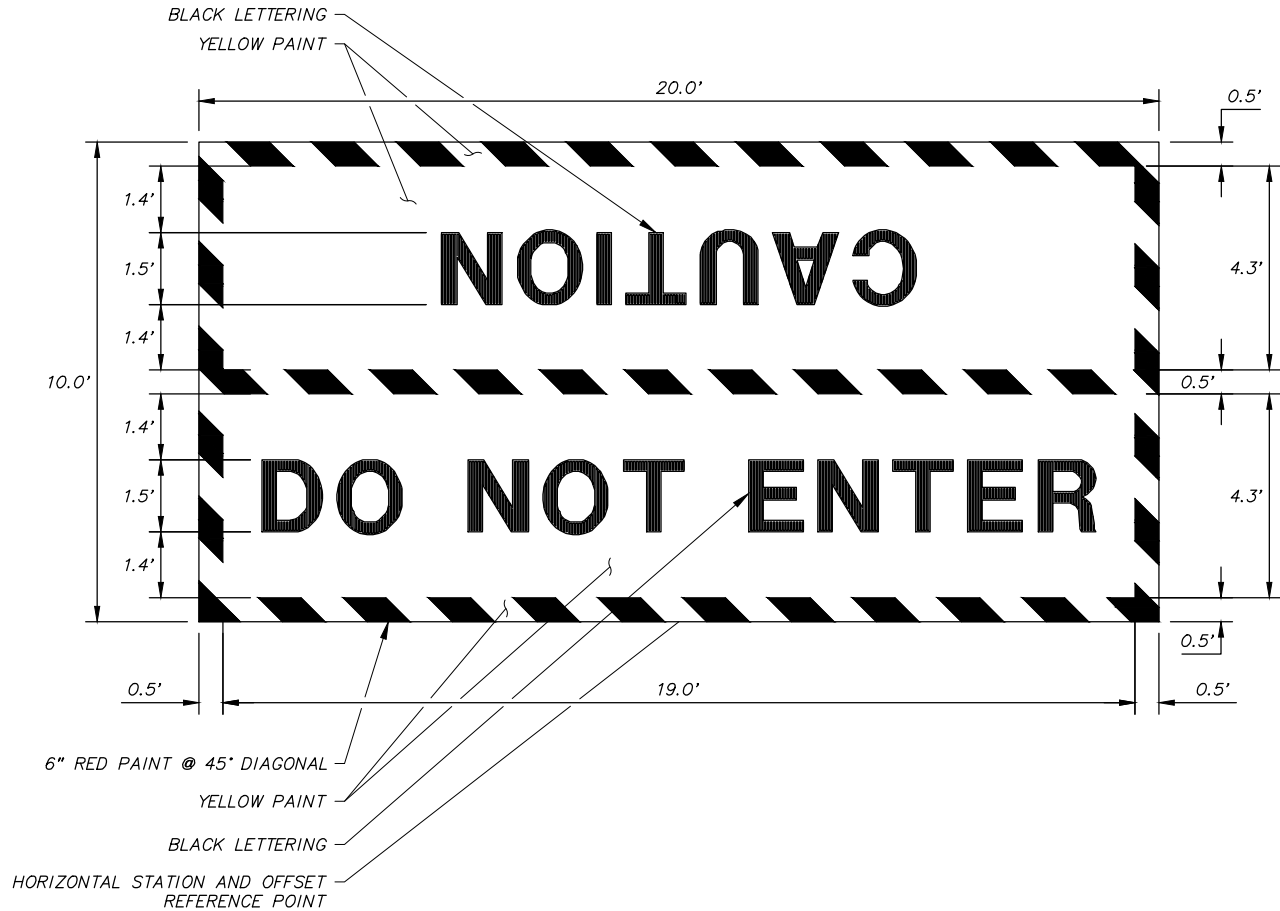
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1
SIGNING & STRIPING PLAN
STA 107+50 TO STA 119+50
SCALE HOR. 1"=20'
VER. N/A
GRID SW1629, SW1630
DATE NOV 2019
STATUS 65%
SHEET S4 of S7

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan Over Plan - S4S - Phase 1.dwg



STRIPING NOTES:

1. ALL STRIPING SHALL CONFORM TO THESE CONTRACT DOCUMENTS AND STANDARD MASS DETAILS. ALL REVISIONS SHALL CONFORM TO THE LATEST EDITION OF THE ALASKA TRAFFIC MANUAL AND THE MUTCD.
3. ALL PAVEMENT MARKINGS SHALL BE INLAID METHYLACRYLATE TO A DEPTH OF 250 MILS.
4. DIMENSIONS REFERENCE CENTER OF STRIPE TO CENTER OF STRIPE OR CENTER OF STRIPE TO EDGE OF PAVEMENT.
5. "W" REFERENCES WHITE MARKINGS AND "Y" REFERENCES YELLOW MARKINGS.
6. INSTALL STOP BARS PER MASS STANDARD DETAIL 70-18.
7. 4" WHITE DASH MARKINGS ACROSS SIDE STREETS SHALL BE 2' LONG STRIP WITH 6' GAP.



AWWU DRIVEWAY STRIPING DETAIL

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
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ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM		GAAB 1972 ADJUST						
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS		

CRW ENGINEERING GROUP LLC
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECCL882-AK

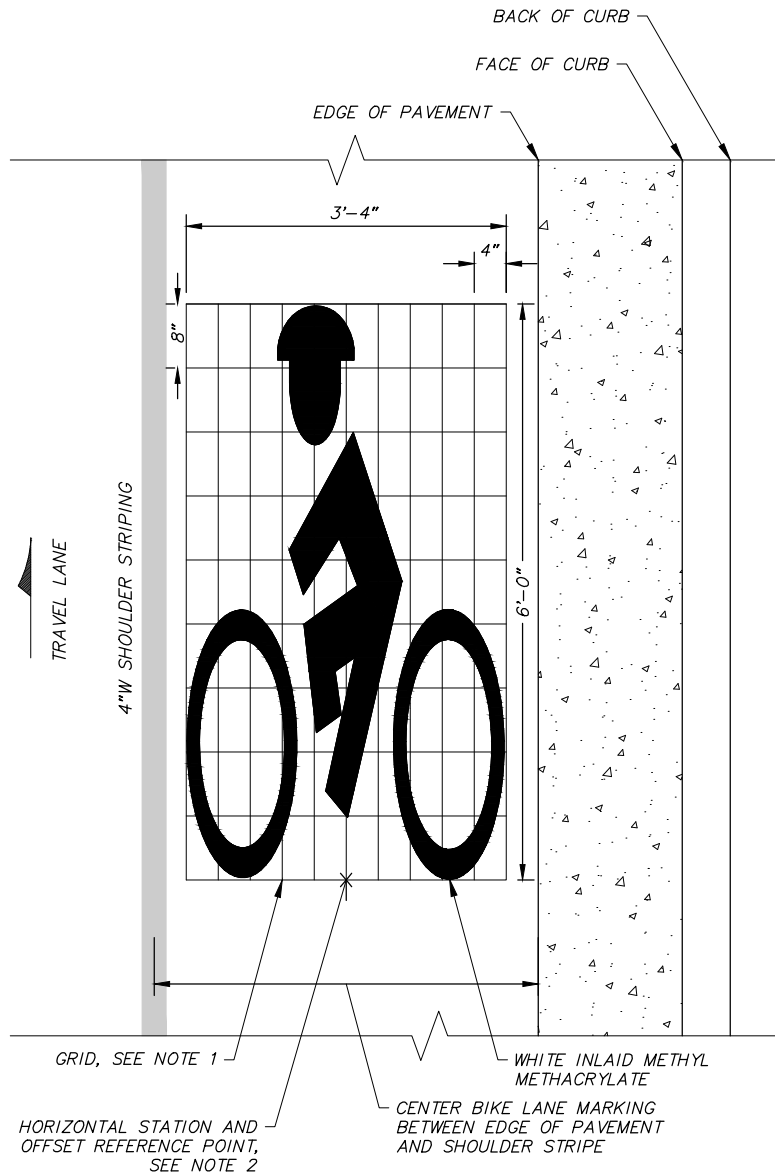
STATE OF ALASKA
49 TH
Caitin M. Singleton
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REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED A
SIGNING & STRIPING PLAN			
W. 32ND AVENUE STA 119+50 TO EOP			
SCALE	HOR. 1"=20' VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET S5 of S7

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\01 Civil\02 Design Phase 1\10138.00 Plan Over Plan - S6S - Phase 1.dwg

SIGN SCHEDULE SUMMARY											
SHEET NO.	POST NO.	STATION	OFFSET	TYPE	LEGEND	WIDTH (INCHES)	HEIGHT (INCHES)	AREA (SF)	SIGN FACES	SIGN POST	COMMENTS
S1	1	10+50.28	27.5' LT	D3-101	SPENARD RD 2900	36	8	2.00	W/E	2.5" PST	SINGLE PANEL BACK TO BACK SIGN
				D3-101	W 30TH AVE 1100	48	12	4.00	N/S		SINGLE PANEL BACK TO BACK SIGN
				R1-1	STOP	30	30	6.25	E		
	2	10+55.18	27.3' RT	R3-17	BIKE LANE	24	18	3.00	W	LIGHT POLE	
				R3-9AP-SP	BEGIN	24	8	1.33	W		
	3	11+58.85	23.4' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	LIGHT POLE	
	4	12+21.62	23.6' RT	R7-9A	NO PARKING, BIKE LANE	12	18	1.50	W	LIGHT POLE	
	5	13+00.00	23.0' RT	R2-1	20 MPH SPEED LIMIT	24	30	5.00	W	2.5" PST	
	6	13+25.00	22.0' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	2.5" PST	
	7	13+66.86	23.5' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	LIGHT POLE	
	8	15+25.00	22.0' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	2.5" PST	
	9	15+26.98	23.5' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	LIGHT POLE	
	10	16+87.82	23.5' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	LIGHT POLE	
	11	17+57.52	22.6' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	LIGHT POLE	
	12	17+79.97	23.0' RT	R3-17	BIKE LANE	24	18	3.00	W	2.5" PST	
				R3-17BP	ENDS	24	8	1.33	W		
	13	18+20.14	23.0' LT	D1-1B	SPENARD - LEFT ARROW	30	6	1.25	S	2.5" PST	
	14	18+40.61	29.1' RT	D3-101	W 30TH AVE 900	30	8	1.67	N/S	2.5" PST	SINGLE PANEL BACK TO BACK SIGN
				D3-101	NORTH STAR ST 3000	42	8	2.33	W/E		SINGLE PANEL BACK TO BACK SIGN
				R1-1	STOP	30	30	6.25	S		
S2	15	18+45.73	23.7' RT	D11-1-SP	PATHWAY	24	24	4.00	W	LIGHT POLE	
				D1-1	32ND AVE - RIGHT ARROW	24	6	1.00	W		
	16	19+20.14	20.0' LT	R3-17	BIKE LANE	24	18	3.00	E	2.5" PST	
				R3-17AP	AHEAD	24	8	1.33	E		
	17	19+95.44	24.0' RT	R8-3A	NO PARKING SYMBOL	24	24	4.00	W	LIGHT POLE	
	18	20+32.03	20.5' LT	R8-3A	NO PARKING SYMBOL	24	24	4.00	E	2.5" PST	
	19	21+32.03	20.5' LT	R2-1	20 MPH SPEED LIMIT	24	30	5.00	E	2.5" PST	
	20	22+25.00	20.0' RT	R8-3A	NO PARKING SYMBOL	24	24	4.00	W	2.5" PST	
	21	22+32.03	20.5' LT	R8-3A	NO PARKING SYMBOL	24	24	4.00	E	2.5" PST	
	22	22+83.20	23.6' RT	D3-101	ARCTIC BLVD 3000	30	8	1.67	W/E	2.5" PST	SINGLE PANEL BACK TO BACK SIGN
				D3-101	W 30TH AVE 800	48	12	4.00	N/S		SINGLE PANEL BACK TO BACK SIGN
				R1-1	STOP	30	30	6.25	W		
	23	30+19.33	14.0' RT	W1-1L	LEFT TURN	30	30	6.25	W	2.5" PST	
				W13-1P	15 MPH ADVISORY SPEED	18	18	2.25	W		
	24	30+75.00	14.0' RT	R8-3A	NO PARKING SYMBOL	24	24	4.00	W	2.5" PST	
	25	31+52.29	15.6' RT	W1-6L	LEFT ARROW	48	24	8.00	W	2.5" PST	
	26	31+60.93	16.0' RT	D3-101	W 32ND AVE 1000	36	8	2.00	N/S	2.5" PST	SINGLE PANEL BACK TO BACK SIGN
				D3-101	NORTH STAR ST 3100	42	8	2.33	W/E		SINGLE PANEL BACK TO BACK SIGN
	27	31+69.57	15.5' RT	W1-6R	RIGHT ARROW	48	24	8.00	N	2.5" PST	
	28	33+19.10	15.5' LT	W1-1R	RIGHT TURN	30	30	6.25	N	2.5" PST	
				W13-1P	15 MPH ADVISORY SPEED	18	18	2.25	N		
	29	33+28.40	22.5' RT	R8-3A	NO PARKING SYMBOL	24	24	4.00	S	LIGHT POLE	
	30	34+16.00	15.0' LT	R8-3A	NO PARKING SYMBOL	24	24	4.00	N	2.5" PST	
	31	41+02.29	7.0' LT	D11-1-SP	PATHWAY	24	24	4.00	N	2.5" PST	
				D1-1	32ND AVE - LEFT ARROW	24	6	1.00	N		
				D11-1-SP	PATHWAY	24	24	4.00	S		
				D1-1	SPENARD RD - STRAIGHT ARROW	24	6	1.00	S		



BIKE LANE MARKING

MARKING NOTES:

- GRID IS SHOWN FOR MARKING STENCIL LAYOUT, DO NOT INCLUDE GRID WHILE INSTALLING BIKE LANE MARKINGS.
- SEE STRIPING PLANS FOR APPROXIMATE LOCATIONS AND ORIENTATION OF BIKE LANE MARKINGS.

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____

BY: _____ TITLE: _____ DATE: _____

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COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	TS	MJ
TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
GAS	RK	JK
TELEPHONE	RK	JK
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196		CB 7D	See MOA Benchmark Book, Page D-56	94.77				
STAKING		CB 7C	See MOA Benchmark Book, Page D-17	106.10				
		GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM		GAAB 1972 ADJUST						
PLAN CHECK		CONSTRUCTION RECORD						
		VERTICAL DATUM						
		REVISIONS						
		CONSULTANT						



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

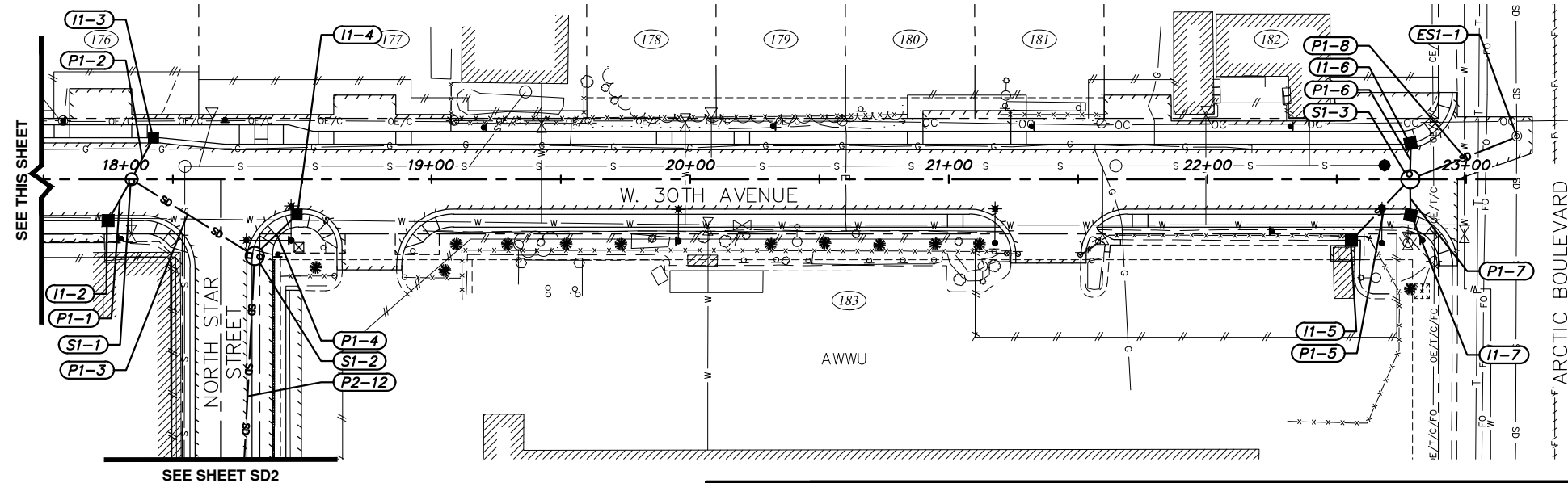
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SIGN SCHEDULE SUMMARY

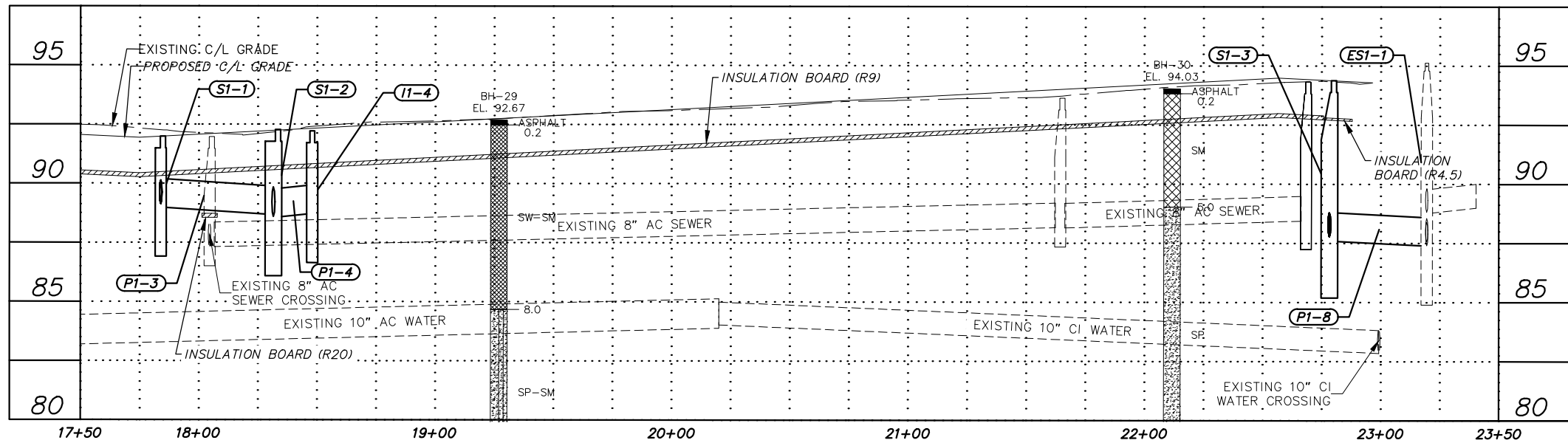
SCALE HOR. N/A VER. N/A

GRID SW1629, SW1630



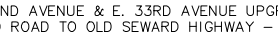
DATE NOV 2019 STATUS 65% SHEET S6 of S7



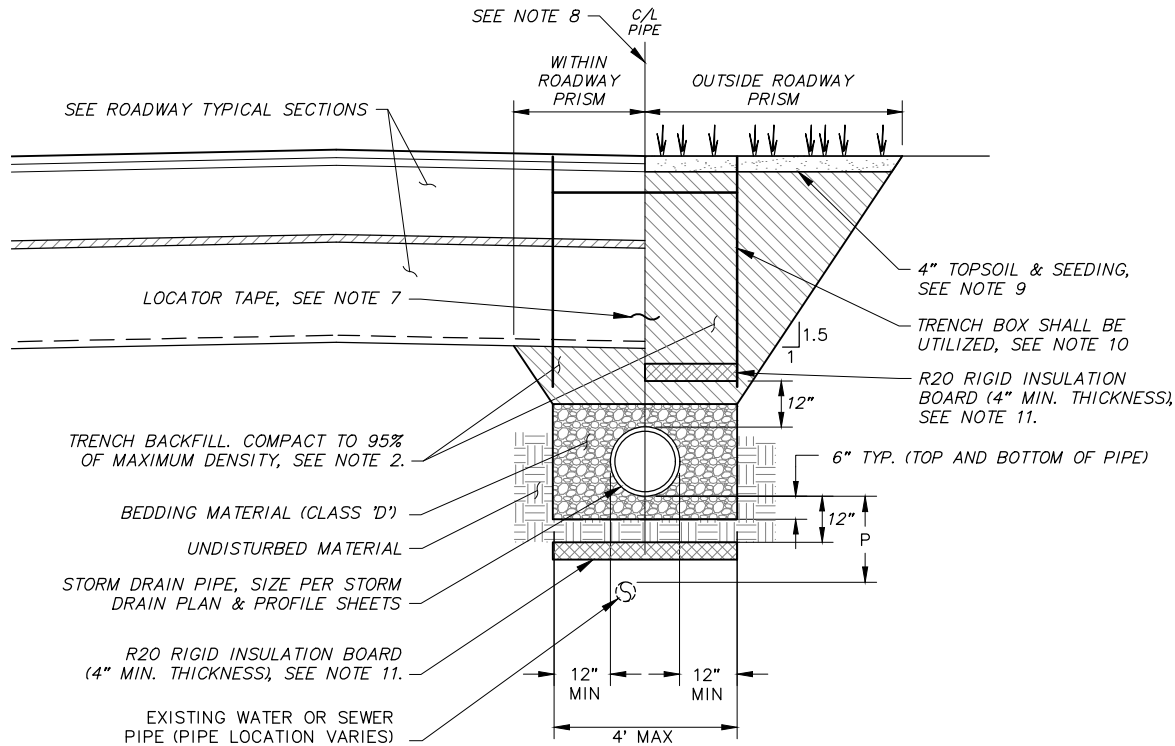
55.04, 55.05 & 55.09 – STORM DRAIN STRUCTURES							
STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
I1-1	CB/CONNECT	CI	10+41.74	18.61' RT	95.61	1	CONNECT EXISTING PIPE EP1-1
I1-2	CB (RED)	CI	17+75.00	16.00' RT	91.92	1	
S1-1	MH I (RED)	MH	17+83.99	CL	91.98	N/A	
I1-3	CB (RED)	CI	17+92.50	16.00' LT	91.73	1	
S1-2	MH II (RED)	MH	18+31.63	29.75' RT	92.27	1	
I1-4	CB (RED)	CI	18+48.05	13.50' RT	92.49	1	
I1-5	CB	CI	22+55.63	23.60' RT	95.20	1	
S1-3	MH II	MH	22+78.37	CL	94.38	N/A	
I1-7	CB	CI	22+78.54	13.87' RT	94.27	1	
I1-6	CB	CI	22+78.56	13.90' LT	94.20	1	
ES1-1	CONNECT	MH	23+19.56	16.63' LT	95.13	N/A	EXISTING MH



1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARITY.
2. REFER TO SHEET SD4 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
3. REFER TO SHEETS SD4—SD6 FOR STORM DRAIN DETAILS.

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ BY: _____ TITLE: _____ DATE: _____			<table border="1"> <tr> <td>DATA</td> <td>DRAWN BY</td> <td>CHECKED BY</td> <td colspan="8"> GRAPHIC <div style="display: inline-block; width: 150px; height: 10px; background: linear-gradient(to right, black 25%, white 25% 50%, white 50% 75%, black 75%); border: 1px solid black;"></div> SCALE </td> </tr> <tr> <td>BASE TOPOGRAPHY</td> <td>TS BW</td> <td>MJ BW</td> <td colspan="2">FIELD BOOKS</td> <td>BM NO.</td> <td>LOCATION</td> <td>ELEV.</td> <td>REV</td> <td>DATE</td> <td>DESCRIPTION</td> <td>BY</td> </tr> <tr> <td>STORM SEWER</td> <td>MV JH</td> <td>JH</td> <td>DESIGN CRW BOOK NO. 149, 169,</td> <td>CB 7D</td> <td>See MOA Benchmark Book, Page D-56</td> <td>94.77</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>RK JK</td> <td>JK</td> <td>105 & 196</td> <td>CB 7C</td> <td>See MOA Benchmark Book, Page D-17</td> <td>106.10</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>GAS</td> <td>RK JK</td> <td>JK</td> <td>STAKING</td> <td>GAAB 8B</td> <td>See MOA Benchmark Book, Page D-18</td> <td>104.53</td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>TELEPHONE</td> <td>RK JK</td> <td>JK</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>ELECTRIC</td> <td>JH TK</td> <td>TK</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>DESIGN</td> <td>RB JK</td> <td>JK</td> <td>ASBUILT</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>QUANTITIES</td> <td>RB JK</td> <td>JK</td> <td>CONTRACTOR</td> <td colspan="8">BASIS OF THIS DATUM GAAB 1972 ADJUST</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB JK</td> <td>JK</td> <td>INSPECTOR</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB JK</td> <td>JK</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td colspan="3">PLAN CHECK</td> <td colspan="3">CONSTRUCTION RECORD</td> <td colspan="3">VERTICAL DATUM</td> <td colspan="3">REVISIONS</td> </tr> </table>										DATA	DRAWN BY	CHECKED BY	GRAPHIC <div style="display: inline-block; width: 150px; height: 10px; background: linear-gradient(to right, black 25%, white 25% 50%, white 50% 75%, black 75%); border: 1px solid black;"></div> SCALE								BASE TOPOGRAPHY	TS BW	MJ BW	FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	STORM SEWER	MV JH	JH	DESIGN CRW BOOK NO. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77						WATER/SANITARY SEWER	RK JK	JK	105 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10						GAS	RK JK	JK	STAKING	GAAB 8B	See MOA Benchmark Book, Page D-18	104.53						TELEPHONE	RK JK	JK										ELECTRIC	JH TK	TK										DESIGN	RB JK	JK	ASBUILT									QUANTITIES	RB JK	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST								PRELIMINARY/FINAL	RB JK	JK	INSPECTOR									MUNICIPAL/STATE	RB JK	JK										PLAN CHECK			CONSTRUCTION RECORD			VERTICAL DATUM			REVISIONS			 3940 ARCTIC BLVD., SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECCL882-AK					
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1 **TYPICAL STORM DRAIN TRENCH SECTION**
SCALE: NTS

STORM DRAIN & SUBDRAIN TRENCH SECTION NOTES:

- TRENCH EXCAVATION AND SHORING SHALL COMPLY WITH ALL LOCAL, STATE, AND OSHA REGULATIONS AND REQUIREMENTS. INDICATED TRENCH WALL SLOPES AND DIMENSIONS ARE FOR PAY QUANTITY DETERMINATIONS ONLY.
- TRENCH BACKFILL SHALL BE NATIVE MATERIAL MEETING TYPE III CLASSIFICATION (MINIMUM) AS APPROVED BY THE ENGINEER. NATIVE MATERIAL NOT MEETING TYPE III CLASSIFICATION SHALL BE REMOVED AND REPLACED WITH TYPE II CLASSIFIED MATERIAL.
- REMOVE AND DISPOSE OF ALL ORGANIC MATERIALS IN ACCORDANCE WITH MASS SECTION 20.13.
- IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, CONTRACTOR SHALL SAW CUT AND REMOVE AN ADDITIONAL 12 INCHES FROM EXISTING PAVEMENT EDGE. THE ENGINEER MAY REQUIRE MORE THAN 12 INCHES ADDITIONAL CUT IF THE EXISTING PAVEMENT HAS BEEN LIFTED IN THE REMOVAL PROCESS, IF THE JOINT DOES NOT OCCUR ON UNDISTURBED MATERIAL, OR IF THE JOINT IS LOCATED WITHIN THE TRAVEL LANE.
- WATER LINES CROSSING STORM DRAIN LINES REQUIRE A MINIMUM VERTICAL SEPARATION OF THREE (3) FEET. INSTALL R20 INSULATION BOARD WHEN 'P' IS LESS THAN 3', AS MEASURED FROM OUTSIDE OF PIPES & WITHIN BEDDING LIMITS, OR AS DIRECTED BY ENGINEER IN FIELD. EIGHTEEN (18) INCHES IS THE MINIMUM INSULATED SEPARATION DISTANCE. IF EIGHTEEN (18) INCHES CAN NOT BE OBTAINED, THE WATER LINE WILL HAVE TO BE RELOCATED.
- WHERE WATER AND STORM DRAIN MAINS CROSS, STORM DRAIN MAIN JOINTS SHALL BE AT LEAST 10 FEET FROM WATER MAIN JOINTS.
- INSTALL DETECTABLE LOCATOR TAPE THREE (3) FEET BELOW FINISH GRADE OR TWO (2) FEET DEEP IN THE STREET STRUCTURAL SECTION PER MASS SECTION 20.13.
- LOCATION OF STORM DRAIN VARIES WITHIN ROADWAY. INSTALL STORM DRAIN AS SHOWN ON STORM DRAIN PLAN & PROFILE SHEETS.
- PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
- TRENCH BOX SHALL BE UTILIZED TO MINIMIZE TRENCH WIDTH AND REDUCE IMPACTS TO ADJACENT PROPERTIES AND RE-VEGETATION. CONTRACTOR SHALL TAKE EXTRA PRECAUTIONS TO AVOID IMPACTS TO TREE PROTECTION ZONES.
- INSTALL R20 INSULATION BOARD:
 - ABOVE SD PIPE WHEN COVER IS LESS THAN 4' IN AREAS OUTSIDE OF INSULATED ROADWAY SECTION, INSULATION PLACEMENT SHALL CONFORM TO MASS DETAIL 20-9.
 - BELOW SD PIPE WHEN 'P' IS LESS THAN 3', AS MEASURED FROM OUTSIDE OF PIPES & WITHIN BEDDING LIMITS, OR AS DIRECTED BY ENGINEER IN THE FIELD.

GENERAL STORM DRAIN STRUCTURE & PIPE NOTES:

- HORIZONTAL AND VERTICAL CONTROL POINTS FOR STORM DRAIN STRUCTURES (REFERENCE POINTS CALLED OUT IN PLAN & PROFILE SHEETS) ARE:

STRUCTURE	HORZ CONTROL	REFERENCE ELEV.
TYPE I MH	CENTER OF MH	FG/TOP OF LID.
TYPE II MH	CENTER OF MH	FG/TOP OF LID.
TYPE II CBMH	CENTER OF MH	TBC @ MID. PT. OF CURB INLET HOOD
CATCH BASIN	CENTER OF CB	TBC @ MID. PT. OF CURB INLET HOOD
CB W/ FIELD INLET	CENTER OF CB	FG/TOP OF FRAME
TYPE I CBMH W/FIELD INLET	CENTER OF MH	FG/TOP OF FRAME
- PIPE LENGTHS ARE BASED ON THE HORIZONTAL DISTANCE BETWEEN THE CENTER OF CONNECTING STRUCTURES OR FITTINGS. PIPE SLOPES ARE CALCULATED USING THE ACTUAL LENGTH OF PIPE FROM THE INSIDE FACE OF STRUCTURES.
- UNLESS OTHERWISE NOTED ALL STORM DRAIN MAIN PIPE SHALL BE CPEP, TYPE S.
- THE FOLLOWING ABBREVIATIONS USED ON THE STORM DRAIN STRUCTURE TABLES ON THE PLAN & PROFILES SHEETS ARE DESCRIBED BELOW:
 - CB - CATCH BASIN
 - CB (RED) - REDUCED HEIGHT CATCH BASIN
 - CB MH I - CATCH BASIN MANHOLE, TYPE I
 - CB MH II - CATCH BASIN MANHOLE, TYPE II
 - MH I - STORM DRAIN MANHOLE, TYPE I
 - MH II - STORM DRAIN MANHOLE, TYPE II
 - OGS - OIL AND GRIT SEPARATOR
 - CONNECT - CONNECT TO STORM DRAIN MANHOLE
 - C.I. - CURB INLET
 - MH - MANHOLE FRAME AND LID

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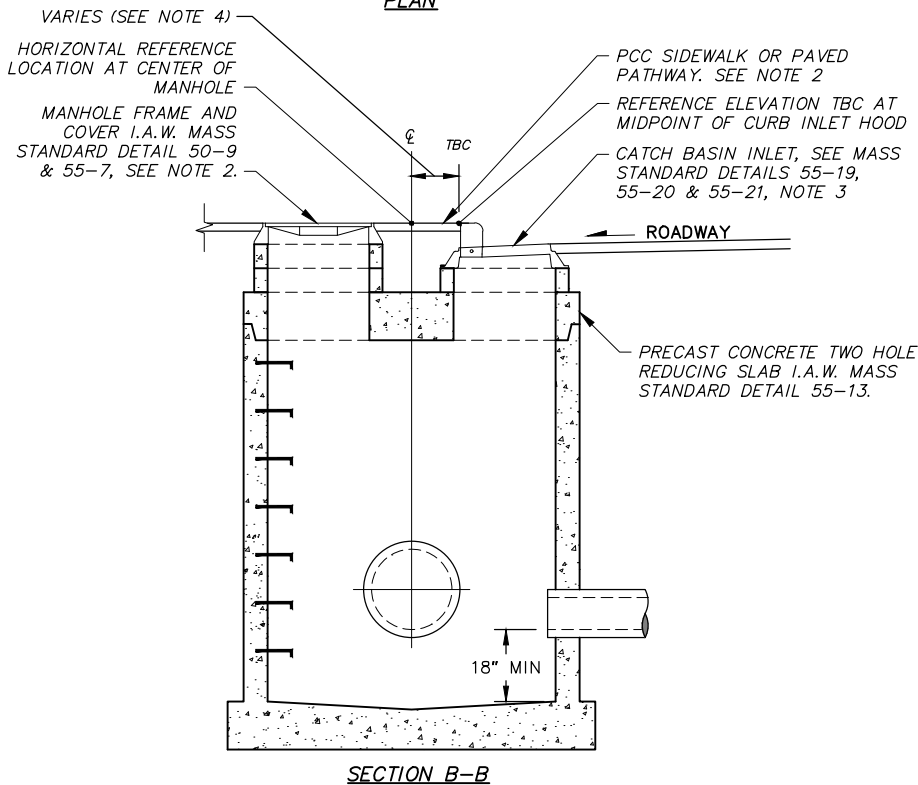
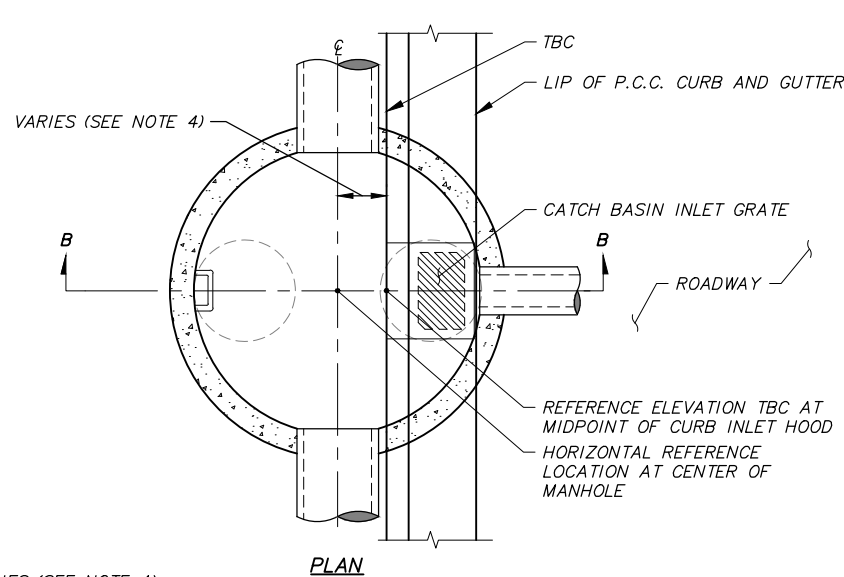
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BASIS OF THIS DATUM GAAB 1972 ADJUST



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED B
STORM DRAIN DETAILS			
SCALE	HOR. NTS VER. NTS	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65% SHEET
			SD4 of SD6

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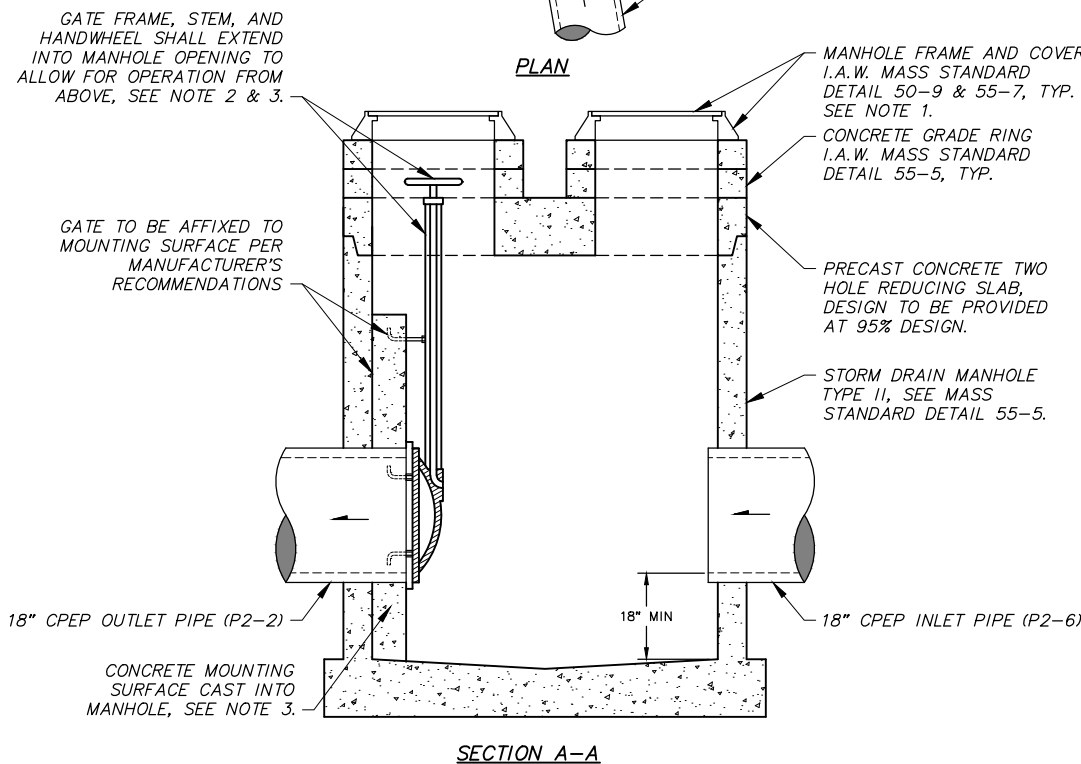
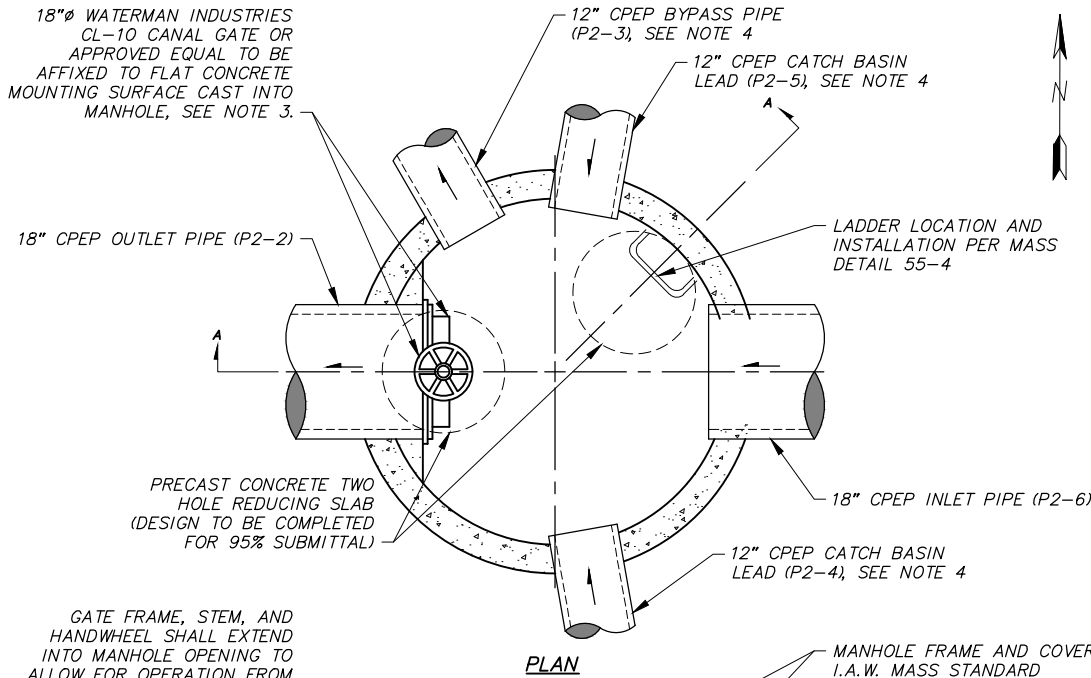
TYPE II CATCH BASIN MANHOLE NOTES

- ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE 2015 MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS AS CURRENTLY AMENDED AND AS MODIFIED ON THIS DETAIL.
- SET MANHOLE COVER 1/4-INCH BELOW PCC SIDEWALK OR PAVED PATHWAY FINISH GRADE OR PER MASS STANDARD DETAIL 55-10 FOR ALL OTHER LOCATIONS.
- MH CENTER MAY BE ON ROADWAY SIDE OF CURB LINE IN SOME LOCATIONS. ALIGN CATCH BASIN INLET WITH CURB LINE.
- OFFSET FOR STANDARD INSTALLATION IS 0.95'.

1

TYPE II CATCH BASIN MANHOLE DETAIL

SCALE: NTS



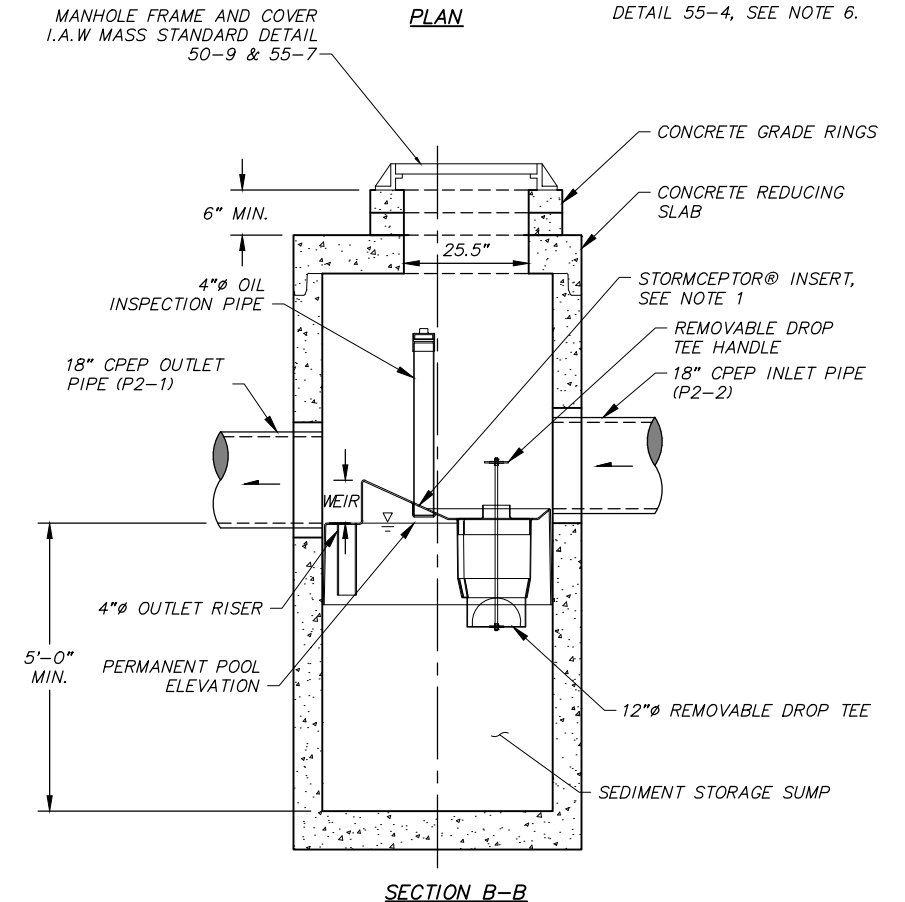
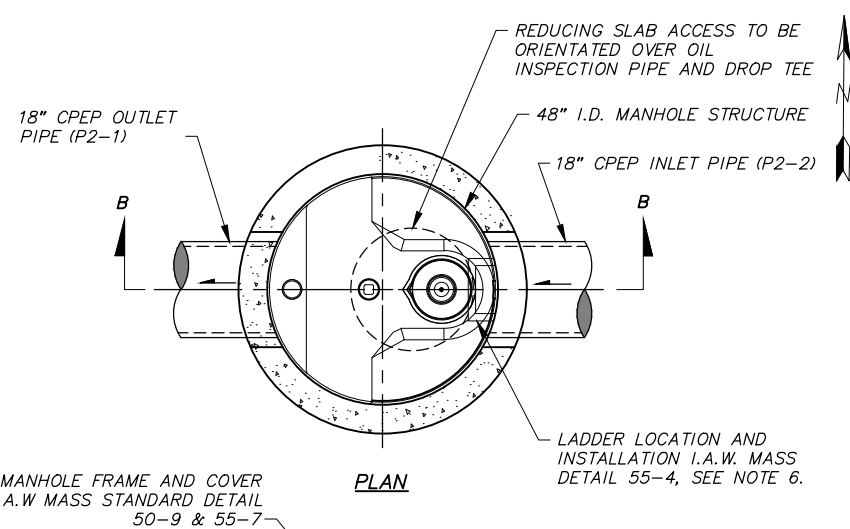
BYPASS MANHOLE NOTES

- CAST CONCRETE MOUNTING SURFACE INTO MANHOLE SUCH THAT BYPASS GATE HANDWHEEL IS CENTERED IN ACCESS OPENING.
- BYPASS GATE STEM SHALL BE NON-RISING TO POSITION HANDWHEEL AT CONVENIENT STATIC OPERATING ELEVATION FROM MANHOLE OPENING ABOVE.
- BYPASS MANHOLE (STRUCTURE S2-2) SHALL BE PAID FOR UNDER PAY ITEM 55.05 CONSTRUCT (TYPE II) BYPASS MANHOLE.
- CATCHBASIN LEADS (P2-4 & P2-5) AND BYPASS PIPE (P2-3) NOT SHOWN IN SECTION A-A FOR CLARITY.

2

BYPASS MANHOLE (S2-2) DETAIL

SCALE: NTS



OIL & GRIT SEPARATOR NOTES

- OIL AND GRIT SEPARATOR (STRUCTURE OGS2-1) SHALL BE STORMCEPTOR MODEL STC4501 MANUFACTURED BY CONTECH ENGINEERED SOLUTIONS LLC OR APPROVED EQUAL.
- ACCESS OPENING THROUGH REDUCING SLAB SHOULD BE POSITIONED OVER THE DROP TEE AND OIL PORT.
- SEE STORM DRAIN PLAN & PROFILE SHEETS FOR INLET AND OUTLET PIPE INVERTS & ORIENTATION AND STRUCTURE INFORMATION.
- LADDER RUNGS NOT SHOWN IN SECTION VIEW FOR CLARITY.

3

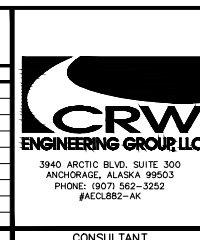
OIL AND GRIT SEPARATOR (OGS2-1)

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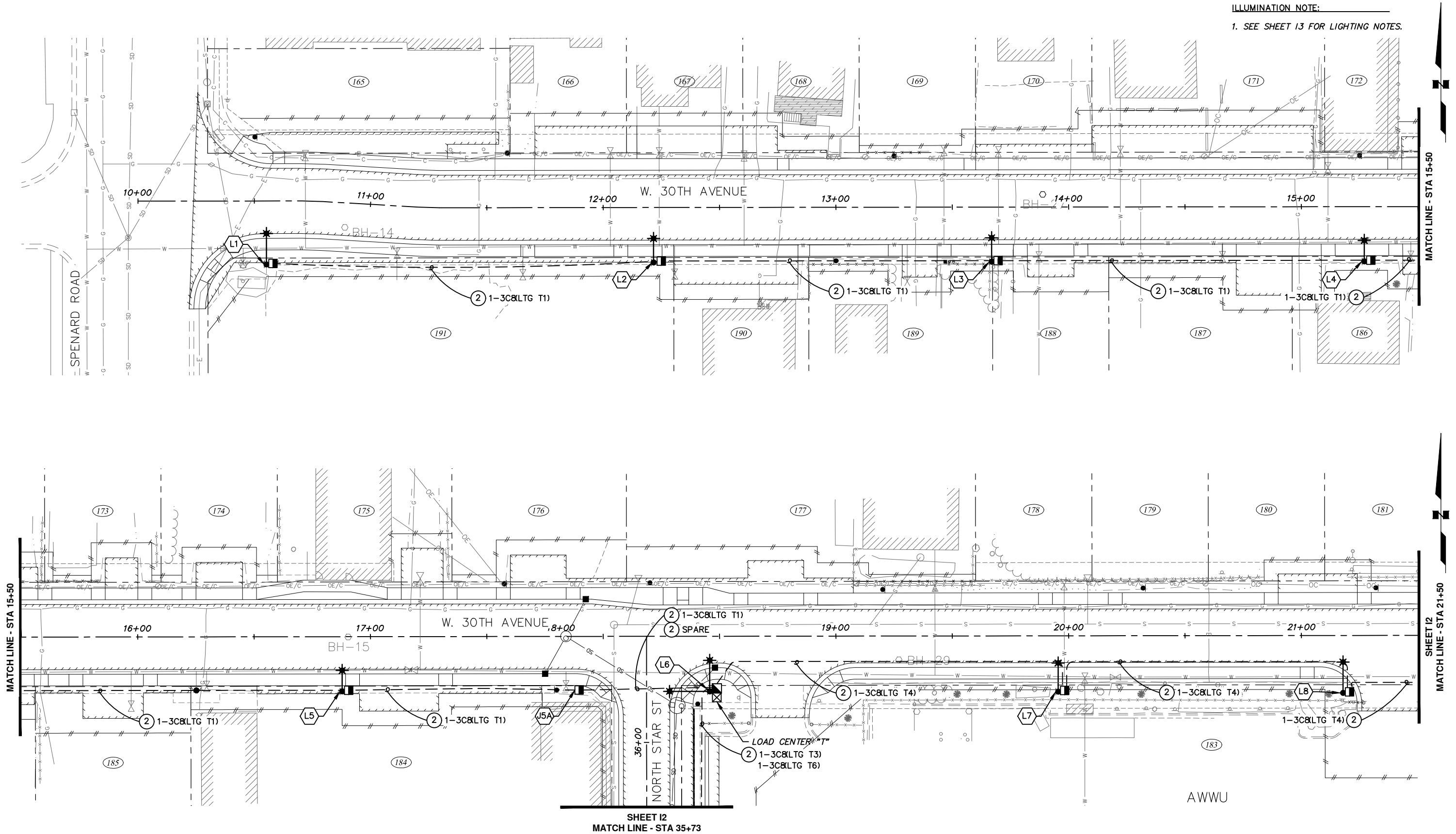
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SCALE	HOR. NTS	GRID SW1629, SW1630	SD5 of SD6
	VER. NTS	DATE NOV 2019	STATUS 65% SHEET

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\03 Electrical\Design Phase 1\10138.00 Illumination - Roadway - Phase 1.dwg



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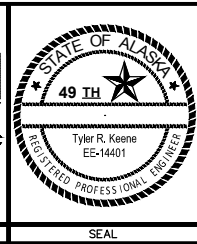
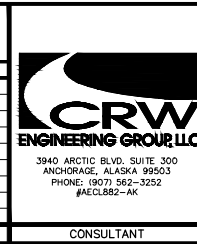
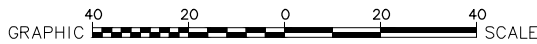
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TOPOGRAPHY	BW	BW
PROFILE	RB	JK
STORM SEWER	MV	JH
WATER/SANITARY SEWER	RK	JK
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DESIGN	RB	JK
QUANTITIES	RB	JK
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MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 149, 169, 195 & 196	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
STAKING	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM GAAB 1972 ADJUST							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED C
NORTH STAR STREET: STA 35+73 TO STA 36+46

ILLUMINATION PLAN

W. 30TH AVE: BOP TO STA 21+50
NORTH STAR STREET: STA 35+73 TO STA 36+46

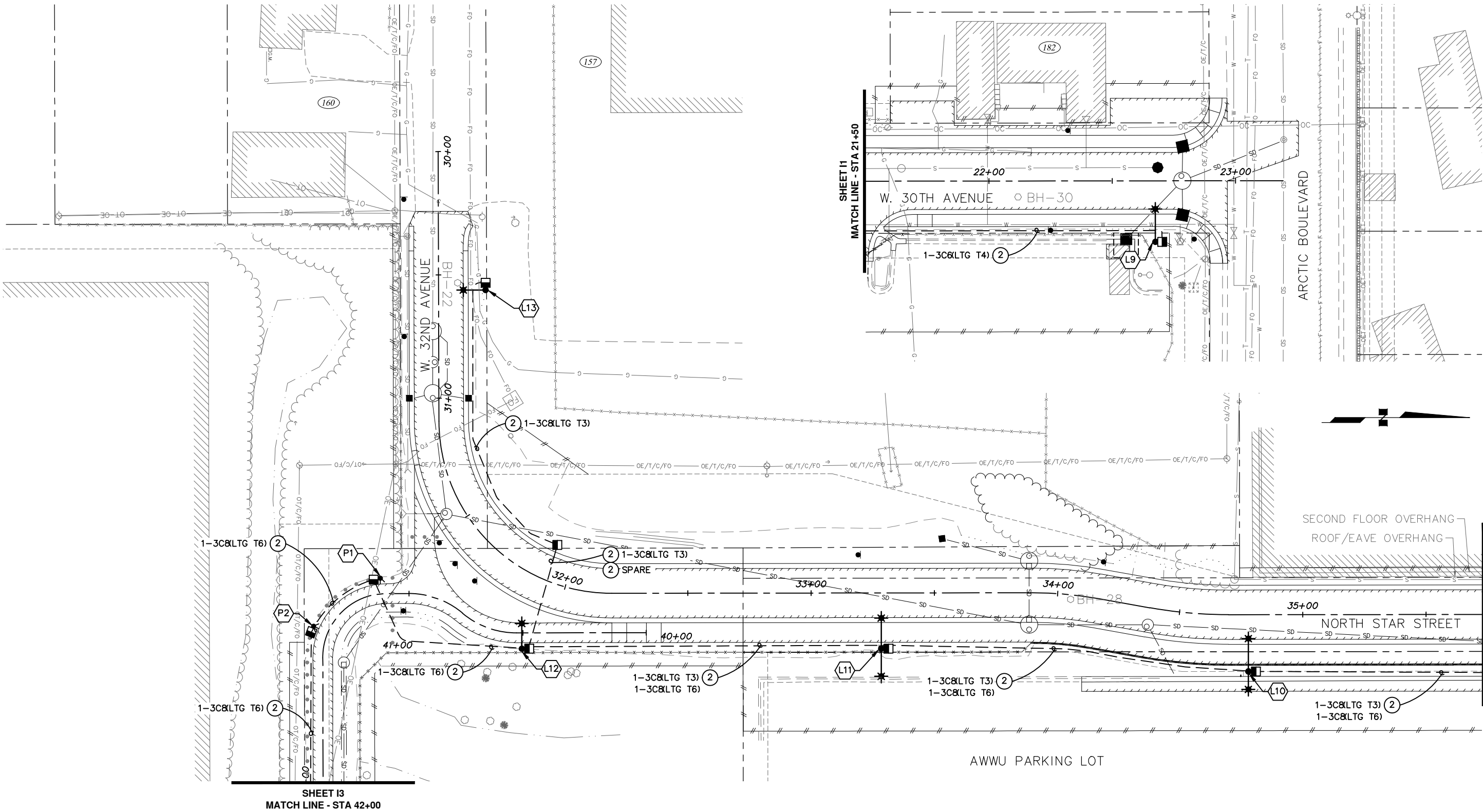
SCALE HOR. 1"=20'
VER. N/A

GRID SW1629, SW1630

DATE NOV 2019 STATUS 65% SHEET 11 of 16

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\03 Electrical Design Phase 1\10138.00 Illumination - Roadway - Phase 1.dwg

ILLUMINATION NOTE:
1. SEE SHEET 13 FOR LIGHTING NOTES.



RECORD DRAWING

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CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM GAAB 1972 ADJUST								
REVISIONS								
CONSULTANT								
SEAL								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

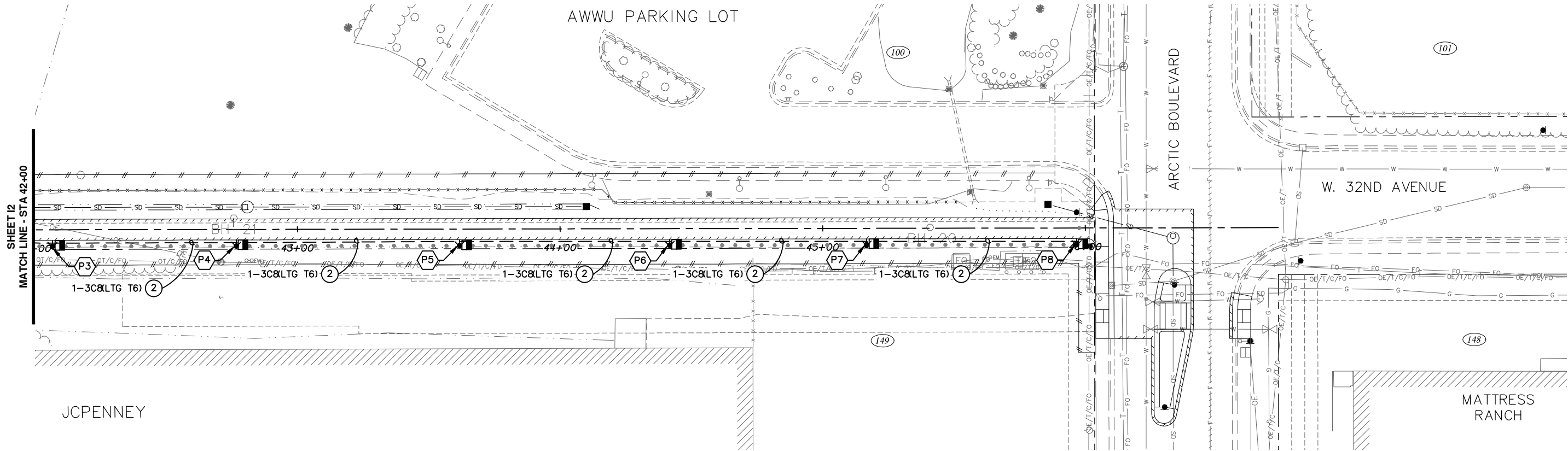
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED C
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

ILLUMINATION PLAN
W. 30TH AVE: STA 21+50 TO EOP
NORTH STAR STREET: STA 30+00 TO STA 35+73
PATHWAY: STA 40+00 TO STA 42+00

SCALE HOR. 1"=20'
VER. N/A

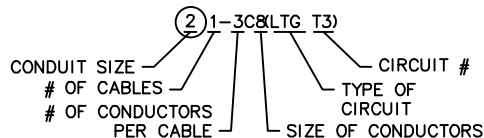
GRID SW1629, SW1630
DATE NOV 2019 STATUS 65% SHEET 12 of 16

File: s:_data\10138.00 W 32nd And E 33rd Avenue Upgrade\00 CADD\01 Working Set\03 Electrical\Design Phase 1\10138.00 Illumination - Roadway - Phase 1.dwg



ILLUMINATION NOTES:

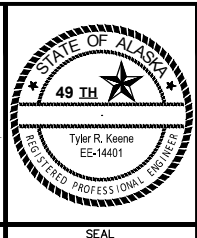
1. PROVIDE HOT DIP GALVANIZED STEEL POLES WITH MAST ARMS PER MOA STANDARDS DETAIL 80-19 AND 80-20, RESPECTIVELY.
2. ALL LUMINAIRE POLE FOUNDATIONS SHALL BE DRIVEN PILE UNLESS OTHERWISE NOTED ON THE DRAWINGS. DUE TO LIMITED ROW, LUMINAIRE POLE FOUNDATION SHALL BE LOCATED A MINIMUM OF 6 INCHES INSIDE OF ROW. WHEN POLE LOCATION IS WITHIN 10' OF A UTILITY, EXCAVATE A HOLE TO 12" BELOW ANTICIPATED UTILITIES DEPTH WITH A VACTOR TRUCK BEFORE DRIVING PILE. SEE MASS DETAIL 80-13.
3. INSTALL THE POLES WITH FIXED BASES PER MOA DETAIL 80-21.
4. LUMINAIRES APPROVED FOR SUBSTITUTION SHALL PROVIDE THE LIGHT LEVELS AND UNIFORMITIES INDICATED IN THE LIGHT LEVELS TABLE.
5. PROVIDE THE POLE SHAFT LENGTHS AND MAST ARM LENGTHS SHOWN IN THE POLE SCHEDULE.
6. PROVIDE RIGID METAL CONDUIT (RMC) WITH A BARE, STRANDED #8 AWG COPPER GROUND FOR ALL RACEWAYS.
7. PROVIDE ONE SPARE 2" RMC WITH PULL ROPE BETWEEN THE JUNCTION BOXES ADJACENT TO EVERY ROAD CROSSING.
8. PROVIDE A 3 CONDUCTOR CABLE FOR EACH BRANCH CIRCUIT. SIZE AS SHOWN ON THE DRAWINGS.
9. INSTALL THE JUNCTION BOX WITHIN 3' OF THE POLE OR LOAD CENTER. DO NOT INSTALL JUNCTION BOXES IN SIDEWALKS, PATHWAYS OR TRAILS. JUNCTION BOXES INSTALLED BEHIND SIDEWALKS, PATHWAYS OR TRAILS SHALL HAVE A MINIMUM SETBACK OF 2' AND BE PLACED BEHIND OR ON THE DOWN TRAFFIC SIDE OF FOUNDATIONS.
10. IN THE DRAWINGS, EACH JUNCTION BOX HAS THE SAME IDENTIFYING NUMBER AS THE LIGHT POLE OR LOAD CENTER NEXT TO IT. FOR JUNCTION BOXES LOCATED BETWEEN POLES, THE IDENTIFYING NUMBER INCLUDES THE SMALLER OF THE TWO POLE NUMBERS BETWEEN WHICH THE JUNCTION BOX IS LOCATED AND AN "A" SUFFIX.
11. INSTALL PEDESTRIAN LIGHT COLUMNS ON CONCRETE FOUNDATIONS PER DETAILS ON SHEET 16.
12. COORDINATE WITH CEA FOR REMOVAL OF UTILITY OWNED LUMINAIRES WITHIN THE PROJECT AREA.





RECORD DRAWING	
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BY: _____	TITLE: _____ DATE: _____
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ASBUILT								
CONTRACTOR								
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BASIS OF THIS DATUM GAAB 1972 ADJUST								



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
16-29	W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1		SCHED C
ILLUMINATION PLAN			
PATHWAY: STA 42+00 - EOP			
SCALE HOR. 1"=20' VER. N/A	GRID SW1629, SW1630 DATE NOV 2019	STATUS 65%	SHEET 13 of 16

LUMINAIRE SUMMARY											
TYPE	SYMBOL	MAKE	MODEL	LAMP	CCT*	DISTRIBUTION	VOLTAGE	DRIVE CURRENT	COLOR	OPTIONS	MOUNT
ROADWAY		CREE	LEDWAY	SEE ROADWAY LUMINAIRE SCHEDULE	4000K	SEE ROADWAY LUMINAIRE SCHEDULE	240	525mA	SILVER	SEE ROADWAY LUMINAIRE SCHEDULE	MAST ARM
PEDESTRIAN LIGHT COLUMN		PHILIPS LUMEC	ULLC100	32 LED	4000K	MEDIUM, TYPE 3	240	SEE PEDESTRIAN LIGHT COLUMN SCHEDULE	RAL XXXX	7-PIN RECEPTACLE, UPLIGHT DEFLECTOR	—

*CCT = CORRELATED COLOR TEMPERATURE

LIGHT LEVELS TABLE						
LOCATION	MOA REQUIRED MIN. AVERAGE ILLUMINANCE (FC)	AVERAGE DESIGN ILLUMINANCE (FC)	MOA REQUIRED MAXIMUM UNIFORMITY RATIO	DESIGN UNIFORMITY RATIO	MOA REQUIRED MAX. VEILING LUMINANCE RATIO	DESIGN VEILING LUMINANCE RATIO
30TH AVENUE	0.7	0.9	6.0:1	2.9:1	0.4:1	0.4:1
NORTH STAR STREET	0.7	0.9	6.0:1	3.1:1	—	—
TRAIL — ARCTIC BOULEVARD to NORTH STAR STREET	0.5	1.4	10:1	7.0:1	—	—
30TH AVENUE/NORTH STAR STREET INTX	1.4	1.4	6.0:1	1.6:1	—	—

NOTES:

1. MOA REQUIREMENTS ARE FROM 2007 DCM CHAPTER 5 FOR A LOCAL ROADWAY WITH MEDIUM PEDESTRIAN CONFLICT (MEDIUM DENSITY RESIDENTIAL).
2. LIGHT LOSS FACTOR (LLF) = 0.93 FOR ROADWAY LUMINAIRES, 0.882 FOR PATHWAY LUMINAIRES.
3. MOUNTING HEIGHTS ARE 30' FOR ROADWAY LUMINAIRES, 12' FOR PEDESTRIAN LIGHT COLUMN.

ROADWAY LUMINAIRE SCHEDULE								
POLE	STATION	OFFSET	SHAFT LENGTH	MAST ARM LENGTH	# OF LED	DISTRIBUTION	OPTIONS	CIRCUIT
L1	10+55.2	27.29 RT	26'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1
L2	12+21.6	23.63 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1
L3	13+66.9	23.50 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1
L4	15+27.0	23.50 RT	28'	8'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1
L5	16+87.8	23.50 RT	28'	8'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1
L6	18+45.7	23.68 RT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4
				16'**	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4
L7	19+95.4	23.98 RT	27'	11'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4
L8	21+17.8	24.71 RT	28'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4
L9	22+67.4	24.76 RT	28'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4
L10	34+77.9	23.00 RT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T3
				6'	80	TYPE 3, MEDIUM	7 PIN RECEPTACLE	
L11	33+28.4	22.50 RT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T3
				6'	80	TYPE 3, MEDIUM	7 PIN RECEPTACLE	
L12	31+92.8	29.46 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T3
L13	30+56.2	19.00 LT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T3

*=EAST

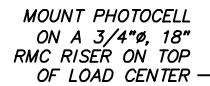
**WEST

PEDESTRIAN LIGHT COLUMN SCHEDULE						
POLE	STATION	OFFSET	HEIGHT	LAMP	DRIVE CURRENT	CIRCUIT
P1	41+11.1	6.33 RT	12'	32 LED	350mA	T6
P2	41+39.3	6.33 RT	12'	32 LED	350mA	T6
P3	42+06.7	6.34 RT	12'	32 LED	350mA	T6
P4	42+76.7	6.33 RT	12'	32 LED	530mA	T6
P5	43+61.7	6.33 RT	12'	32 LED	530mA	T6
P6	44+41.7	6.33 RT	12'	32 LED	350mA	T6
P7	45+16.7	6.33 RT	12'	32 LED	350mA	T6
P8	45+96.7	6.33 RT	12'	32 LED	350mA	T6

JUNCTION BOX SCHEDULE				
J-BOX	TYPE	CIRCUIT	STATION	OFFSET
J5A	1A	T1	17+89.6	23.08 RT
J12A	1A	T3	31+90.3	15.00 LT
NOTE: JUNCTION BOX NOT ASSOCIATED WITH A LIGHT POLE OR LOAD CENTER ARE SHOWN IN THIS TABLE.				

[illegible]

2 LOAD CENTER LIGHTING CONTROL SCHEMATIC
NTS



PLACE PLACARD ON FRONT OF LOAD
CENTER INSCRIBED WITH THE FOLLOWING:
MAXIMUM FAULT CURRENT = XXXXA
CALCULATED X/XX/XXXX.

LOAD CENTER NOTES:

1. PLACARDS FOR LOAD CENTERS SHALL HAVE SUFFICIENT DURABILITY TO WITHSTAND THE ENVIRONMENT INVOLVED. CONTACT ENGINEER PRIOR TO ORDER OF PLACARD TO VERIFY MAXIMUM FAULT CURRENT.
2. LABEL THE FRONT WITH 3M SCOTCH CAL REFLECTIVE DECALS NOTING OWNERSHIP: MOA, PURPOSE: LU (ILLUMINATION) AND THE VOLTAGE.
3. PROVIDE ARC FLASH WARNING LABELS WITH INCIDENT ENERGY VALUES AND PERSONAL PROTECTIVE EQUIPMENT (PPE) ON EACH PIECE OF EQUIPMENT IN ACCORDANCE WITH NEC ARTICLE 110.16 AND NFPA 70E.

VOLTAGE DROP					
CIRCUIT	SIZE	LENGTH	VOLTAGE	CURRENT	V.D.
T1	#8 AWG	789'	240V	2.2A	1.0%
T3	#8 AWG	618'	240V	3.3A	1.2%
T4	#8 AWG	464'	240V	1.3A	0.4%
T6	#8 AWG	1023'	240V	3.2A	2.0%

LOAD CENTER NO. <u> T </u>	TYPE: <u> 1A </u>
LOCATION: <u>W 30TH AVE: STA 18+48.8, 26.3' RT</u>	
<u>2-6</u> POLE,	<u>30</u> AMP CONTACTORS
MAIN BREAKER A: 2 POLE,	<u>100</u> AMPS, 240 VOLTS

PANEL A	<u>120/240</u> VOLTS	<u>SINGLE</u> PHASE	<u>3</u> WIRE
<u>100</u> AMPS MAIN LUGS,	<u>10,000</u> AMPS INTERRUPT CAPACITY		

CKT.	CIRCUIT DESCRIPTION	KVA	AMP		AMP	KVA	CIRCUIT DESCRIPTION	CKT.
T1	LUMINAIRES L1-L5	0.5	20/2		15/2	0.2	PHOTOELECTRIC CONTROL	T2
T3	LUMINAIRES L6, L10-L13	0.8	20/2		20/2	0.3	LUMINAIRES L7-L9	T4
T5	SPARE	0.0	20/2		20/2	0.7	PEDESTRIAN LUMINAIRES P1-P8	T6
					20/2	0.0	SPARE	T8



TOTAL CONNECTED LOAD = 2.5 KVA

TOTAL AMPS = 10.5 A

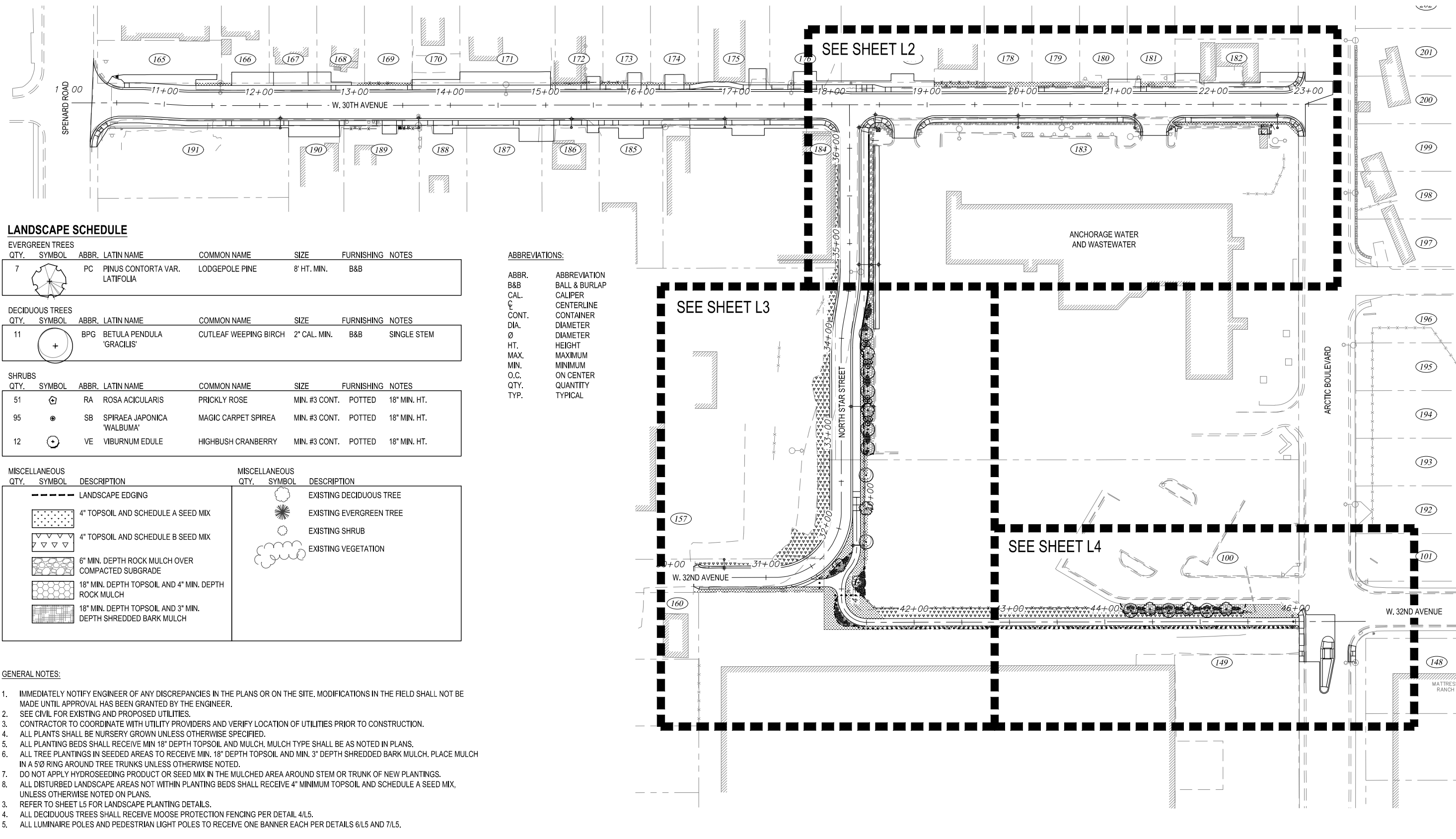
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		15 of 16																							

1. CONNECT NO. 4 GROUND WIRE TO ONE OF THE TOP SPIRALS WITH TWO IRREVERSIBLE, HYDRAULICALLY SWAGED CONNECTORS AS SHOWN. GROUND WIRE SHALL BE BARE SOLID, STRANDED, OR BRAIDED COPPER. PROTECT GROUND WIRE WITH CONDUIT AS SHOWN AND FILL CONDUIT WITH SILICON SEALANT. ALLOW 1" OF THE SLEEVE AND 24" OF CONDUCTOR TO PROTRUDE FROM THE FOUNDATIONS.
2. THIS FOUNDATION IS APPROVED FOR APPLICATIONS IN COHESIONLESS SOILS WITH AN NI-60 VALUE OF 10 OR GREATER PER AASHTO T-206, "STANDARD PENETRATION TEST" (SPT). THIS FOUNDATION SHALL NOT BE USED IF ANY OF THE FOLLOWING ARE ENCOUNTERED; WATER TABLE ABOVE THE BOTTOM OF FOUNDATION, VERY LOOSE SOILS, ORGANIC SOILS OR, COHESIVE SOILS (CLAY), OR SOILS SUSCEPTIBLE TO FROST JACKING. IF ANY OF THESE CONDITIONS ARE ENCOUNTERED, STOP FOUNDATION WORK AND CONTACT THE ENGINEER.



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MUNICIPAL/STATE	RB	JK																																																																																																																					
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195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10																																																																																																																				
STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53																																																																																																																				
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CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST																																																																																																																						
INSPECTOR																																																																																																																							
2. DATA TRANSFERRED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____			3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: _____ TITLE: _____ COMPANY: _____ DATE: _____ BY: _____			<table border="1"> <tr> <th>PLAN CHECK</th> <th>CONSTRUCTION RECORD</th> <th>VERTICAL DATUM</th> <th>REVISIONS</th> <th>CONSULTANT</th> <th>SEAL</th> </tr> <tr> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> </table>			PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL							SCALE HOR. N/A VER. N/A GRID SW6629, SW6630 DATE NOV 2019 STATUS 65% SHEET 16 of 16																																																																																																		
PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL																																																																																																																		

File P:\17-171_32nd Avenue\2-CAD\Drawings\LA\BN-PLANS.dwg



GENERAL NOTES:

1. IMMEDIATELY NOTIFY ENGINEER OF ANY DISCREPANCIES IN THE PLANS OR ON THE SITE. MODIFICATIONS IN THE FIELD SHALL NOT BE MADE UNTIL APPROVAL HAS BEEN GRANTED BY THE ENGINEER.
2. SEE CIVIL FOR EXISTING AND PROPOSED UTILITIES.
3. CONTRACTOR TO COORDINATE WITH UTILITY PROVIDERS AND VERIFY LOCATION OF UTILITIES PRIOR TO CONSTRUCTION.
4. ALL PLANTS SHALL BE NURSERY GROWN UNLESS OTHERWISE SPECIFIED.
5. ALL PLANTING BEDS SHALL RECEIVE MIN 18" DEPTH TOPSOIL AND MULCH. MULCH TYPE SHALL BE AS NOTED IN PLANS.
6. ALL TREE PLANTINGS IN SEEDED AREAS TO RECEIVE MIN. 18" DEPTH TOPSOIL AND MIN. 3" DEPTH SHREDDED BARK MULCH. PLACE MULCH IN A 5' Ø RING AROUND TREE TRUNKS UNLESS OTHERWISE NOTED.
7. DO NOT APPLY HYDROSEEDING PRODUCT OR SEED MIX IN THE MULCHED AREA AROUND STEM OR TRUNK OF NEW PLANTINGS.
8. ALL DISTURBED LANDSCAPE AREAS NOT WITHIN PLANTING BEDS SHALL RECEIVE 4" MINIMUM TOPSOIL AND SCHEDULE A SEED MIX, UNLESS OTHERWISE NOTED ON PLANS.
3. REFER TO SHEET L5 FOR LANDSCAPE PLANTING DETAILS.
4. ALL DECIDUOUS TREES SHALL RECEIVE MOOSE PROTECTION FENCING PER DETAIL 4/L5.
5. ALL LUMINAIRE POLES AND PEDESTRIAN LIGHT POLES TO RECEIVE ONE BANNER EACH PER DETAILS 6/L5 AND 7/L5.

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	
BY: _____	TITLE: _____ DATE: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
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DATA TRANSFER CHECKED BY: _____ TITLE: _____	
COMPANY: _____	DATE: _____
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE		
TOPOGRAPHY		
PROFILE		
STORM SEWER		
WATER/SANITARY SEWER		
GAS		
TELEPHONE		
ELECTRIC		
DESIGN	MB	MK
QUANTITIES	MB	MK
PRELIMINARY/FINAL		
MUNICIPAL/STATE		

GRAPHIC SCALE								
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FIELD BOOKS		BM NO.	LOCATION	ELEV.	REV.	DATE	DESCRIPTION	BY
DESIGN								
STAKING								
ASBUILT								
CONTRACTOR								
INSPECTOR								
BASIS OF THIS DATUM								
PLAN CHECK		CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS		CONSULTANT

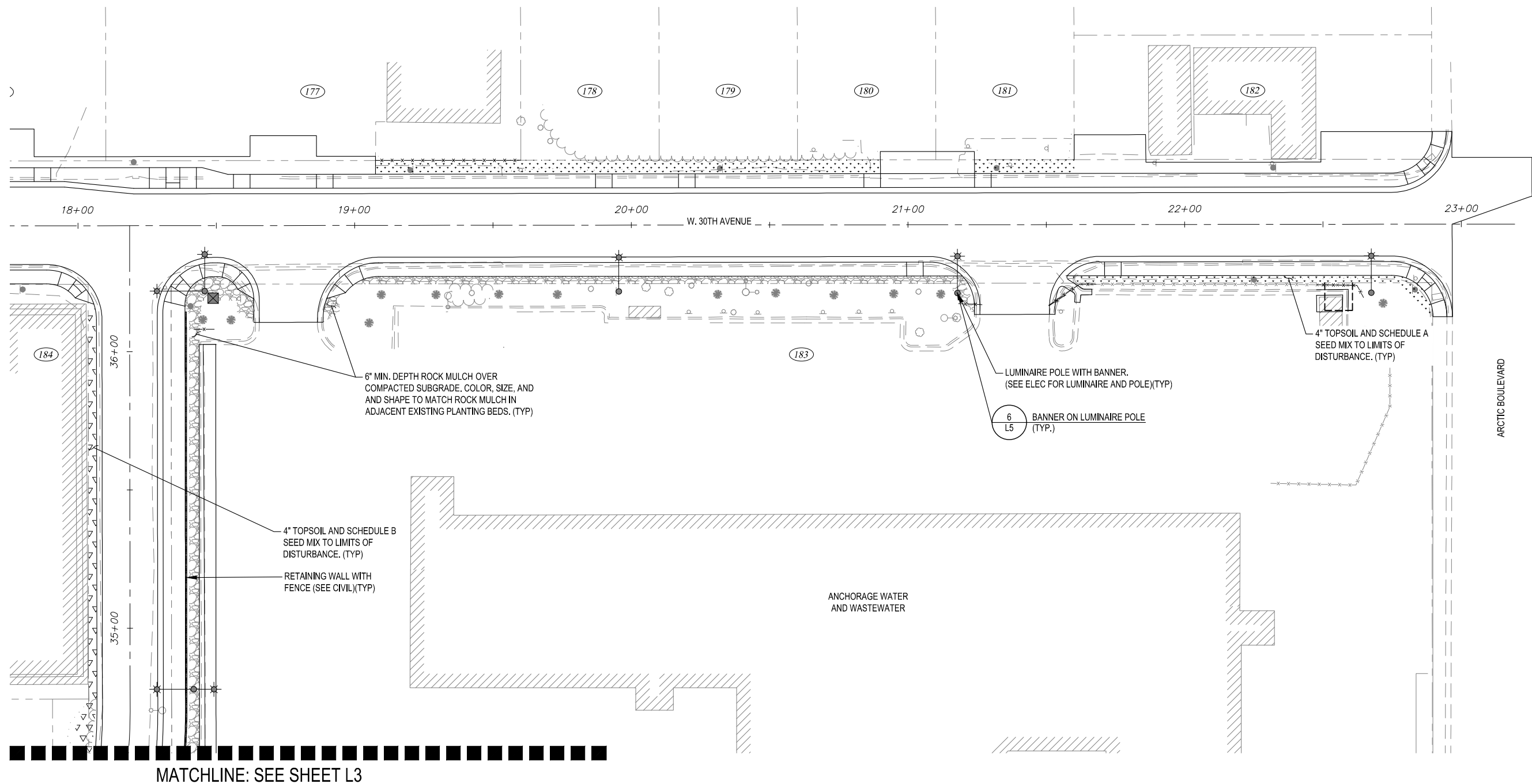
BETTISWORTH NORTH
ARCHITECTS AND PLANNERS
2004 DENALI STREET SUITE 200
ANCHORAGE, ALASKA 99501
(907) 561-0700
CORPORATE NO. 080019

STATE OF ALASKA
49th
MARK M KIMERER
No. 11157
PROFESSIONAL LANDSCAPE ARCHITECT

MUNICIPALITY OF ANCHORAGE
SEAL

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
16-29	W. 32ND AND E. 33RD AVENUE UPGRADES Seward Road to Old Seward Highway - PHASE 1
LANDSCAPE PLAN AND SCHEDULE	
SCALE HOR. 1"=50'	GRID SW1629, SW1630
VER. N/A	DATE NOV 2019 STATUS 65%
SHEET	L1 of L5

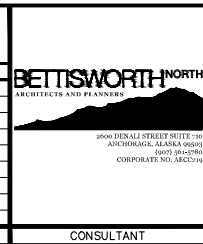
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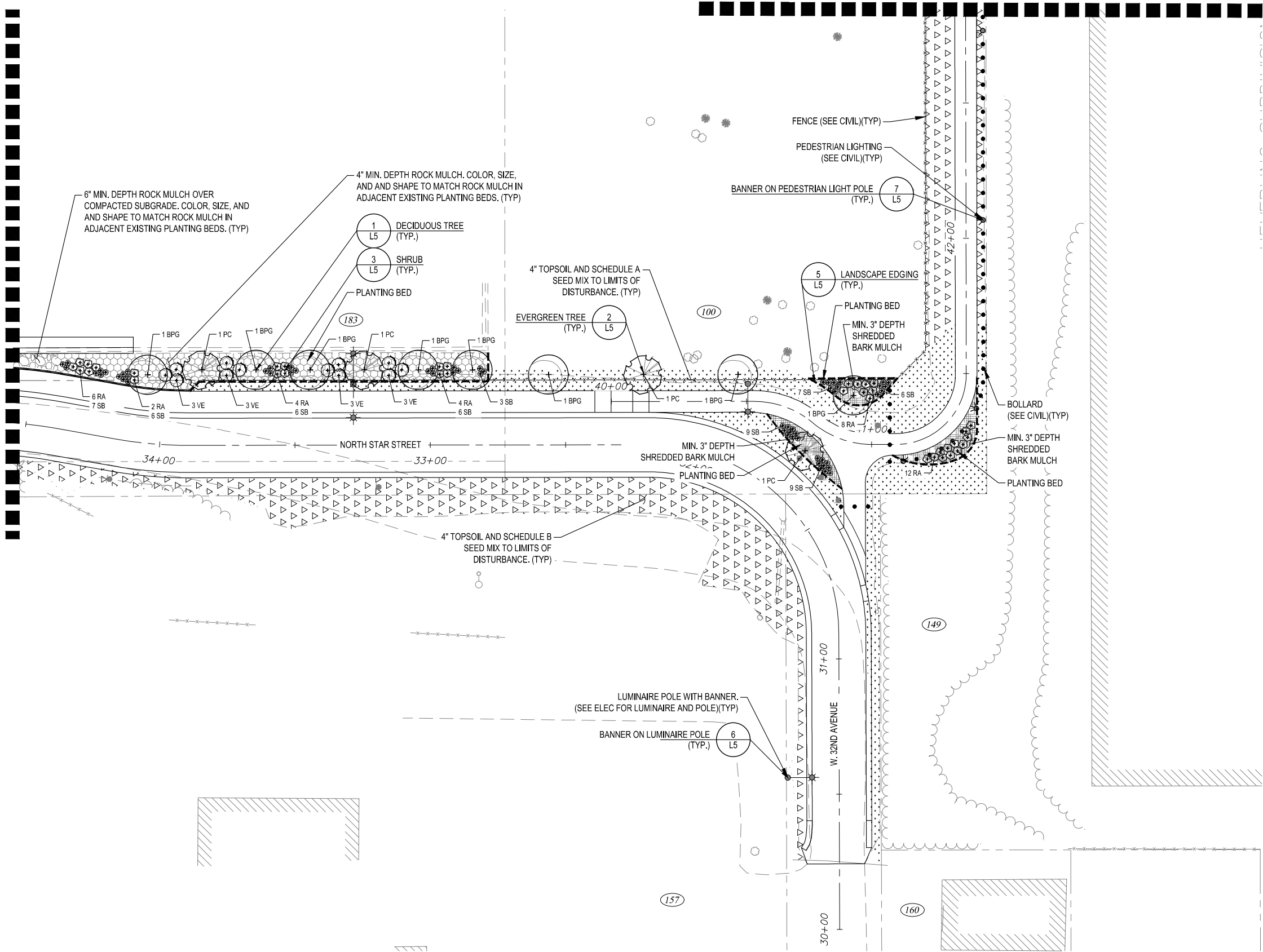
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THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.	
CONTRACTOR: _____	DATE: _____
BY: _____	TITLE: _____
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COMPANY: _____	DATE: _____
BY: _____	

DATA	DRAWN BY	CHECKED BY
BASE		
TOPOGRAPHY		
PROFILE		
STORM SEWER		
WATER/SANITARY SEWER		
GAS		
TELEPHONE		
ELECTRIC		
DESIGN	MB	MK
QUANTITIES	MB	MK
PRELIMINARY/FINAL		
MUNICIPAL/STATE		

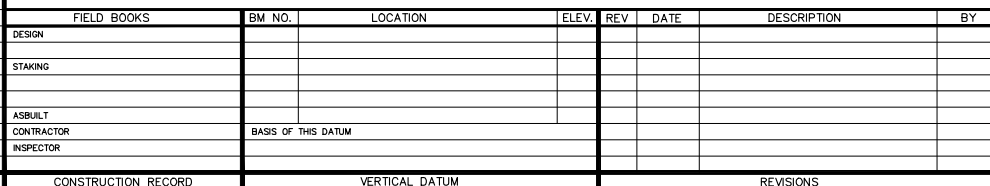
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DESIGN	BM NO.
STAKING	LOCATION
	ELEV.
	REV
	DATE
	DESCRIPTION
	BY
ASBUILT	
CONTRACTOR	
INSPECTOR	
BASIS OF THIS DATUM	
PLAN CHECK	CONSTRUCTION RECORD
VERTICAL DATUM	REVISIONS



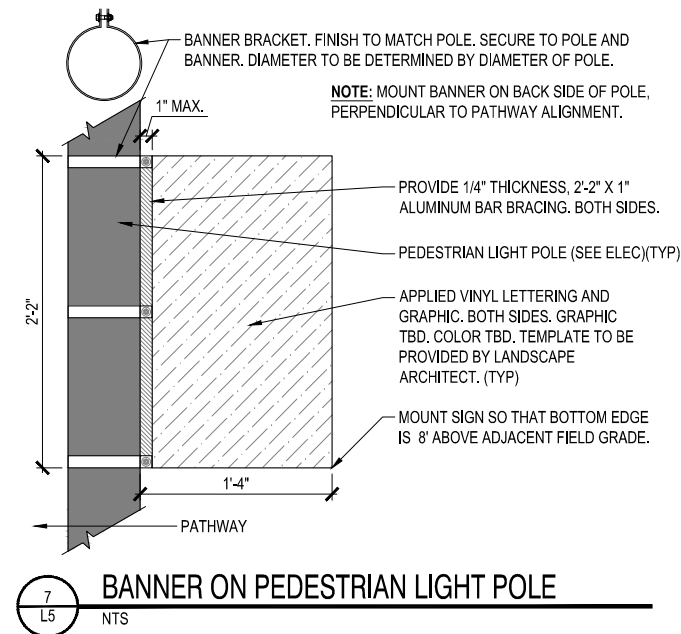
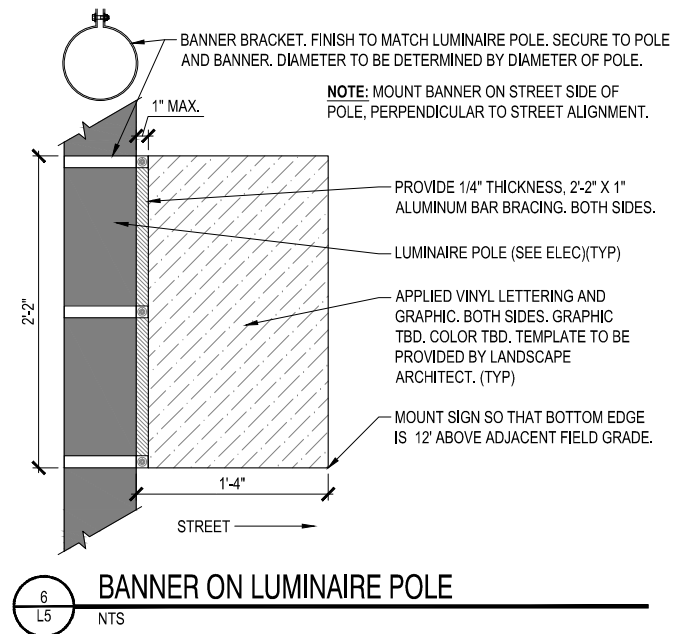
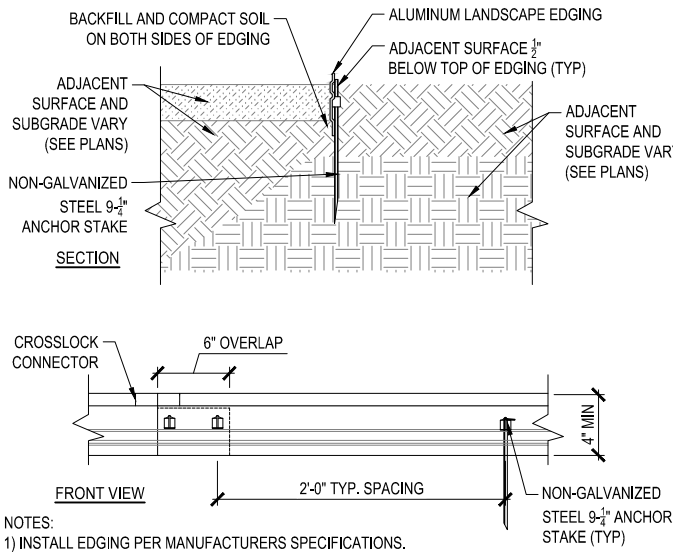
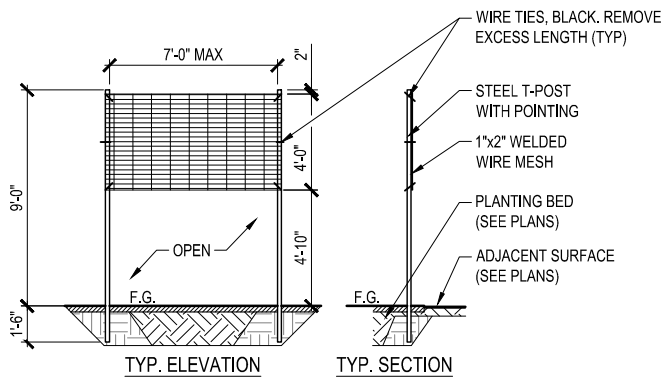
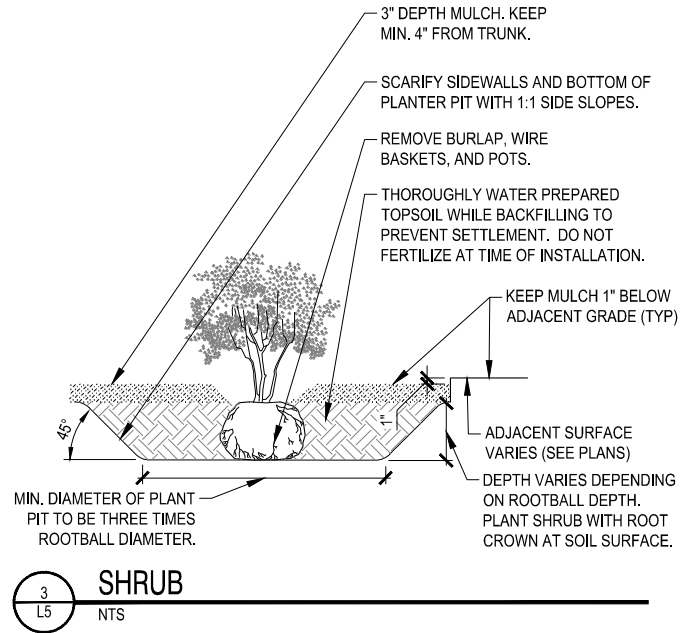
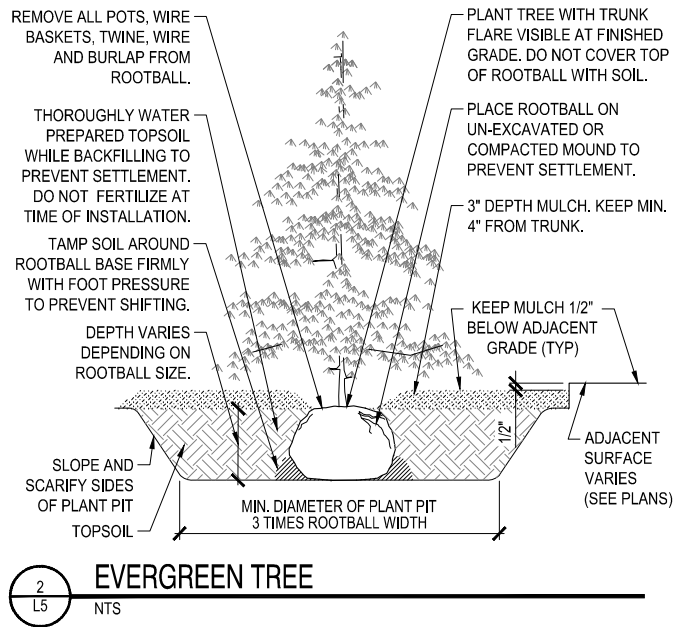
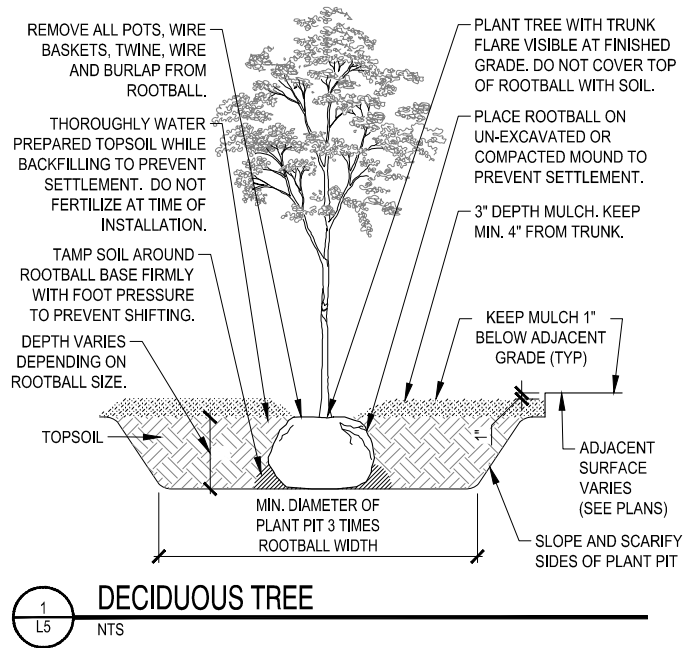
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
16-29	W. 32ND AND E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1
LANDSCAPE ENLARGEMENT	
SCALE	HOR. 1"=20'
VER. N/A	GRID SW1629, SW1630
DATE NOV 2019	STATUS 65%
SHEET	L2 of L5



DATA	DRAWN BY	CHECKED BY
BASE		
TOPOGRAPHY		
PROFILE		
STORM SEWER		
WATER/SANITARY SEWER		
GAS		
TELEPHONE		
ELECTRIC		
DESIGN	MB	MK
QUANTITIES	MB	MK
PRELIMINARY/FINAL		
MUNICIPAL/STATE		
PLAN CHECK		



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
W. 32ND AND E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1			SCHED D
<h1 style="margin: 0;">LANDSCAPE ENLARGEMENT</h1>			
HOR. 1"=20' VER. N/A	GRID SW1629, SW1630 DATE NOV. 2019 STATUS 65%		L3 of L5 SHEET



RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

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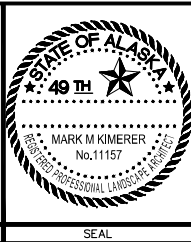
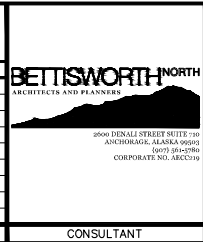
DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE		
TOPOGRAPHY		
PROFILE		
STORM SEWER		
WATER/SANITARY SEWER		
GAS		
TELEPHONE		
ELECTRIC		
DESIGN	MB	MK
QUANTITIES	MB	MK
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MUNICIPAL/STATE		

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN							
STAKING							
ASBUILT							
CONTRACTOR							
INSPECTOR							
BASIS OF THIS DATUM							
VERTICAL DATUM							
REVISIONS							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AND E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 SCHED D

LANDSCAPE DETAILS

SCALE: HOR. N/A VER. N/A GRID: SW1629, SW1630 DATE: NOV 2019 STATUS: 65% SHEET: L5 of L5