



MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 30TH AVENUE & NORTH STAR STREET UPGRADES SPENARD ROAD TO ARCTIC BOULEVARD

PROJECT NUMBER: 16-29 NOVEMBER 2019

65% DESIGN

PREPARED BY:



APPROVED BY:

KENT KOHLHASE, P.E. MUNICIPAL ENGINEER

OUEET NO	SHEET INDEX	l work
SHEET NO.	DESCRIPTION	SCHEDU
GENERAL		
G1	COVER SHEET	ALL
G2	INDEX	ALL
G3	GENERAL NOTES	ALL
G4	LEGEND AND ABBREVIATIONS	ALL
G5	KEY MAP	ALL
SURVEY	_	
V1	SURVEY CONTROL	ALL
V2	SURVEY CONTROL	ALL
V3	SURVEY CONTROL	ALL
V4	RIGHT OF WAY MAP	ALL
V5	RIGHT OF WAY MAD	ALL
V6	RIGHT OF WAY MAD	ALL
V7	RIGHT OF WAY MAD	ALL
V8 V9	RIGHT OF WAY MAP EASEMENT & PERMIT INDEX MAP	ALL
V9 V10	EASEMENT & PERMIT INDEX MAP	ALL
V10 V11	EASEMENT & PERMIT INDEX MAP	ALL
V12	EASEMENT & PERMIT INDEX MAP	ALL
V12 V13	EASEMENT & PERMIT INDEX MAP	ALL
DEMOLITION	ENSEMBLY & LEMMY HADEN WITH	, ALL
B1	DEMOLITION PLAN	ALL
B2	DEMOLITION PLAN	ALL
B3	DEMOLITION PLAN	ALL
B4	DEMOLITION PLAN	ALL
B5	DEMOLITION SUMMARY TABLES	ALL
В6	DEMOLITION SUMMARY TABLES	ALL
В7	DEMOLITION SUMMARY TABLES	ALL
TYPICAL SEC	CTIONS	
C1	TYPICAL SECTIONS	SCHED A
C2	TYPICAL SECTIONS	SCHED A
C3	TYPICAL SECTIONS	SCHED A
C4	TYPICAL SECTIONS	SCHED A
C5	TYPICAL SECTIONS	SCHED A
ROADWAY	T	
R1	ROADWAY PLAN & PROFILE	SCHED A
R2	ROADWAY PLAN & PROFILE	SCHED A
R3	ROADWAY PLAN & PROFILE	SCHED A
R4	PATHWAY PLAN & PROFILE	SCHED A
R5	INTERSECTION LAYOUT PLAN	SCHED A
R6	INTERSECTION LAYOUT TABLE	SCHED A
R7	INTERSECTION LAYOUT TABLE	SCHED A
R8 R9	INTERSECTION LAYOUT PLAN INTERSECTION LAYOUT PLAN	SCHED A
R10	INTERSECTION LAYOUT PLAN	SCHED A
R11	DRIVEWAY RECONSTRUCTION PLAN	SCHED A
	JMMARY TABLES	1 JOHED A
T1	ROADWAY SUMMARY TABLES	SCHED A
T2	ROADWAY SUMMARY TABLES	SCHED A
T3	ROADWAY SUMMARY TABLES	SCHED A
ROADWAY DI	1	1
D1	ROADWAY DETAILS	SCHED A
D2	ROADWAY DETAILS	SCHED A
D3	ROADWAY DETAILS	SCHED A
D4	ROADWAY DETAILS	SCHED A
D5	ROADWAY DETAILS	SCHED A
RETAINING \		
RW1	RETAINING WALL PLAN & PROFILE	SCHED A
RW2	RETAINING WALL DETAILS	SCHED A

SHEET NO.	DESCRIPTION	WORK SCHEDULE							
SIGNING & STRIPING									
S1	SIGNING & STRIPING PLAN	SCHED A							
S2	SIGNING & STRIPING PLAN	SCHED A							
S3	SIGNING & STRIPING PLAN	SCHED A							
S4	SIGNING & STRIPING PLAN	SCHED A							
S5	SIGNING & STRIPING PLAN	SCHED A							
S6	SIGN SCHEDULE SUMMARY	SCHED A							
S7	SIGN SCHEDULE SUMMARY	SCHED A							
STORM DRAII	N								
SD1	STORM DRAIN PLAN & PROFILE	SCHED B							
SD2	STORM DRAIN PLAN & PROFILE	SCHED B							
SD3	STORM DRAIN PLAN & PROFILE	SCHED B							
SD4	STORM DRAIN DETAILS	SCHED B							
SD5	STORM DRAIN DETAILS	SCHED B							
SD6	STORM DRAIN DETAILS	SCHED B							
ILLUMINATIO	N								
I1	ILLUMINATION PLAN	SCHED C							
12	ILLUMINATION PLAN	SCHED C							
13	ILLUMINATION PLAN	SCHED C							
14	ILLUMINATION SCHEDULES	SCHED C							
15	LOAD CENTER DETAILS	SCHED C							
16	PEDESTRIAN LIGHT COLUMN DETAILS	SCHED C							
LANDSCAPE									
L1	LANDSCAPE PLAN AND SCEHDULE	SCHED D							
L2	LANDSCAPE ENLARGEMENT	SCHED D							
L3	LANDSCAPE ENLARGEMENT	SCHED D							
L4	LANDSCAPE ENLARGEMENT	SCHED D							
L5	LANDSCAPE DETAILS	SCHED D							

WORK SCHEDULES	
А	ROADWAY IMPROVEMENTS
В	DRAINAGE IMPROVEMENTS
С	ILLUMINATION IMPROVEMENTS
D	LANDSCAPING IMPROVEMENTS

₹E	CORD DRAWING			
ı.	DATA PROVIDED BY:		TITLE:	F
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	CONTRACTOR:			I
	BY:	_ TITLE:	DATE:	[
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3.	BASED ON PERIODIC FIELD OBSERVATION: SUPERVISION), THE CONTRACTOR-PROVIDE	S BY THE ENGINEER (OR A D DATA APPEARS TO REF	AN INDIVIDUAL UNDER HIS/HER DIRECT PRESENT THE PROJECT AS CONSTRUCTED.	ŀ
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DATA	DRAWN BY	CHECKED				
BASE	TS	MJ				
TOPOGRAPHY	BW	BW				
PROFILE	RB	JK	FIELD BOOKS	BM NO. LOCATION ELEV. REV DATE DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-56 94.77		17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-17 106.10		11 <i></i>
GAS	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53		II
TELEPHONE	RK	JK				ENGINE
ELECTRIC	JH	TK				
DESIGN	RB	JK	ASBUILT			3940 A ANCHO
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST		PHO
PRELIMINARY/FINAL	RB	JK	INSPECTOR			
MUNICIPAL/STATE	RB	JK				
PLAN (CHECK		CONSTRUCTION RECORD	VERTICAL DATUM REVISIONS		

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DEF 16-29 W. 32ND AVENUE & SPENARD ROAD TO O

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

9 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY – PHASE 1

INDEX

SCALE HOR. N/A VER. N/A DATE NOV 2019 STATUS 65% SHEET G5

GENERAL NOTES

- 1. ALL WORK SHALL BE IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE (MOA) STANDARD SPECIFICATIONS, DATED 2015, (HEREINAFTER REFERRED TO AS MASS), THE LATEST EDITION OF THE ANCHORAGE WATER AND WASTEWATER UTILITY (AWWU) DESIGN AND CONSTRUCTION PRACTICES MANUAL (DCPM) AND THE SPECIAL PROVISIONS.
- 2. THE LOCATION OF THE EXISTING FEATURES AND UTILITIES SHOWN IN THESE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL FEATURES AND UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE ENGINEER.
- 3. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO BEGINNING CONSTRUCTION. THE PERMITS SHALL BE MAINTAINED ON THE PROJECT SITE, COPIES SHALL BE GIVEN TO THE ENGINEER.
- 4. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD TELEPHONE AND ELECTRIC UTILITIES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE AND LOCAL STATUTES, CODES AND GUIDELINES AND THE SHORING AND CLEARANCE REQUIREMENTS OF THE SERVING UTILITY.
- 5. LIMITS OF ROADWAY EXCAVATION SHOWN ON THE DRAWINGS ARE APPROXIMATE. ACTUAL LIMITS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER DURING CONSTRUCTION OPERATIONS.
- 6. GEOTECHNICAL (SOILS) INFORMATION IS INCLUDED IN THE CONTRACT DOCUMENTS.
- 7. ALL WORK SHALL BE PERFORMED WITHIN PUBLIC RIGHT-OF-WAY, PUBLIC USE EASEMENT, SLOPE EASEMENT, TEMPORARY CONSTRUCTION EASEMENT, DRAINAGE EASEMENT, ELECTRIC EASEMENT, INTRAGOVERNMENTAL USE PERMIT OR, TEMPORARY CONSTRUCTION PERMIT AREAS. ALL DISTURBED AREAS SHALL BE RESTORED TO ORIGINAL CONDITION, UNLESS OTHERWISE NOTED. REVEGETATION SHALL BE IN ACCORDANCE WITH THE PLANS AND
- 8. CONTRACTOR SHALL RESTORE DISTURBED PROPERTY TO PRE-CONSTRUCTION CONDITIONS, UNLESS OTHERWISE DIRECTED BY ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY OUTSIDE OF IDENTIFIED CONSTRUCTION LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE. DISTURBED AREAS NOT BEING PAVED SHALL BE TOPSOILED AND SEEDED WITH SCHEDULE A SEEDING MIX UNLESS
- 9. PROJECT CLEARING AND GRUBBING LIMITS SHALL COINCIDE WITH THE LIMITS OF DISTURBANCE AS SHOWN ON THE DEMOLITION (B) SHEETS. CONTRACTOR SHALL OBTAIN APPROVAL OF THE CLEARING AND GRUBBING LIMITS BY THE ENGINEER PRIOR TO CLEARING AND GRUBBING, SEE SPECIFICATIONS FOR MORE INFORMATION.
- 10. SLOPE LIMITS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE ACTUAL SLOPE LIMITS BASED ON PRECONSTRUCTION SURVEY DATA.
- 11. IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, THE CONTRACTOR SHALL SAW CUT AND REMOVE ADDITIONAL PAVEMENT BEYOND THE INITIAL SAW CUT, A MINIMUM OF 1-FOOT ONTO UNDISTURBED ASPHALT. AT TRANSVERSE JOINTS FINAL SAW CUT LINE SHALL BE SKEWED 15' - 25' PER DETAIL 3, SHEET D4. TACK COAT SHALL BE APPLIED TO THE SAWN FACE OF ASPHALT PRIOR TO BEGINNING PAVING.
- 12. PAVEMENT CROSS SLOPE ON SIDE STREETS SHALL VARY AT INTERSECTIONS TO PROVIDE POSITIVE DRAINAGE. SEE ROADWAY (R) SHEETS FOR INTERSECTION LAYOUTS.
- 13. ALL WORK AND MATERIALS REQUIRED FOR REMOVING ANY LITTER OR DEBRIS CREATED BY CONSTRUCTION OPERATIONS WITHIN THE PROJECT LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT WILL BE MADE.
- 14. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE SUBGRADE TO A DEPTH TO BE DETERMINED BY THE ENGINEER. NO ORGANIC MATERIAL OR OTHER DELETERIOUS MATERIAL SHALL BE UTILIZED FOR BACKFILL.
- 15. THE CONTRACTOR SHALL SUBMIT RECORD SURVEY NOTES WITH THE RECORD DRAWINGS.
- 16. EXCAVATION SHALL BE MEASURED BY EXCAVATED CROSS—SECTION AND SHALL BE LIMITED TO THE PAY LIMITS IDENTIFIED IN THE TYPICAL CROSS SECTIONS. UNLESS ADDITIONAL EXCAVATION IS DIRECTED BY THE ENGINEER.
- 17. THE PROJECT CENTERLINE STATIONING IS NOT RIGHT-OF-WAY CENTERLINE PER SURVEY CONTROL DRAWING UNLESS OTHERWISE NOTED. SEE SURVEY CONTROL DRAWING FOR HORIZONTAL AND VERTICAL CONTROL AND LAYOUT OF THE PROJECT CENTERLINE.
- 18. THE EASEMENTS AND TEMPORARY CONSTRUCTION PERMITS ACQUIRED FOR THIS PROJECT MAY HAVE RESTRICTIONS. SEE CONTRACT DOCUMENTS FOR RESTRICTIONS.
- 19. ALL CURB LOCATIONS, RADIUS MEASUREMENTS AND ELEVATIONS ARE TO THE TOP BACK OF CURB (TBC) UNLESS OTHERWISE NOTED.
- 20. FURNISH AND INSTALL 4" PIPE INSULATION BOARD (R-20) BETWEEN THE STORM DRAIN IMPROVEMENTS AND THE WATER AND SEWER UTILITIES WHEN THE VERTICAL CLEARANCE IS LESS THAN THREE FEET. IF 18 INCHES OF VERTICAL SEPARATION BETWEEN WATER AND SEWER/STORM DRAINS CANNOT BE MAINTAINED THEN WATER RELOCATION WILL BE REQUIRED. SEWER/STORM DRAIN PIPE JOINTS SHALL BE PLACED AT LEAST NINE (9) FEET
- 21. EXISTING WATER AND SEWER SERVICE LINES ARE NOT SHOWN IN THE PROFILES UNLESS SPECIFICALLY CALLED OUT.
- 22. WATER RESULTING FROM THE CONTRACTOR'S DEWATERING EFFORT MAY NOT BE PUMPED OR OTHERWISE DIVERTED INTO EXISTING STORM DRAINS OR SANITARY SEWERS UNLESS REQUIRED PERMITS. INCLUDING. BUT NOT LIMITED TO, PERMITS FROM THE MOA STORM WATER PLAN REVIEW OFFICE, AWWU, AND THE ALASKA DEPARTMENT OF ENVIRONMENTAL CONSERVATION, ARE OBTAINED BY THE CONTRACTOR. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE ALLOWED TO DIVERT WATER FROM EXCAVATION ONTO ROADWAYS. THE CONTRACTOR SHALL PROVIDE DISPOSAL SITE FOR EXCESS WATER AND SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS AND APPROVALS. THE CONTRACTOR SHALL PROVIDE COPIES OF PERMITS AND APPROVALS TO THE ENGINEER AND MOA ROW PERMIT OFFICE PRIOR TO BEGINNING DEWATERING.
- 23. ALL CURB AND GUTTER SHALL BE PAID AS "P.C.C. CURB AND GUTTER (ALL TYPES)" EXCEPT FOR CURBS WITH STEEL CURB FACING WHICH SHALL BE PAID AS "P.C.C. CURB AND GUTTER (TYPE 1, STEEL CURB FACING)".
- 24. EXISTING UTILITIES AND PROPOSED UTILITIES ARE NOT SHOWN IN THE TYPICAL CROSS SECTIONS.
- 25. THE MATCH EXISTING ELEVATIONS AS SHOWN IN THE PLANS ARE APPROXIMATE. CONTRACTOR SHALL ADJUST PROPOSED GRADES AS REQUIRED TO MATCH INTO EXISTING ELEVATIONS PER THE DIRECTION OF THE ENGINEER.
- 26. ALL FILL, USABLE EXCAVATION, AND TRENCH BACKFILL SHALL BE COMPACTED TO NINETY-FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT, PER MASS DIVISION 20 EARTHWORK, BASED ON MODIFIED PROCTOR TEST VALUES. ALL FILLS SHALL BE PLACED IN LIFTS NOT EXCEEDING 12-INCHES.
- 27. CAUTION!!! THERE ARE EXISTING BUILDING FOUNDATIONS AT UNKNOWN LOCATIONS AND DEPTHS NEAR OR WITHIN THE PROJECT LIMITS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE LOCATION AND DEPTH OF EXISTING BUILDING FOUNDATIONS PRIOR TO CONSTRUCTION. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE. CONTRACTOR SHALL REPAIR BUILDING FOUNDATIONS THAT ARE DAMAGED BY CONTRACTOR'S OPERATIONS AT NO COST TO OWNER.
- 28. FIRE HYDRANTS WILL BE ADJUSTED TO FINAL GRADE BY AWWU O&M DIVISION ON A REIMBURSABLE BASIS. THE CONTRACTOR IS TO PROVIDE WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE NEED FOR FINAL FIRE HYDRANT ADJUSTMENT. THE WRITTEN NOTICE IS TO CONTAIN, AT A MINIMUM, THE MANUFACTURER AND MODEL NUMBER OF THE HYDRANT AND VERTICAL ADJUSTMENT NEEDED IN SIX (6") INCREMENTS.

Г	CALL BEFORE YOU DIG!!!
<i>F</i>	Alaska Digline, Inc. Statewide
l N	Alaska Railroad

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	RF	CORD DRAWING		Г
	1.	DATA PROVIDED BY:	TITLE:	е
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DATA	DRAWN BY	CHECKED									
BASE	TS	MJ									
TOPOGRAPHY	BW	BW									
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					11.
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
TELEPHONE	RK	JK									ENG
ELECTRIC	JH	TK									394
DESIGN	RB	JK	ASBUILT								394 Ah
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST							
PRELIMINARY/FINAL	RB	JK	INSPECTOR								1
MUNICIPAL/STATE	RB	JK									
PLAN (CHECK		CONSTRUCTION RECORD		VERTICAL DATUM		REVISIONS				





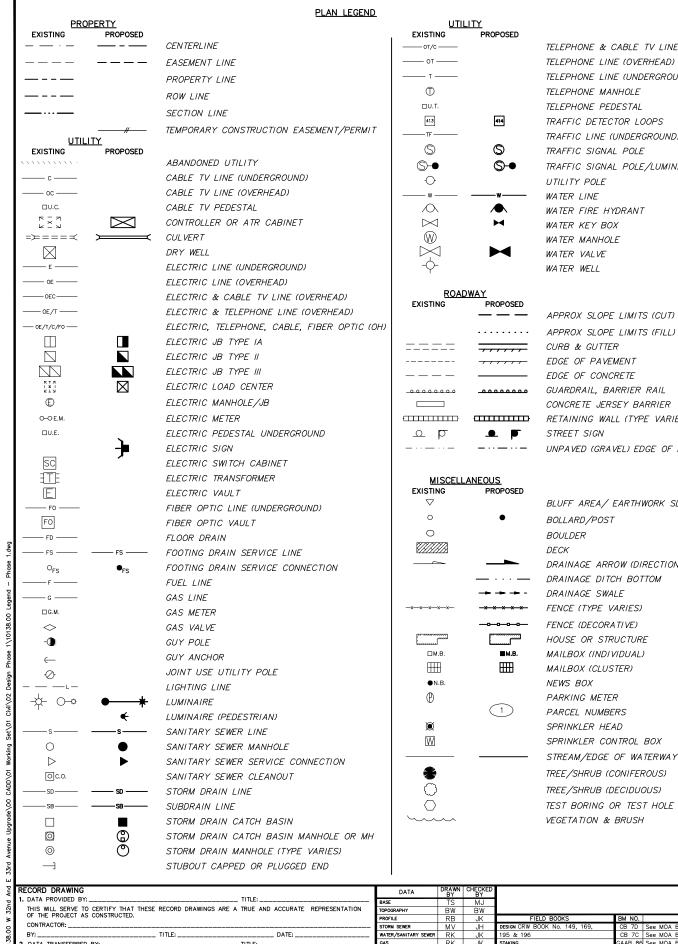
DEPARTMENT 16-29

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

GENERAL NOTES

PROJECT MANAGEMENT AND ENGINEERING

G3_{of} G5 RID SW1629, SW163 HOR. N/A



DATE:

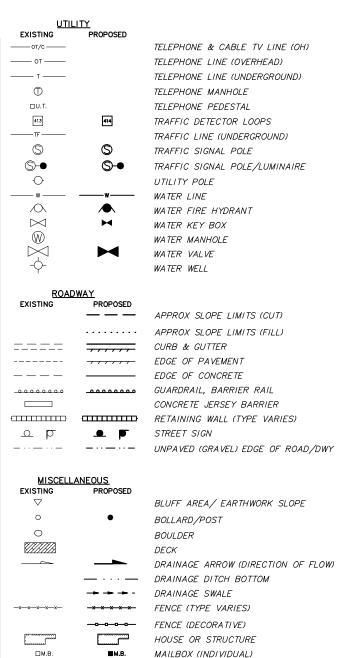
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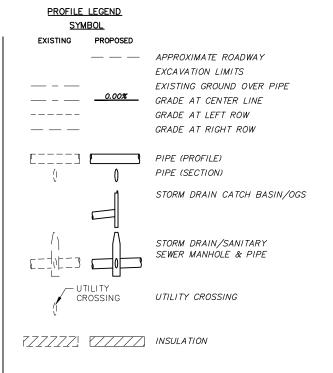
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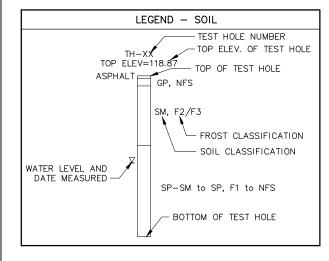
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ADDD	COMMON ABBREVI		DESCRIPTION
ABBR.	DESCRIPTION	ABBR.	DESCRIPTION
AC	ASPHALT CONCRETE	ОС	ON CENTER
AC	ASBESTOS CONCRETE	OCEW	ON CENTER EACH WAY
APPROX	APPROXIMATE	OD	OUTSIDE DIAMETER
ВМ	BENCH MARK	OGS	OIL AND GRIT SEPARATOR
BOP	BEGINNING OF PROJECT	OH	OVERHEAD
C&G	CURB AND GUTTER	PC	POINT OF CURVATURE
CB	CATCH BASIN	PCC	PORTLAND CONCRETE CEMENT
СВМН	CATCH BASIN MANHOLE		POINT OF CONTINUOUS CURVATURE
CI OI	CAST IRON	PI	POINT OF INTERSECTION
C/L, CL	CENTERLINE	<i>PL, P/L</i>	PROPERTY LINE
CMP	CORRUGATED METAL PIPE	PCMP	PRECOATED CORRUGATED METAL PIPE
CO	CLEANOUT	PCPEP	PERFORATED CORRUGATED
CONST	CONSTRUCTION		POLYETHYLENE PIPE
CPEP	CORRUGATED POLYETHYLENE PIPE	PT	POINT OF TANGENCY
DIA	DIAMETER	PUE	PUBLIC USE EASEMENT
DIP	DUCTILE IRON PIPE	PVC	POINT OF VERTICAL CURVATURE
DW	DETECTABLE WARNING	PVC	POLYVINYL CHLORIDE
DWY	DRIVEWAY	PVI	POINT OF VERTICAL INTERSECTION
E	EAST	PVT	POINT OF VERTICAL TANGENT
ELEC	ELECTRIC / ELECTRICAL	REINF .	REINFORCEMENT
ELEV, EL		ROW, R/W	RIGHT OF WAY
EOP	END OF PROJECT / EDGE OF PAVEMENT	RT, R	RIGHT
F&I	FURNISH AND INSTALL	S	SOUTH
FG	FINISHED GRADE	S/W	SIDEWALK
GALV	GALVINIZED	SS	STAINLESS STEEL
GB	GRADE BREAK	SEC COR	SECTION CORNER
JB	JUNCTION BOX	SI	STREET INTERSECTION
LC	LOAD CENTER	ST	STREET
IAW	IN ACCORDANCE WITH	STA	STATION / STATIONING
ID	INSIDE DIAMETER	STD	STANDARD
IE	INVERT ELEVATION	STRUCT	STRUCTURE
INTX	INTERSECTION	TBC	TOP BACK OF CURB
INV	INVERT	ТВМ	TEMPORARY BENCH MARK
LF	LINEAR FOOT	TCP	TEMPORARY CONSTRUCTION PERMIT
LT, L	LEFT	TELE	TELEPHONE
LUM	LUMINAIRE	TH	TEST HOLE
MAX	MAXIMUM	TW	TOP OF WALL
ME	MATCH EXISTING	TYP	TYPICAL
МН	MANHOLE	UG	UNDERGROUND
MIN	MINIMUM	UON	UNLESS OTHERWISE NOTED
MON	MONUMENT	UTIL	UTILITY
MSL	MEAN SEA LEVEL	VERT	VERTICAL
N	NORTH	VB	VALVE BOX
N/A	NOT APPLICABLE	VC	VERTICAL CURVE
N. I. C.	NOT IN CONTRACT	W	WEST
NTS	NOT TO SCALE	W/	WITH
NWT	NO WATER TABLE		

COMMON ABBREVIATIONS

- 1. STANDARD LEGEND AND ABBREVIATIONS SHOWN. NOT ALL LEGEND ITEMS AND ABBREVIATIONS ARE PART OF THIS CONTRACT.
- 2. SOIL CLASSIFICATION IS BASED UPON UNIFIED SOIL CLASSIFICATION (ASTM D 2487-00), SEE GEOTECHNICAL SOIL BORING LOGS FOR MORE INFORMATION.
- 3. SEE LEGEND ON SHEET V1 FOR SURVEY CONTROL SYMBOLS. ADDITIONAL LEGEND AND ABBREVIATION ITEMS NOT SHOWN HERE ARE PROVIDED ON SPECIFIC SHEETS THROUGHOUT THE DRAWINGS.

DATA	DRAWN BY	CHECKED									
ASE	TS	MJ									i
OPOGRAPHY	BW	BW									
ROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
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MUNICIPAL/STATE	RB	JK									



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GINEERING GROUP LLC



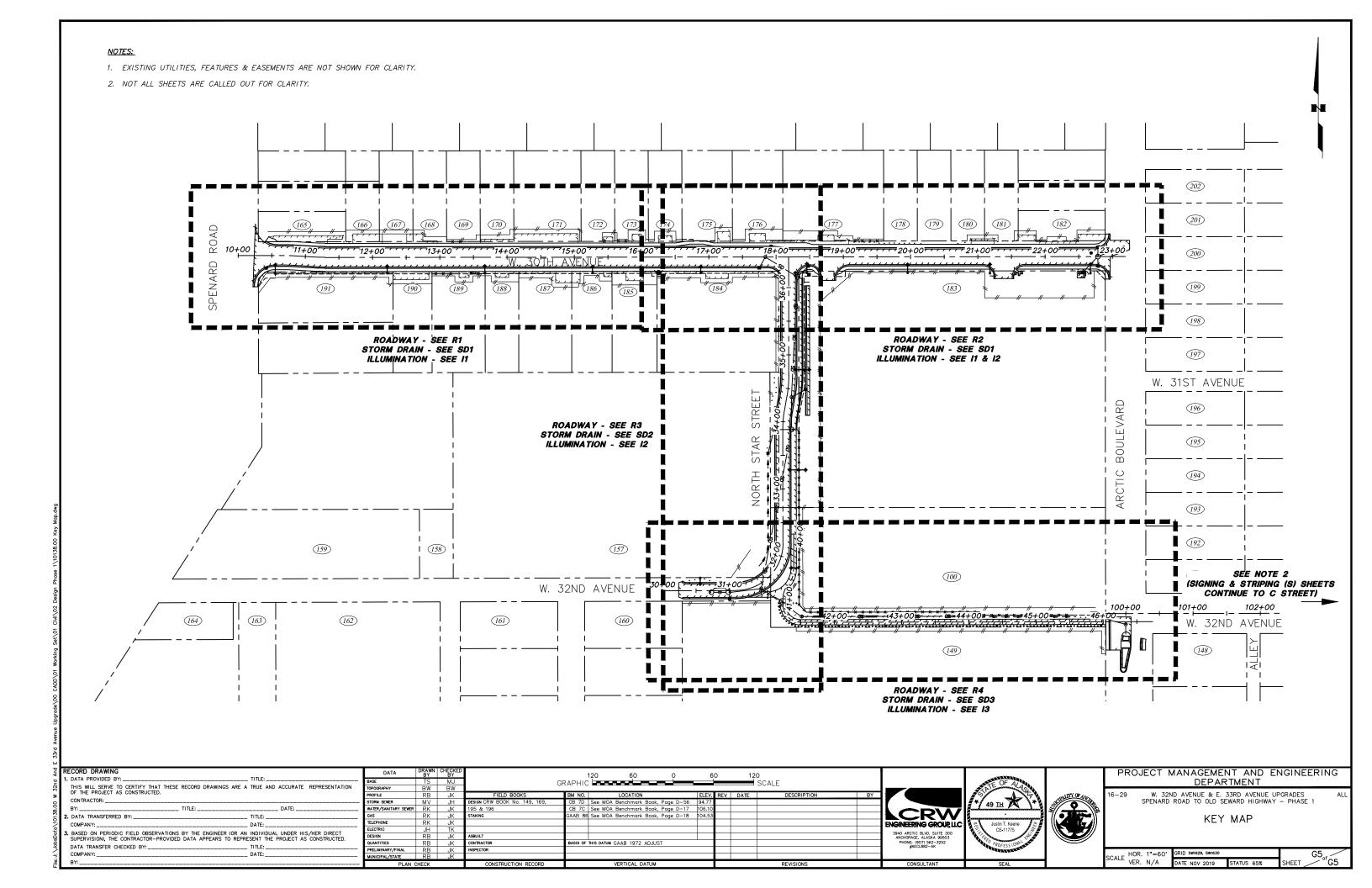
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

LEGEND AND ABBREVIATIONS

GRID SW1629, SW1630 HOR, N/A

G4_{of} G5



		Но	rizontal C	Control —	W 30th Avenue Alignment
Point	Station	Offset	Northing	Easting	Description
632	10+29.95	175.61 Rt	331236.46	344796.90	Found 5/8" Rebar 0.1' above grade
634	10+40.08	24.50 Rt	331387.58	344806.85	Found 1 1/2" Aluminum Cap flush with pavement
420	12+14.59	20.92 Rt	331388.37	344981.27	Set 1 1/8" Copper Survey Marker flush with pavement
636	12+30.54	171.37 Rt	331237.93	344997.40	Found 5/8" Rebar 0.5' below grad
635	12+88.28	21.50 Rt	331387.88	345054.96	Found 3" Aluminum Cap 0.35' below grade
429	14+04.18	13.14 Lt	331422.65	345170.82	Set 1 1/8" Copper Survey Marker flush with pavement
430	16+19.90	17.23 Rt	331392.54	345386.57	Set 1 1/8" Copper Survey Marker flush with pavement
431	18+18.10	21.46 Lt	331431.47	345584.73	Set 1 1/8" Copper Survey Marker flush with pavement
432	18+21.41	220.04 Rt	331189.97	345588.33	Set 1 1/8" Copper Survey Marker flush with pavement
625	18+33.61	21.16 Rt	331388.87	345600.29	Found 1 1/4" Brass Cap in 2" Iron Pipe 0.8' below grade
435	18+91.50	193.88 Rt	331216.22	345658.39	Set 1 1/8" Copper Survey Marker flush with pavement
436	19+95.77	14.94 Rt	331395.29	345762.44	Set 1 1/8" Copper Survey Marker flush with pavement
437	20+77.84	17.57 Lt	331427.89	345844.47	Set 1 1/8" Copper Survey Marker flush with pavement
438	22+04.10	14.50 Rt	331395.97	345970.77	Set 1 1/8" Copper Survey Marker flush with pavement
624	22+81.85	18.92 Rt	331391.64	346048.53	Found 2" Aluminum Cap 0.1' below grade
439	22+86.30	26.89 Lt	331437.46	346052.92	Set 1 1/8" Copper Survey Marker flush with pavement

		Hori	zontal Co	ntrol — N	North Star Street Alignment
Point	Station	Offset	Northing	Easting	Description
432	34+26.57	7.77 Rt	331189.97	345588.33	Set 1 1/8" Copper Survey Marker flush with pavement
435	34+56.96	73.55 Rt	331216.22	345658.39	Set 1 1/8" Copper Survey Marker flush with pavement
625	36+24.43	15.00 Rt	331388.87	345600.29	Found 1 1/4" Brass Cap in 2" Iron Pipe 0.8' below grade
430	36+28.34	198.71 Lt	331392.54	345386.57	Set 1 1/8" Copper Survey Marker flush with pavement
436	36+30.66	177.16 Rt	331395.29	345762.44	Set 1 1/8" Copper Survey Marker flush with pavement
431			331431.47	345584.73	Set 1 1/8" Copper Survey Marker flush with pavement

<u>Horizontal Control</u>

Coordinate System:

This project is located entirely within the Anchorage Bowl 2000 adjustment, a local surface grid coordinate system expressed in U.S. Survey feet units developed by the Alaska Department of Transportation.

Basis of Coordinates:

The Basis of Coordinates is NGS Station O'Malley, located near the intersection of the New Seward Highway and O'Molley Road. Said station has Anchorage Bowl 2000 coordinates of 303939.2310 N, 353362.5446 E. U.S. Survey Feet.

Basis of Bearings:

The Basis of Bearings is a local plane bearing between NGS Station O'Malley and NGS Station Loop 2 USE RM 3 1964. NGS Station Loop 2 USE RM 3 1964 bears N 01'43'26.4" E a distance of 49488.4476 feet from NGS Station O'Malley. NGS Station Loop 2 USE RM 3 1964 has Anchorage Bowl 2000 coordinates of 353405.2778 N, 354851.3982 E. U.S. Survey Feet.

Translation Parameters:

To convert the local coordinates to NAD83 (92) State Plane coordinates expressed in U.S. Survey Feet, translate using $\pm 2,296,868.6878$ N U.S. Survey Feet, $\pm 1,312,517.4904$ E U.S. Survey Feet, and scale using 0.9998910192.

<u>Vertical Control</u>

Vertical control is based on the MOA Benchmark GAAB-86, Elevation = 104.53 feet (GAAB), as described on page D-18 of the MOA Benchmark Book, MOA Benchmark CB 7C, Elevation = 106.10 feet (GAAB), as described on page D-17 of the MOA Benchmark Book, and MOA Benchmark CB 7D, Elevation = 94.77 feet (GAAB), as described on page D-56 of the MOA Benchmark Book.

LEGEND

Existing Brass Cap

Existing Aluminum Cap

Existing Rebar or Iron Pipe

Control set by CRW

(500) Control Point Number

ECORD DRAWING			
DATA PROVIDED BY:		TITLE:	
THIS WILL SERVE TO CERTIFY THAT THESE OF THE PROJECT AS CONSTRUCTED.	RECORD DRAWINGS ARE A	TRUE AND ACCURATE	REPRESENTATION
CONTRACTOR:			
BY:	TITLE:	DATE:	
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COMPANY:		DATE:	
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COMPANY:		DATE:	

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STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77				7
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10				I L
GAS	RK	JK	STAKING	GAAB 86	6 See MOA Benchmark Book, Page D-18	104.53				
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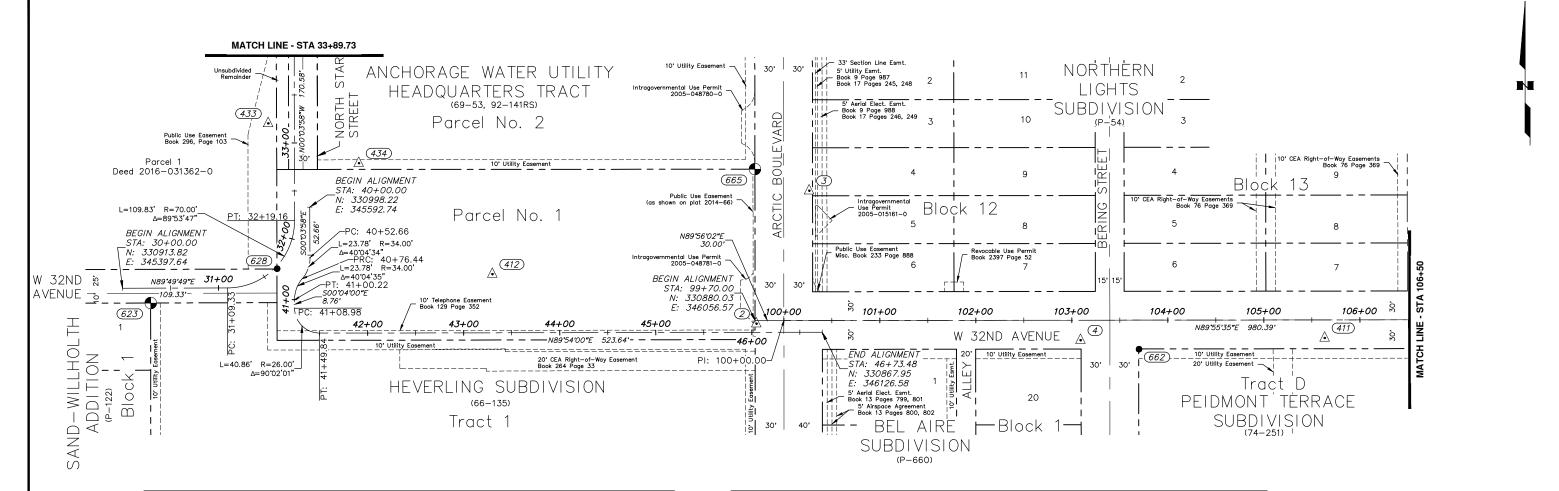
DEPARTMENT W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

SURVEY CONTROL

PROJECT MANAGEMENT AND ENGINEERING

W. 30TH AVENUE - SPENARD ROAD TO ARCTIC BLVD NORTH STAR ST - STA 33+89 TO STA 36+46

HOR. 1"=50



		Hor	rizontal C	ontrol —	North Star Street Alignment
Point	Station	Offset	Northing	Easting	Description
623	30+29.29	15.57 Rt	330898.33	345426.97	Found 1/2" Copperweld in 2" Iron Pipe 0.4' below ground
628	31+65.38	2.12 Rt	330934.08	345558.68	Found 5/8" Rebar with Orange Plastic Cap 0.1' above grade
412	32+05.14	211.90 Rt	330928.38	345783.09	Set 5/8" Rebar with Red Plastic Cap
434	32+78.86	67.31 Rt	331044.00	345644.00	Set 1 1/8" Copper Survey Marker flush with pavement
433	33+20.33	27.05 Lt	331085.36	345549.59	Set 1 1/8" Copper Survey Marker flush with pavement

			Horizon	tal Contr	ol — Pathway Alignment
Point	Station	Offset	Northing	Easting	Description
628	40+81.35	27.38 Rt	330934.08	345558.68	Found 5/8" Rebar with Orange Plastic Cap 0.1' above grade
623	41+03.48	149.91 Rt	330898.33	345426.97	Found 1/2" Copperweld in 2" Iron Pipe 0.4' below ground
434	41+91.20	176.90 Lt	331044.00	345644.00	Set 1 1/8" Copper Survey Marker flush with pavement
412	43+30.09	61.03 Lt	330928.38	345783.09	Set 5/8" Rebar with Red Plastic Cap
665	46+03.47	169.88 Lt	331037.70	346056.27	Found 3 1/4" Brass Cap 0.35' below rim of monument case
2	46+05.04	9.03 Lt	330876.86	346058.12	Set 1 1/8" Copper Survey Marker flush with sidewalk
3	46+60.26	147.41 Lt	331015.33	346113.10	Set 1 1/8" Copper Survey Marker flush with sidewalk

		Но	rizontal (Control —	W 32nd Avenue Alignment
Point	Station	Offset	Northing	Easting	Description
665			331037.7	346056.27	Found 3 1/4" Brass Cap 0.35' below rim of monument case
2	99+71.55	3.18 Rt	330876.86	346058.12	Set 1 1/8" Copper Survey Marker flush with sidewalk
3	100+26.71	135.23 Lt	331015.33	346113.10	Set 1 1/8" Copper Survey Marker flush with sidewalk
4	103+09.15	20.87 Rt	330859.60	346395.74	Set 1 1/8" Copper Survey Marker flush with sidewalk
662	103+69.84	30.27 Rt	330850.27	346456.45	Found 5/8" Rebar 0.1' below grade
411	105+63.42	18.78 Rt	330862.01	346650.01	Set 1 1/8" Copper Survey Marker flush with sidewalk

Existing Brass Cap

Existing Aluminum Cap

Existing Rebar or Iron Pipe

Control set by CRW

500 Control Point Number

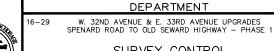
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1.	DATA PROVIDED BY:	_ TITLE:	BASE
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	CONTRACTOR:		STOR
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LE	RB	JK	FIELD BOOKS	BM NO.		LOCATION			ELEV.	REV	DATE		DESCRIPTION
A SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA	Benchmark	Book, Po	age D-56	94.77				
R/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA	Benchmark	Book, Po	ge D−17	106.10				
	RK	JK	STAKING	GAAB 86	See MOA	Benchmark	Book, Po	age D−18	104.53				
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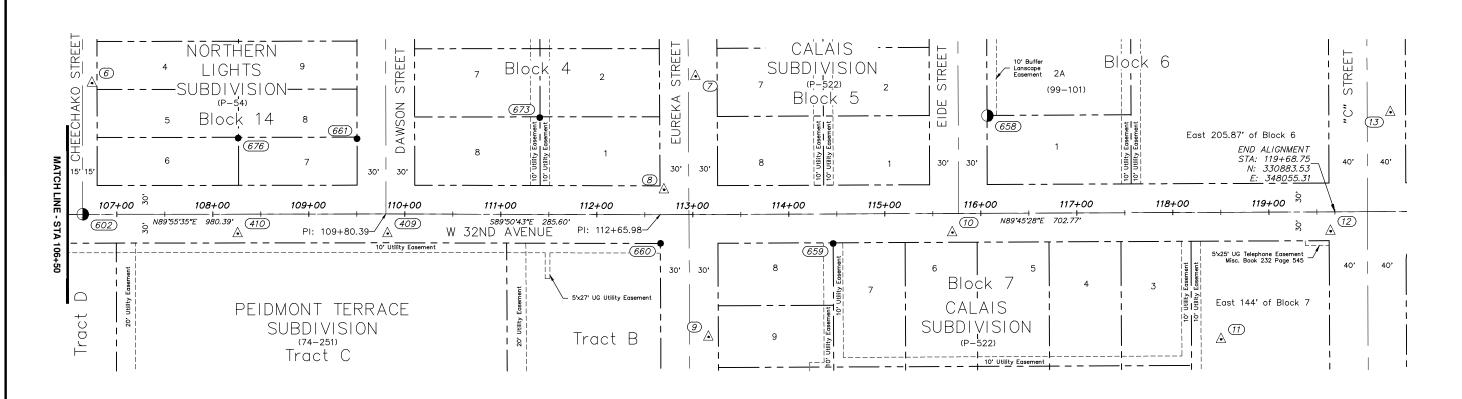


SURVEY CONTROL

PROJECT MANAGEMENT AND ENGINEERING

NORTH STAR ST-STA 30+00 TO STA 33+89/PATHWAY - STA 40+00 TO ARCTIC BLVD / W. 32ND AVE-ARCTIC BLVD TO STA 106+50

			,			
041.5	HOR.	1"=50'	GRID SW1629, SW1630			V2./
CALE	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET	<u></u> ° ∨13



		Нс	rizontal (Control -	W 32nd Avenue Alignment
Point	Station	Offset	Northing	Easting	Description
602	106+64.63	0.07 Lt	330881.00	346751.20	Found 1 1/2" Aluminum Cap flush with asphalt
6	106+74.47	136.71 Lt	331017.64	346760.86	Set 1 1/8" Copper Survey Marker flush with pavement
410	108+26.07	20.19 Rt	330860.94	346912.66	Set 1 1/8" Copper Survey Marker flush with sidewalk
676	108+26.52	79.16 Lt	330960.29	346912.98	Found 5/8" Rebar 0.6' below grade
661	109+50.34	78.41 Lt	330959.70	347036.80	Found 5/8" Rebar flush with ground
409	109+82.26	20.38 Rt	330860.95	347068.77	Set 1 1/8" Copper Survey Marker flush with sidewalk
673	111+40.30	100.81 Lt	330981.70	347227.13	Found 5/8" Rebar, bent
660	112+66.27	29.88 Rt	330850.68	347352.96	Found 5/8" Rebar 0.3' below grade
8	112+70.24	25.93 Lt	330906.51	347356.70	Set 1 1/8" Copper Survey Marker flush with sidewalk
7	113+03.61	144.00 Lt	331024.71	347389.57	Set 1 1/8" Copper Survey Marker flush with pavement
9	113+15.93	127.49 Rt	330753.28	347403.03	Set 1 1/8" Copper Survey Marker flush with sidewalk
659	114+46.03	30.74 Rt	330850.58	347532.72	Found 5/8" Rebar 0.3' below grade
10	115+70.37	19.88 Rt	330861.97	347657.02	Set 1 1/8" Copper Survey Marker flush with sidewalk
658	116+07.15	101.70 Lt	330983.70	347693.28	Found 3 1/4" Aluminum Cap 0.2' below grade
11	118+49.77	132.30 Rt	330750.73	347936.89	Set 1 1/8" Copper Survey Marker flush with pavement
12	119+64.25	20.15 Rt	330863.36	348050.89	Set 1 1/8" Copper Survey Marker flush with sidewalk
13			330987.24	348113.29	Set 1 1/8" Copper Survey Marker flush with sidewalk

Existing Brass Cap

Existing Aluminum Cap

Existing Rebar or Iron Pipe

△ Control set by CRW

500 Control Point Number

TITLE:

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR:

BY:

TITLE:

DATE:

2. DATA TRANSFERRED BY:

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S. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark	Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark	Book, Page D-17	106.10					ш
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark	Book, Page D-18	104.53					J B
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PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

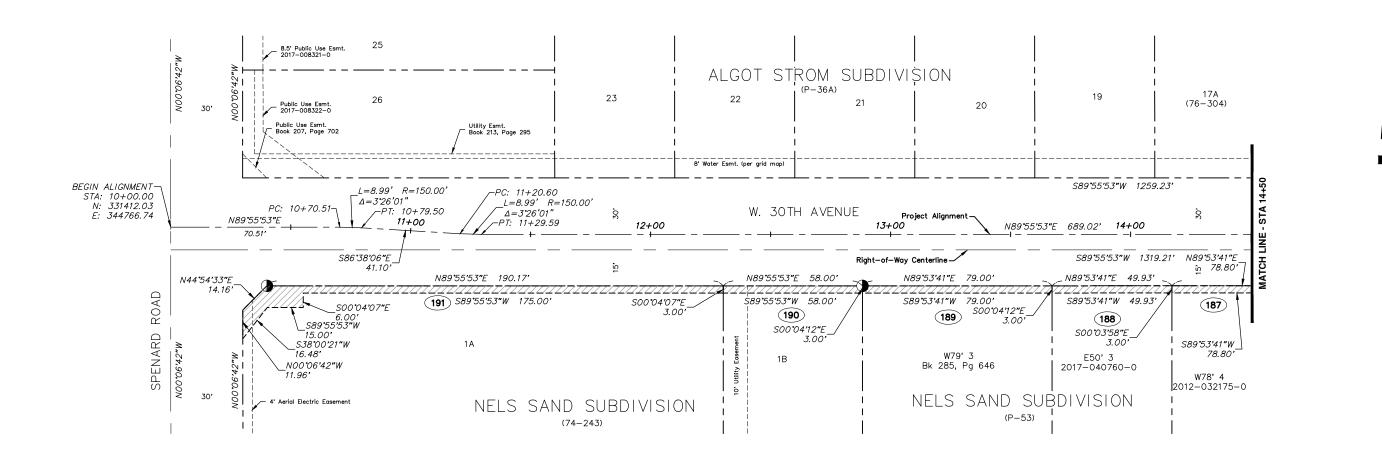
9 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

SURVEY CONTROL

W. 32ND AVENUE - STA 106+50 TO EOP

HOR. 1"=50" GRID SW1629, SW1630 V3 of V13

ALE VER. N/A DATE NOV 2019 STATUS 65% SHEET



			PARCEL INF	ORMATION		EASI	EASEMENT ACQUISITION INFORMATION		
PARCEL	LOT	BLOCK	SUBDIVISION	PLAT	OWNER	PUE (SF)	RECORDER'S SERIAL NUMBER		
187	W78' Tract 4		Nels Sand	P-53	WILBER MICHELLE M	236			
188	E50' Tract 3		Nels Sand	P-53	VINNITSKY VICTOR A	150			
189	W79' Tract 3		Nels Sand	P-53	COLTER ALAN W	237			
190	1B		Nels Sand	74-243	JACKSON VERDELLA M	174			
191	1A		Nels Sand	74-243	BERGER REAL ESTATE LLC	767			

1 Parcel Number

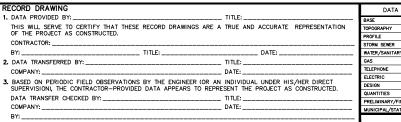
Public Use Easement (PUE)

Drainage Easement (DE)

Existing Brass Cap or Copperweld

Existing Aluminum Cap

Existing Rebar or Iron Pipe



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PROFILE	RB	JK	FIELD BOOKS	BM NO.		LOCATION			ELEV.	REV	DATE		DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA	Benchmark Bo	ook,	Page D-56	94.77						1 7
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA	Benchmark Bo	ook,	Page D-17	106.10						11.
GAS	RK	JK	STAKING	GAAB 86	See MOA	Benchmark Bo	ook,	Page D-18	104.53						
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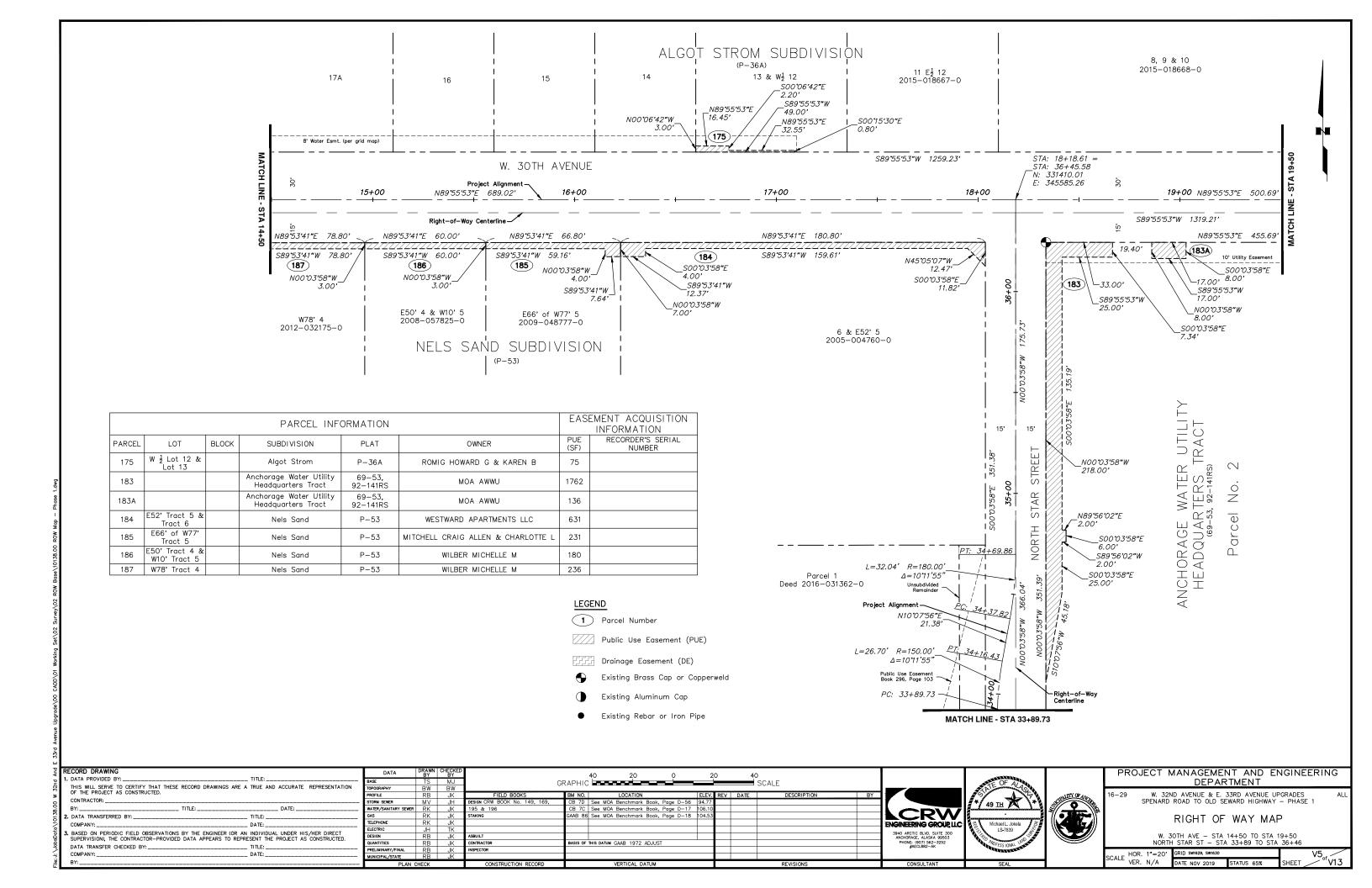


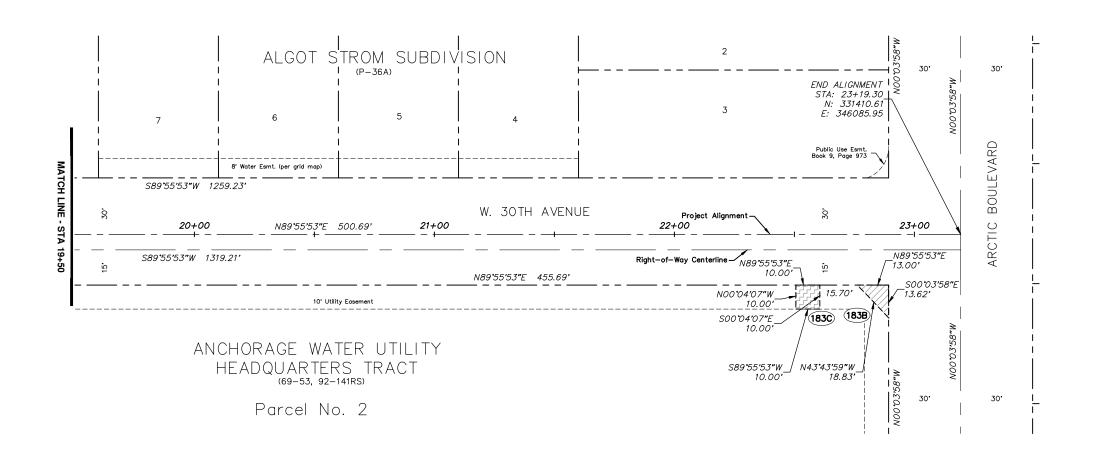


RIGHT OF WAY MAP

W. 30TH AVE - SPENARD ROAD TO STA 14+50

0415	HOR.	1"=20'	GRID SW1629, SW1630				V4	
CALE	VER.	N/A	DATE NOV 2019	STATUS	65%	SHEET		V13





			EASEMENT ACQUISITION INFORMATION					
PARCEL	LOT	BLOCK	SUBDIVISION	PLAT	OWNER	PUE (SF)	DE (SF)	RECORDER'S SERIAL NUMBER
183B			Anchorage Water Utility Headquarters Tract	69-53, 92-141RS	MOA AWWU	89		
183C			Anchorage Water Utility Headquarters Tract	69-53, 92-141RS	MOA AWWU		100	

1 Parcel Number

Public Use Easement (PUE)

Drainage Easement (DE)

• Existing Brass Cap or Copperweld

Existing Aluminum Cap

Existing Rebar or Iron Pipe

RE	CORD DRAWING		
1.	DATA PROVIDED BY:	_ TITLE:	BASE
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOPO
	OF THE PROJECT AS CONSTRUCTED.		PROF
	CONTRACTOR:		STOR
	BY: TITLE:	DATE:	WATER
2.	DATA TRANSFERRED BY:	TITLE:	GAS
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PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATIO	N		ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmo	rk Book,	Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmo	rk Book,	Page D-17	106.10					11.
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmo	rk Book,	Page D-18	104.53					
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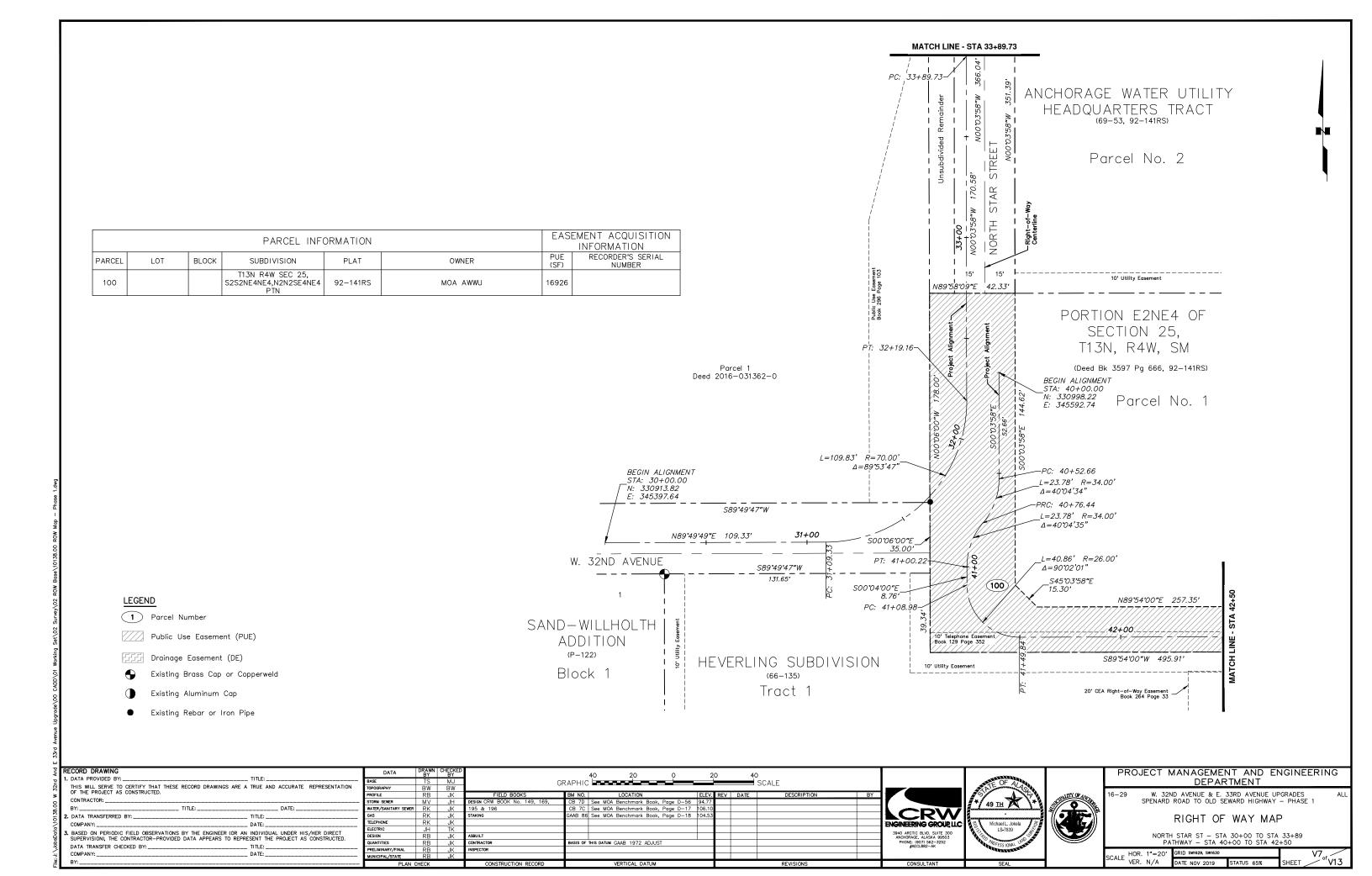


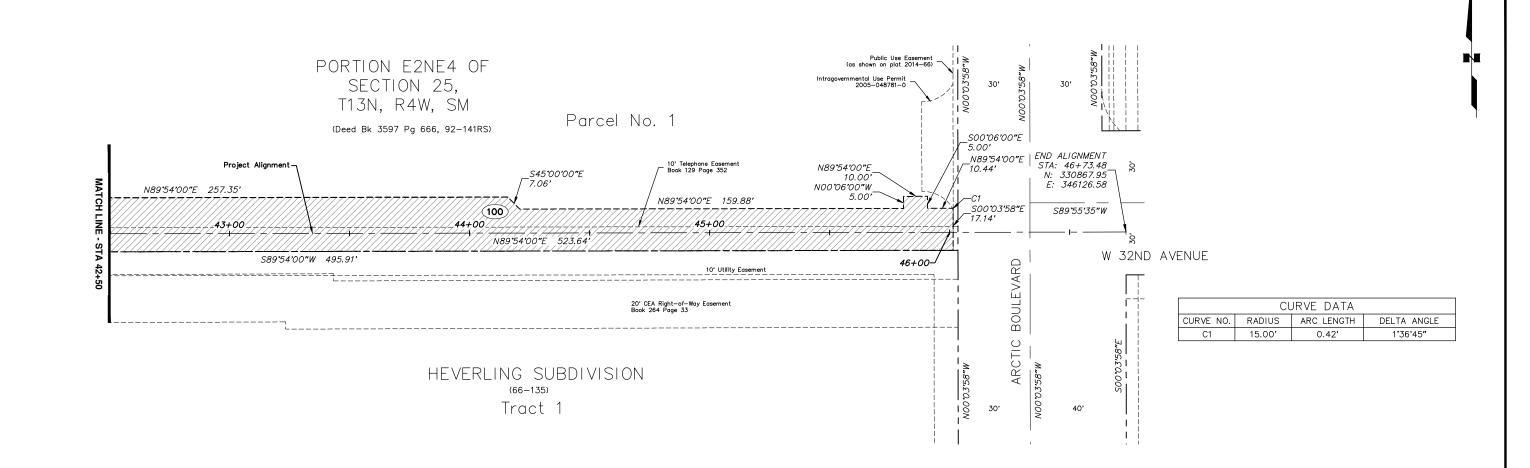
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

RIGHT OF WAY MAP

W. 30TH AVE - STA 19+50 TO ARCTIC BLVD

_	HOR.	1"=20'	GRID SW1629, SW1630		V6./
.E	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET V13





		EASEMENT ACQUISITION INFORMATION					
PARCEL	ARCEL LOT BLOCK SUBDIVISION PLAT OWNER					PUE (SF)	RECORDER'S SERIAL NUMBER
100			T13N R4W SEC 25, S2S2NE4NE4,N2N2SE4NE4 PTN	92-141RS	MOA AWWU	16926	

1 Parcel Number

Public Use Easement (PUE)

Drainage Easement (DE)

Existing Brass Cap or Copperweld

Existing Aluminum Cap

Existing Rebar or Iron Pipe

RE	CORD DRAWING		
1.	DATA PROVIDED BY:	TITLE: BASI	SE
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STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark	Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark	Book, Page D-17	106.10					11
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark	Book, Page D-18	104.53					I I
TELEPHONE	RK	JK										ENG
ELECTRIC	JH	TK										
DESIGN	RB	JK	ASBUILT									394 Al
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PRELIMINARY/FINAL	RB	JK	INSPECTOR									1
MUNICIPAL/STATE	RB	JK										
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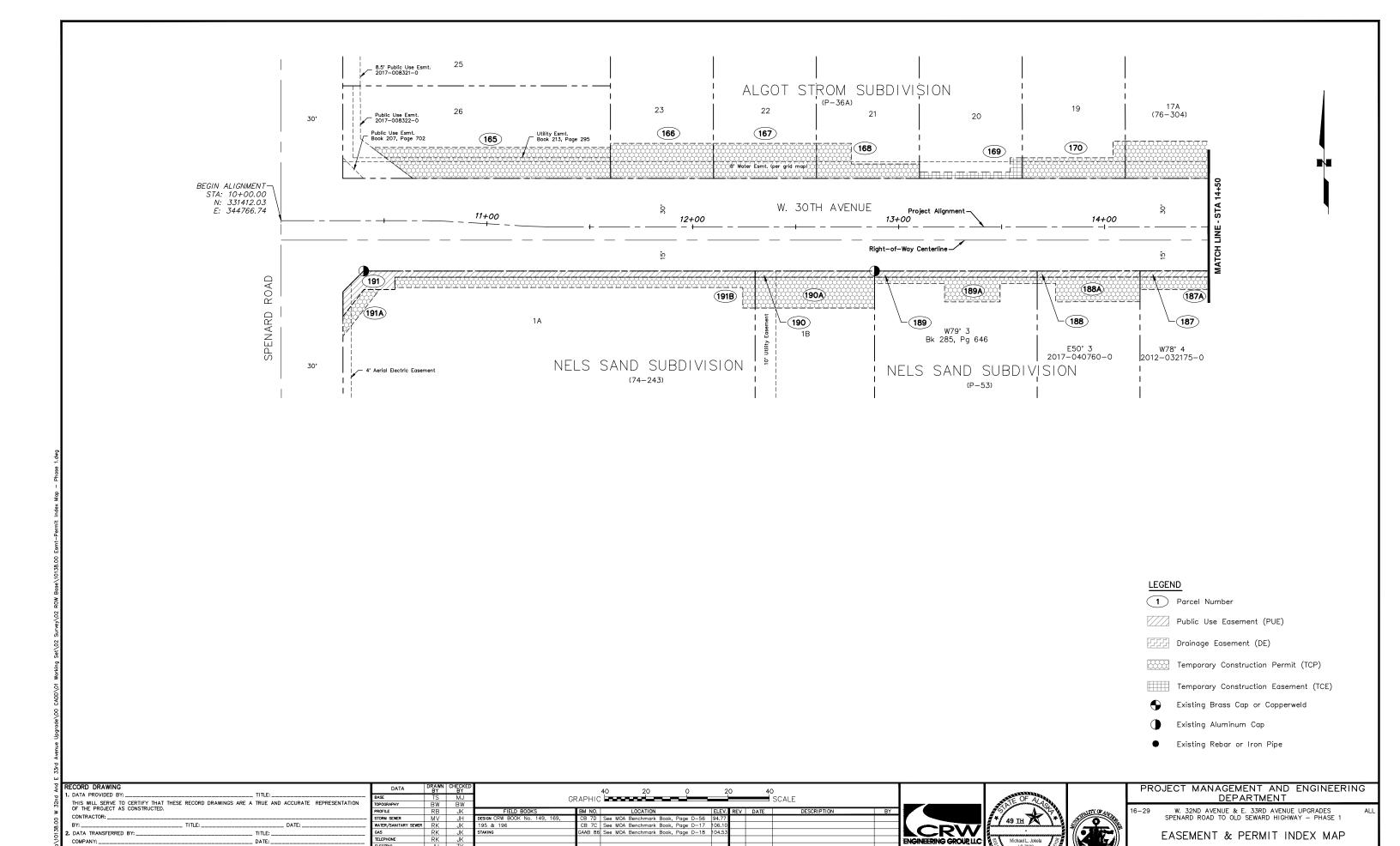
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

RIGHT OF WAY MAP

PATHWAY - STA 42+50 TO ARCTIC BLVD

SCALE HOR. 1"=20' VER. N/A



SIS OF THIS DATUM GAAB 1972 ADJUST

__ DATE: __

DATE:

BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

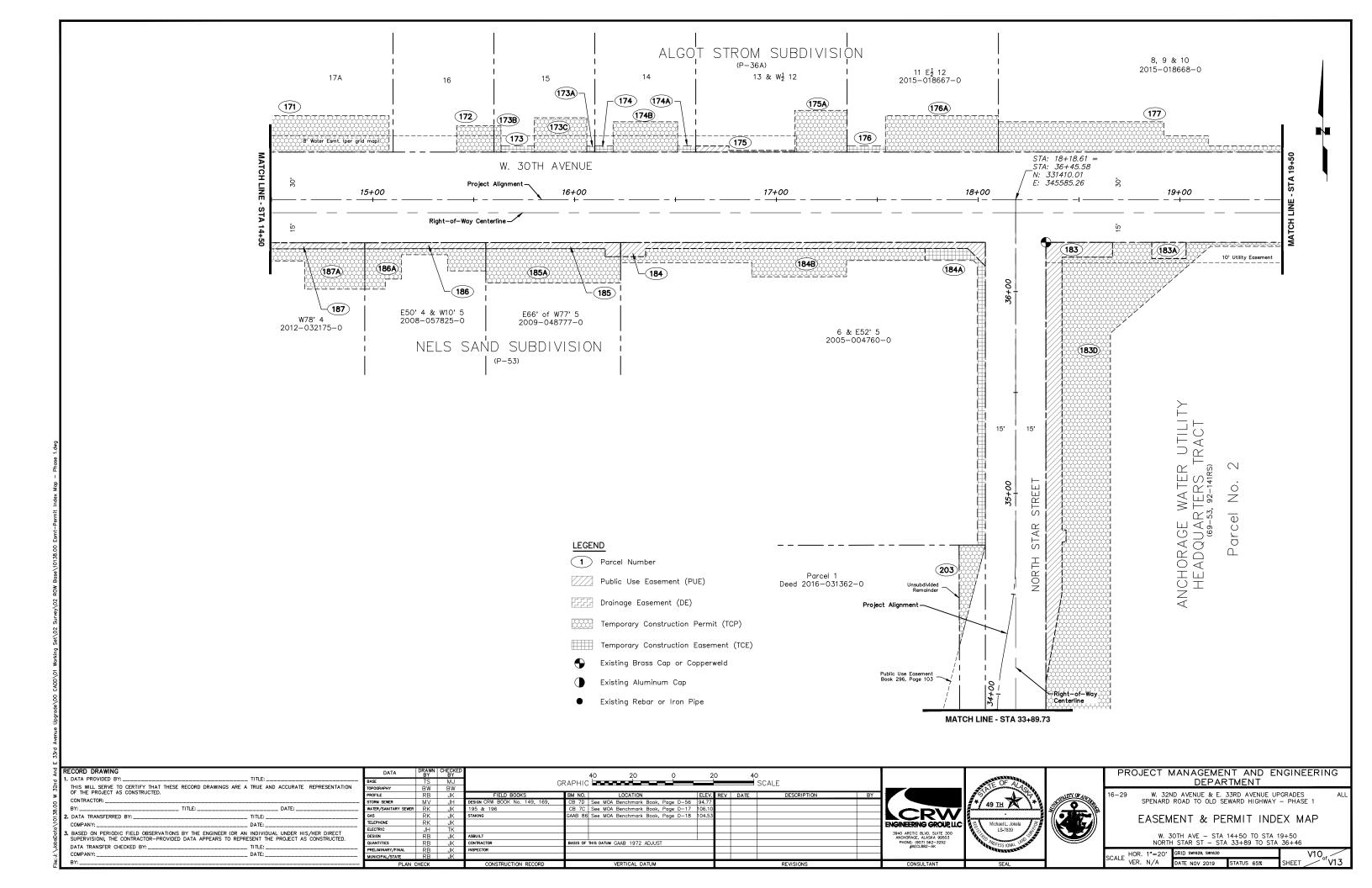
COMPANY: ___

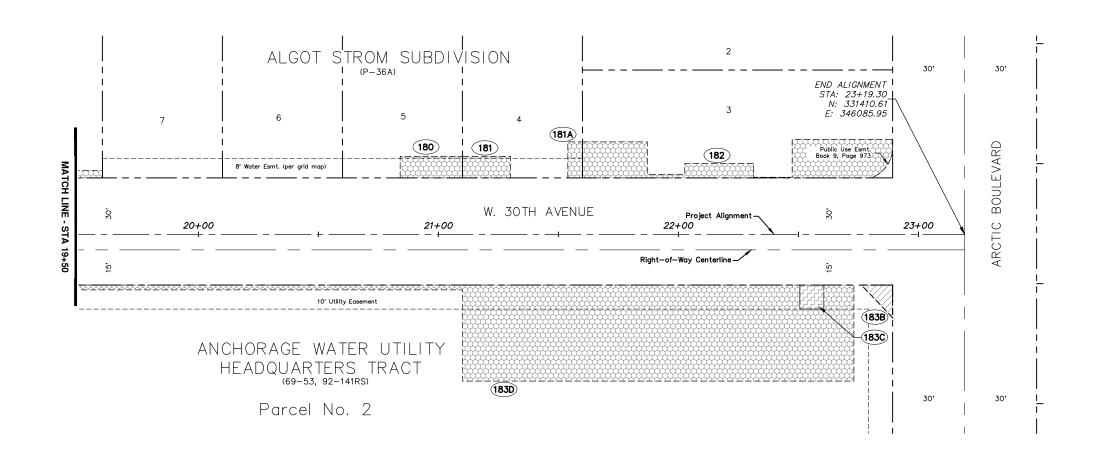
DATA TRANSFER CHECKED BY: __

EASEMENT & PERMIT INDEX MAP W. 30TH AVE - SPENARD ROAD TO STA 14+5

V9_{of} V13

SCALE HOR. 1"=20' GRID SW1629, SW1630 VER. N/A DATE NOV 2019 STATUS 65%





1 Parcel Number

Public Use Easement (PUE)

Drainage Easement (DE)

Temporary Construction Permit (TCP)

Temporary Construction Easement (TCE)

Existing Brass Cap or Copperweld

Existing Aluminum Cap

Existing Rebar or Iron Pipe

RE	CORD DRAWING		
1.	DATA PROVIDED BY:	 TITLE:	RASE
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	CONTRACTOR:		PROFILI STORM
	BY:		01011
	DATA TRANSFERRED BY:	 	
	COMPANY:		TELEPH
			ELECTR
	BASED ON PERIODIC FIELD OBSERVATIONS SUPERVISION). THE CONTRACTOR-PROVIDED		DESIGN
	DATA TRANSFER CHECKED BY:	 	QUANTI
	COMPANY:	 	PRELIM
	BY.	 DATE:	MUNICI

DATA	DRAWN BY	CHECKED			40 20	0	2	0	40)		
BASE	TS	MJ	GF	RAPHIC						SCALE		
TOPOGRAPHY	BW	BW	01	., ., ., .,	×					COTTEE		
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION		ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark	Book, Page D-56	94.77]7
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark	Book, Page D-17	106.10					11 -
GAS	RK	JK	STAKING	GAAB 86	6 See MOA Benchmark	Book, Page D-18	104.53					
TELEPHONE	RK	JK										ENGIN
ELECTRIC	JH	TK										
DESIGN	RB	JK	ASBUILT									3940 ANCH
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972	ADJUST						PH
PRELIMINARY/FINAL	RB	JK	INSPECTOR									1
MUNICIPAL/STATE	RB	JK										
PLAN CHECK CONSTRUCTION RECORD			VERTICAL DA	ATUM				REVISIONS				







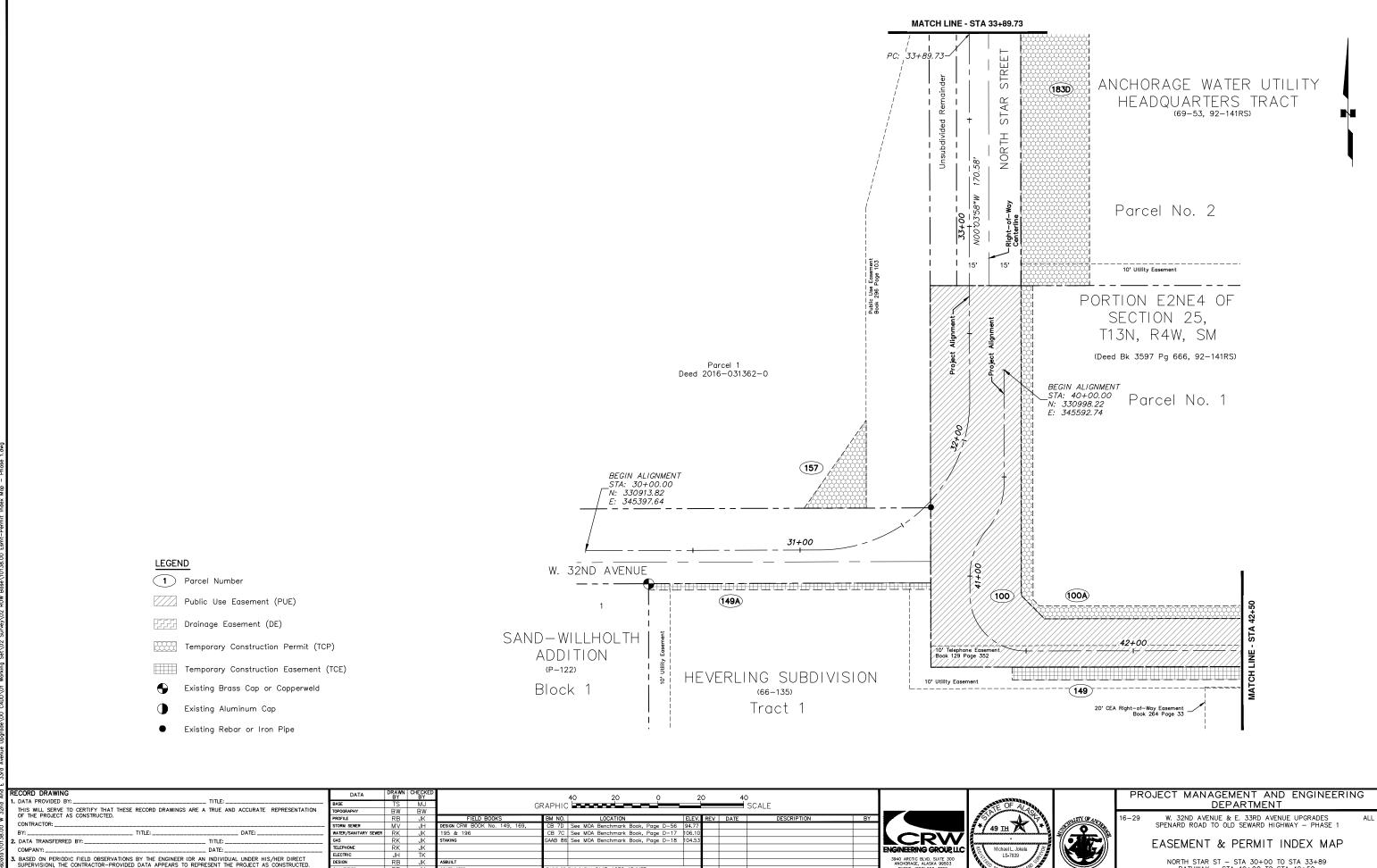
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

6-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

EASEMENT & PERMIT INDEX MAP

W. 30TH AVE - STA 19+50 TO ARCTIC BLVD

ı								
ı	00415	HOR.	1"=20'	GRID SW1629, SW1630			V11	
	SCALE	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET		"V13



DATA TRANSFER CHECKED BY: __

DATE:

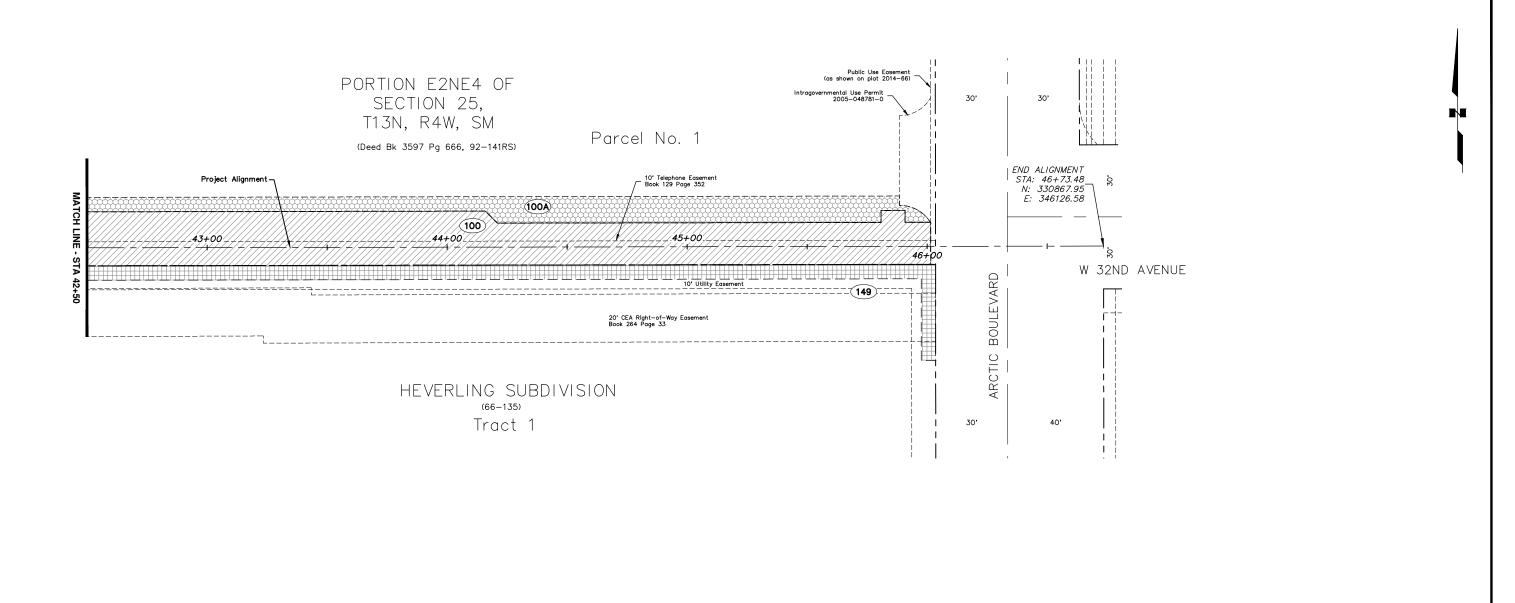
SIS OF THIS DATUM GAAB 1972 ADJUST





NORTH STAR ST - STA 30+00 TO STA 33+89 PATHWAY - STA 40+00 TO STA 42+50

GRID SW1629, SW1630 SCALE HOR. 1"=20' VER. N/A



1 Parcel Number

Public Use Easement (PUE)

Drainage Easement (DE)

Temporary Construction Permit (TCP)

Temporary Construction Easement (TCE)

Existing Brass Cap or Copperweld

Existing Aluminum Cap

• Existing Rebar or Iron Pipe

	COND DIVINING		
1.	DATA PROVIDED BY:	TITLE:	
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOP
	OF THE PROJECT AS CONSTRUCTED.		PRO
	CONTRACTOR:		STC
	BY: TITLE:	DATE:	WA [*]
2.	DATA TRANSFERRED BY:	TITLE:	GAS
	COMPANY:	DATE:	TEL
٠,	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN	INDIVIDUAL LINDED LUC (LED DIDECT	ELE
٥.	SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRE		DES
	DATA TRANSFER CHECKED BY:		QU
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BASE	TS	MJ	G	RAPHIC						S	CALE		1
TOPOGRAPHY	BW	BW	9	10/11/11/0							37122		
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOC	ATION		ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	ΜV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Bend	hmark Book	, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Bend	hmark Book	, Page D-17	106.10					ш
GAS	RK	JK	STAKING	GAAB 86	See MOA Bend	hmark Book	, Page D-18	104.53					ı
TELEPHONE	RK	JK											EN
LECTRIC	JH	TK											
DESIGN	RB	JK	ASBUILT										31
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB	1972 ADJU	ST						1
PRELIMINARY/FINAL	RB	JK	INSPECTOR										1
MUNICIPAL/STATE	RB	JK											
DIAN C	CHECK		CONCEDICTION DECORD	1	VEDTI	CAL DATUM					DEVICIONS		







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

EASEMENT & PERMIT INDEX MAP

PATHWAY - STA 42+50 TO ARCTIC BLVD

3CALE HOR. 1"=20' GRID SW1629, SW1630 VER. N/A DATE NOV 2019

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 2 REMOVE SIDEWALK OR CONCRETE APRON (SECTION 20.07).
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 7 REMOVE AND RESET FENCE (SECTION 70.08).
- REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 70.11).
- (1) RELOCATE MAILBOX (SECTION 70.17).

DATA TRANSFER CHECKED BY: __

- 12) REMOVAL/DISPOSAL AND/OR SALVAGE OF OBSTRUCTIONS (SECTION 70.22).
- REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
- PROTECT IN PLACE.
- 18 REMOVE AND RELOCATE SIGNS (SECTION 70.10).
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- -- APPROXIMATE LIMITS OF DISTURBANCE
- · \ \ REMOVE PIPE
- ** TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE SHEET B3 FOR DETAIL.

- 1. SEE SUMMARY TABLE SHEETS B5 B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
- 2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.
- 3. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

. DATA PROVIDED BY: _ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: COMPANY: _ DATE: _ . BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATE:

60 SCALE GRAPHIC LANGE TO THE STATE OF T LOCATION B 7D See MOA Benchmark Book, Page D-56
B 7C See MOA Benchmark Book, Page D-17 AAB 86 See MOA Benchmark Book, Page D-18 104 SIS OF THIS DATUM GAAB 1972 ADJUS







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

DEMOLITION PLAN

W. 30TH AVE - SPENARD ROAD TO STA 16+00

	HOR.	1"=30'	GRID SW1629, SW1630		B1./
LE	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET 7

- (1) CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 2 REMOVE SIDEWALK OR CONCRETE APRON (SECTION 20.07).
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 5 DECOMMISSION FIRE HYDRANT ASSEMBLY (SINGLE PUMPER) (SECTION 60.08).
- 6 REMOVE PIPE (SECTION 70.07).
- (7) REMOVE AND RESET FENCE (SECTION 70.08).
- 8 REMOVE FENCE (SECTION 70.08).
- (9) REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 70.11).
- 10 REMOVE BOLLARD (SECTION 70.13).

- (3) REMOVE LUMINAIRE POLE (SECTION 80.28).
- (4) REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
- PROTECT IN PLACE.
- B REMOVE EXISTING SANITARY SEWER CLEANOUT (SECTION 50.06).
- 17) REMOVE AND RESET GATE (SECTION 70.08).
- (18) REMOVE AND RELOCATE SIGNS (SECTION 70.10).
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- -- APPROXIMATE LIMITS OF DISTURBANCE
- · \ \ REMOVE PIPE
- ** ** TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE SHEET B3 FOR DETAIL.

SCALE

NOTES:

- 1. SEE SUMMARY TABLE SHEETS B5 B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
- 2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.
- 3. CONTRACTOR SHALL REMOVE THE EXISTING CONNEX TO A LOCATION ON PARCEL 183 PROPERTY
 ACCEPTABLE TO PROPERTY OWNER AS REQUIRED TO CONSTRUCT PROPOSED IMPROVEMENTS ON
 PARCEL 183. ONCE PARCEL 183 PROPOSED IMPROVEMENTS ARE COMPLETE AND HAVE BEEN
 ACCEPTED BY ENGINEER, CONTRACTOR SHALL RE—INSTALL CONNEX TO EXISTING LOCATION AS
 SHOWN OR A LOCATION ON PARCEL 183 ACCEPTABLE TO PARCEL 183 PROPERTY OWNER. THIS
 WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

RECORD DRAWING

COMPANY:

DATE:

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY:

DATE:

ENGINEERING GROUP, LIC 3940 ARCITE BLVD. SUITE 300 ANCHORACE, ALASKA 995.03 PHONE: (907) 562–2322 AECL.882–AK





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

DEMOLITION PLAN

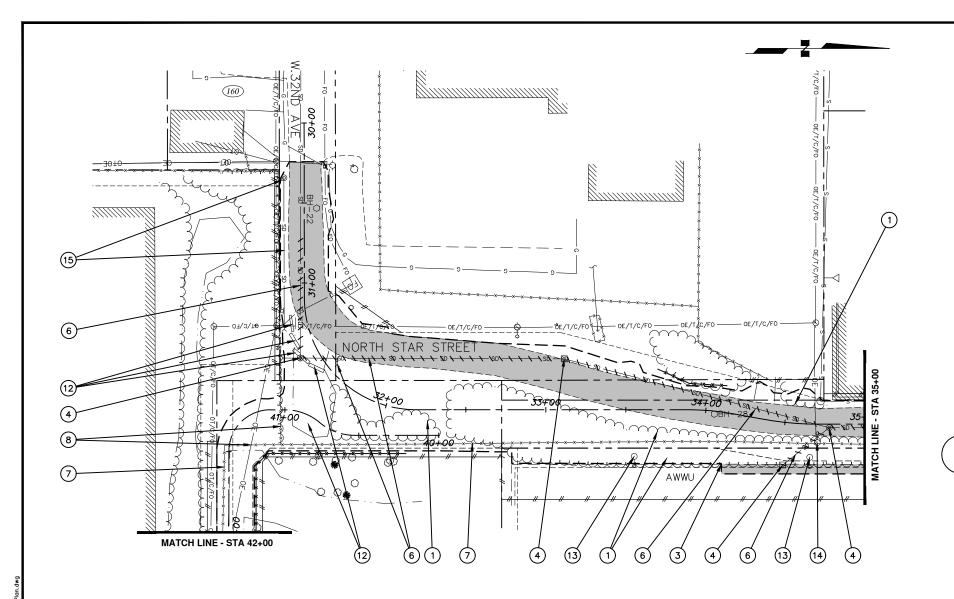
W. 30TH AVE - STA 16+00 TO ARCTIC BLVD NORTH STAR ST - STA 35+00 TO 36+46

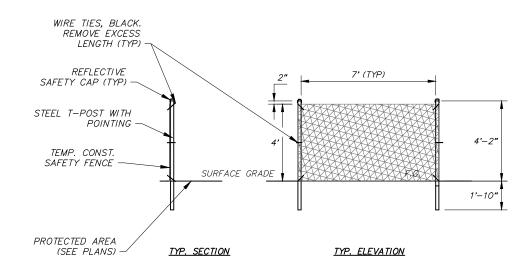
HOR. 1"=30' GRID swi629, swi630

VER. N/A

DATE NOV. 2010. STATUS. 65%

B2_{of} B7





TEMPORARY TREE PROTECTION FENCE DETAIL

SCALE: NTS

LEGEND

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 6 REMOVE PIPE (SECTION 70.07).
- 7 REMOVE AND RESET FENCE (SECTION 70.08).
- 8 REMOVE FENCE (SECTION 70.08).

- 12 REMOVAL/DISPOSAL AND/OR SALVAGE OF OBSTRUCTIONS (SECTION 70.22).
- (3) REMOVE LUMINAIRE POLE (SECTION 80.28).
- REMOVE UTILITY POLE OR LUMINAIRE ARM (BY OTHERS).
- 15) PROTECT IN PLACE.
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- - APPROXIMATE LIMITS OF DISTURBANCE
- · \ \ REMOVE PIPE
- ----- TEMPORARY TREE PROTECTION FENCE (SECTION 75.12), LOCATIONS TO BE FIELD VERIFIED, SEE THIS SHEET FOR DETAIL.

- 1. SEE SUMMARY TABLE SHEETS B5 B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
- 2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

. DATA PROVIDED BY:

DATA TRANSFER CHECKED BY: __

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR:

COMPANY: _ DATE: _ . BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATE:

60 SCALE GRAPHIC LOCATION CB 7D | See MOA Benchmark Book, Page D-56 CB 7C | See MOA Benchmark Book, Page D-17 AAB 86 See MOA Benchmark Book, Page D-18 104 SIS OF THIS DATUM GAAB 1972 ADJUS

CRW ENGINEERING GROUP LLC





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

DEMOLITION PLAN

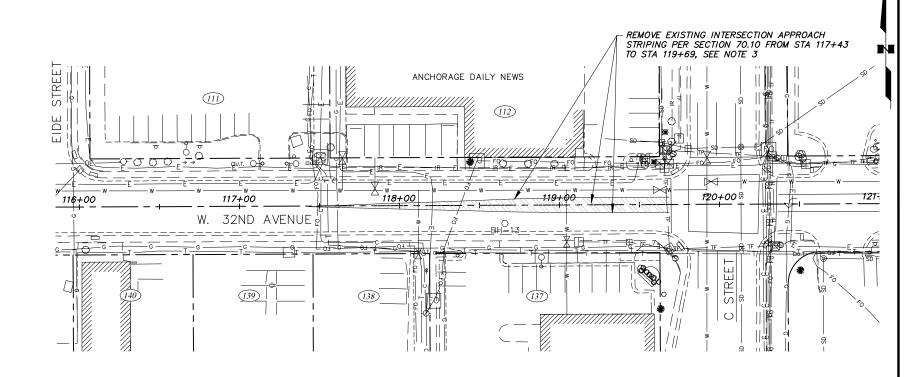
NORTH STAR ST - STA 30+24 TO STA 35+00 PATHWAY - STA 40+00 TO 42+00

GRID SW1629, SW1630 HOR. 1"=30"

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TEMPORARY TREE PROTECTION FENCES (SECTION 75.12) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
- 2 REMOVE SIDEWALK OR CONCRETE APRON (SECTION 20.07).
- 3 REMOVE CURB AND GUTTER (SECTION 20.08).
- 4 REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
- 6 REMOVE PIPE (SECTION 70.07).
- 7 REMOVE AND RESET FENCE (SECTION 70.08).
- REMOVE FENCE (SECTION 70.08).
- REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 70.11).
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
- - APPROXIMATE LIMITS OF DISTURBANCE
- · \ \ REMOVE PIPE

RECORD DRAWING

SHEET B3 FOR DETAIL.

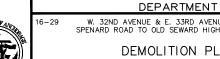


٦	1.	DATA PROVIDED BY:		TITLE:
V 32n		THIS WILL SERVE TO CERTIFY THAT THESE OF THE PROJECT AS CONSTRUCTED. \ensuremath{TH}	RECORD DRAWINGS ARE A	TITLE: TRUE AND ACCURATE REPRESENTATION
2		CONTRACTOR:		
Š.				DATE:
13	2.	DATA TRANSFERRED BY:		TITLE:
ž		COMPANY:		DATE:
\JobsData\10138.00	3.	BASED ON PERIODIC FIELD OBSERVATIONS SUPERVISION), THE CONTRACTOR-PROVIDED	BY THE ENGINEER (OR AN DATA APPEARS TO REPRE	INDIVIDUAL UNDER HIS/HER DIRECT SENT THE PROJECT AS CONSTRUCTED.
힞		DATA TRANSFER CHECKED BY:		TITLE:
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PROFILE	RB	JK	FIELD BOOKS	BM NO.		LOCATION		ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA	Benchmark Book,	Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA	Benchmark Book,	Page D-17	106.10					II -
GAS	RK	JK	STAKING	GAAB 86	See MOA	Benchmark Book,	Page D-18	104.53					
TELEPHONE	RK	JK											ENGINE
ELECTRIC	JH	TK											3940 A
DESIGN	RB	JK	ASBUILT										ANCHO
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PRELIMINARY/FINAL	RB	JK	INSPECTOR										i
MUNICIPAL/STATE	RB	JK											
PLAN (CHECK	·	CONSTRUCTION RECORD		·	VERTICAL DATUM		·			REVISIONS		







1. SEE SUMMARY TABLE SHEETS B5 - B7 FOR STATION AND OFFSET OF DEMOLITION ITEMS. 2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

3. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

DEMOLITION PLAN

PROJECT MANAGEMENT AND ENGINEERING

PATHWAY - STA 42+00 TO ARCTIC BLVD W. 32ND AVE - STA 115+85 TO STA 121+00

A1 E	HOR.	1"=30'	GRID SW1629, SW1630		B4 . /
ALE	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET 50 B7

REMOVE	SIDEWALK OR CONCR	ETE APRON				2
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	AREA (SY)	REMARKS
B1	10+28.9	38.0 RT	10+36.0	29.1 RT	9	W. 30TH AVE AND SPENARD ROAD
B1	10+45.4	27.3 LT	10+54.7	24.0 LT	11	W. 30TH AVE AND SPENARD ROAD
B1	11+43.3	17.6 RT	11+50.8	17.6 RT	2	W. 30TH AVE, PARCEL 191
B1	12+14.5	34.8 LT	12+50.8	34.8 LT	49	PARCEL 167 DWY
B1	12+96.7	21.4 LT	13+08.4	20.8 LT	6	W. 30TH AVE, PARCEL 168
B2	16+72.9	24.3 LT	16+93.5	24.2 LT	8	PARCEL 175
B2	18+99.7	22.0 LT	19+07.6	21.7 LT	20	W. 30TH AVE, PARCEL 177
B2	21+56.2	20.5 RT	21+67.7	15.1 RT	4	W. 30TH AVE, PARCEL 183
B2	21+82.1	18.8 LT	21+86.4	17.9 LT	1	W. 30TH AVE, PARCEL 183
B2	21+93.6	22.0 LT	22+34.2	21.9 LT	18	W. 30TH AVE, PARCEL 182
B2	22+57.3	21.7 RT	22+59.2	21.7 RT	1	PARCEL 183 SPILLWAY
B2	22+69.3	23.0 LT	22+89.6	33.5 LT	18	W. 30TH AVE AND ARCTIC BLVD
B2	22+86.1	21.8 RT	22+89.6	33.5 RT	8	W. 30TH AVE AND ARCTIC BLVD
B4	46+02.3	7.4 LT	46+06.4	42.1 RT	29	ARCTIC BLVD
B4	46+62.9	42.2 RT	46+63.4	26.2 RT	9	ARCTIC BLVD

REMOVE	CURB AND GUTTER	?				3
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	LENGTH (FT)	REMARKS
B1	10+24.1	37.5 RT	10+50.7	21.6 RT	35	W. 30TH AVE
B1	10+40.7	21.0 LT	10+56.5	15.6 LT	17	W. 30TH AVE
B1	11+33.5	16.8 LT	11+46.5	16.8 LT	13	W. 30TH AVE
B1	11+56.7	16.8 LT	11+62.5	16.8 LT	6	W. 30TH AVE
B1	12+84.9	16.0 RT	13+25.4	16.0 RT	41	W. 30TH AVE
B1	12+96.6	16.8 LT	13+42.4	17.0 LT	46	W. 30TH AVE
B1	13+44.3	16.1 RT	13+52.8	16.1 RT	8	W. 30TH AVE
B1	15+06.7	17.3 LT	15+54.7	17.2 LT	39	W. 30TH AVE
B1	15+07.8	15.5 RT	15+38.7	15.5 RT	31	W. 30TH AVE
B1	15+60.2	17.4 LT	15+82.7	17.2 LT	23	W. 30TH AVE
B2	16+02.4	17.3 LT	16+22.2	17.4 LT	20	W. 30TH AVE
B2	16+23.1	15.7 RT	16+88.2	15.4 RT	65	W. 30TH AVE
B2	16+46.7	17.4 LT	17+11.6	17.5 LT	65	W. 30TH AVE
B2	17+23.9	15.5 RT	17+92.8	15.0 RT	69	W. 30TH AVE
B2	17+36.9	17.4 LT	17+89.7	17.4 LT	53	W. 30TH AVE
B2	18+29.3	17.5 LT	18+45.2	17.6 LT	16	W. 30TH AVE
B2	18+38.7	16.5 RT	21+24.0	32.5 RT	376	W. 30TH AVE
B2	18+73.7	17.7 LT	20+82.1	17.9 LT	208	W. 30TH AVE
B2	21+20.3	17.8 LT	21+64.7	17.9 LT	41	W. 30TH AVE
B2	21+53.1	32.5 RT	21+60.1	28.0 RT	18	W. 30TH AVE
B2	21+66.4	23.4 RT	22+94.8	33.5 RT	156	W. 30TH AVE
B2	21+86.6	17.9 LT	22+04.5	18.0 LT	18	W. 30TH AVE
B2	22+21.7	18.2 LT	22+94.5	33.5 LT	81	W. 30TH AVE
B2	22+47.6	23.7 RT	22+59.0	31.6 RT	20	PARCEL 183
B2	35+00.0	26.9 RT	3+62.6	26.6 RT	103	PARCEL 183
B3	34+15.8	34.5 RT	35+00.0	26.9 RT	90	PARCEL 183
	31113.3	3 1.3 111	30100.0	20.0 111		
B4	46+06.4	12.7 LT	46+08.9	42.1 RT	55	ARCTIC BLVD
В4	46+57.9	42.2 RT	46+58.6	25.2 RT	17	ARCTIC BLVD

20.09

REMOVE PAVEMENT								
SHEET	STATION TO STATION	OFFSET	AREA (SY)	REMARKS				
B1	BOP TO 16+00	LT & RT	3,058	W. 30TH AVE, DRIVEWAYS				
B2	16+00 TO EOP	LT & RT	3,428	W. 30TH AVE, NORTH STAR ST, ARCTIC BLVD, DWYS				
В3	30+24 TO 35+00	LT & RT	1,099	NORTH STAR ST, PARKING AREA				
B4	42+00 TO EOP	LT & RT	211	ARCTIC BLVD				

1. SEE ROADWAY IMPROVEMENT (R) SHEETS FOR ROADWAY PAVEMENT REMOVAL LIMITS.

2. SEE DRIVEWAY RECONSTRUCTION TABLE ON (T) SHEETS FOR DRIVEWAY REMOVAL LIMITS.

50	06
\sim \sim	

REMOVE EXISTING SANITARY SEWER CLEANOUT (6)							
SHEET	STATION	OFFSET (FT)	REMARKS				
B2	22+87.2	5.3 LT					

55.11

REMOVE MA	REMOVE MANHOLE OR CATCH BASIN							
SHEET	APPX STATION	APPX OFFSET (FT)	CATCH BASIN	MANHOLE	REMARKS			
B1	10+41.6	20.6 RT	Χ					
B2	18+20.5	3.4 RT		X				
B2	18+20.9	18.0 LT	X					
B2	18+39.9	15.6 RT	X					
B2	22+74.7	17.6 LT	Х					
В3	31+42.7	11.8 RT		X				
В3	33+11.5	31.8 LT		X				
В3	34+51.3	26.7 RT	X					
В3	34+51.3	26.7 RT	X					
В3	34+77.7	2.2 RT		X				
B4	45+77.83	0.9 LT	Χ					

1. DATA PROVIDED BY: ___ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR:

2. DATA TRANSFERRED BY: ______ _____ TITLE: _____

DATA TRANSFER CHECKED BY: ____ COMPANY: _ _ DATE:_

DATA	BY	BY	
BASE	TS	MJ	
TOPOGRAPHY	BW	BW	
PROFILE	RB	JK	
STORM SEWER	MV	JH	DESIGN C
WATER/SANITARY SEWER	RK	JK	195 &
GAS	RK	JK	STAKING
TELEPHONE	RK	JK	
ELECTRIC	JH	TK	
DESIGN	RB	JK	ASBUILT
QUANTITIES	RB	JK	CONTRAC
PRELIMINARY/FINAL	RR	.lK	INSPECTO

	ום	D1	
	TS	MJ	
	BW	BW	
	RB	JK	FIELD BOO
	MV	JH	DESIGN CRW BOOK No.
₹	RK	JK	195 & 196
	RK	JK	STAKING
	RK	JK	
	JH	TK	
	RB	JK	ASBUILT
	RB	JK	CONTRACTOR
	RB	JK	INSPECTOR
_	0.0	11/	

 BM NO.
 LOCATION
 ELEV.

 CB 7D
 See MOA Benchmark Book, Page D-56
 94.77

 CB 7C
 See MOA Benchmark Book, Page D-17
 106.53

 GAAB 86
 See MOA Benchmark Book, Page D-18
 104.53
 ASIS OF THIS DATUM GAAB 1972 ADJUST







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

DEMOLITION SUMMARY TABLES

GRID SW1629, SW1630

DATE NOV 2019 STATUS 65% B5_{of}B7 SCALE HOR. N/A

60.08

DECOMMISS	ION FIRE HYDRA	NT ASSEMBLY		5
SHEET	STATION	OFFSET (FT)	REMARKS	
B1	16+25.2	21.7 RT	HYDRANT VALVE & LEG SHALL REMAIN IN PLACE	

70.07

70.07							
REMOVE P	PIPE						6
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	SIZE (INCH)	LENGTH (FT)	REMARKS
B2	18+04.7	5.0 LT	18+18.3	13.7 RT	8	23.2	SEWER
B2	18+20.5	3.4 RT	18+20.9	18.0 LT	8	21.4	STORM DRAIN
B2	18+20.5	3.4 RT	18+39.9	15.6 RT	10	22.9	STORM DRAIN
B2	22+68.5	5.4 LT	22+87.2	5.4 LT	8	18.7	SEWER
B2	22+74.7	17.6 LT	23+19.6	16.6 LT	12	44.8	STORM DRAIN
B2	35+00.0	2.1 RT	36+42.2	1.9 RT	12	142.2	STORM DRAIN
В3	30+69.3	2.4 RT	31+42.7	11.8 RT	15	77.6	STORM DRAIN
В3	31+42.7	11.8 RT	33+11.5	31.8 LT	15	165.0	STORM DRAIN
В3	31+54.8	7.8 LT	31+58.3	5.2 LT	12	4.0	STORM DRAIN
В3	33+11.5	31.8 LT	34+77.7	2.2 RT	12	171.0	STORM DRAIN
В3	34+51.3	26.7 RT	34+77.7	2.2 RT	12	37.3	STORM DRAIN
В3	34+77.7	2.2 RT	35+00.0	1.9 RT	12	22.3	STORM DRAIN
					•		
B4	45+77.8	0.9 LT	46+09.9	21.9 RT	12	39.4	STORM DRAIN

70.08

REMOVE FENCE									
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	REMARKS			
В3	41+40.6	14.3 LT	41+46.3	2.9 RT	17.7	CHAIN W/ BARBED WIRE			
В3	41+46.3	2.9 RT	41+58.6	3.9 RT	12.7	CHAIN W/ BARBED WIRE			

70.08

REMOVE	AND RESET F	ENCE								9
		EXISTING LO	OCATION			PROPOSED I	LOCATION			
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	REMARKS
B1	12+88.4	23.0 RT	13+22.5	23.3 RT	12+88.3	24.5 RT	13+22.5	24.5 RT	34.2	WOOD
B1	13+09.2	20.4 LT	13+39.5	20.4 LT	13+10.1	23.5 LT	13+39.5	23.5 LT	29.4	CHAIN
B1	13+45.9	23.3 RT	13+66.0	23.6 RT	13+45.9	24.5 RT	13+66.0	24.5 RT	20.1	CHAIN
B1	15+37.3	23.7 LT	15+52.9	23.6 LT	15+37.4	23.9 LT	15+53.0	23.8 LT	15.6	CHAIN
B1	15+64.0	23.8 LT	15+82.7	23.7 LT	15+58.8	24.0 LT	15+82.8	24.0 LT	24.1	CHAIN
B2	19+07.6	23.5 LT	19+63.8	21.6 LT	19+07.7	23.5 LT	19+60.1	23.5 LT	52.4	CHAIN
B2	18+41.8	37.7 RT	18+49.2	37.6 RT	18+41.8	37.5 RT	18+49.2	37.5 RT	7.4	CHAIN
B2	21+61.8	21.8 RT	21+62.0	21.6 RT	21+62.0	21.6 RT	21+68.0	21.8 RT	6.0	CHAIN
B2	22+47.6	22.0 RT	22+62.1	22.3 RT	22+47.6	21.9 RT	22+62.2	22.1 RT	14.6	WOOD
B2	22+62.1	22.3 RT	22+64.5	26.6 RT	22+62.2	22.1 RT	22+64.6	26.5 RT	5.0	WOOD
B2	35+00.0	12.5 RT	36+21.9	15.2 RT	35+00.0	20.3 RT	36+14.4	20.3 RT	114.5	CHAIN W/BARBED WIRE
B3	31+64.4	59.6 RT	33+85.5	19.8 RT	31+65.2	61.1 RT	33+85.5	24.0 RT	221.1	CHAIN W/BARBED WIRE
В3	33+85.5	19.8 RT	33+89.0	19.8 RT	33+85.5	24.0 RT	33+89.0	20.5 RT	4.9	CHAIN W/BARBED WIRE
В3	33+89.0	19.8 RT	35+00.0	12.5 RT	33+89.0	20.5 RT	35+00.0	20.3 RT	113.9	CHAIN W/BARBED WIRE
В3	41+58.6	2.9 RT	42+00.0	3.9 RT	41+58.6	15.0 LT	42+00.0	15.0 LT	41.4	CHAIN W/BARBED WIRE
В3	41+10.2	21.8 LT	41+40.6	14.3 LT	41+11.2	24.0 LT	41+58.6	15.0 LT	15.3	CHAIN W/BARBED WIRE
	10.000	7.0.07		0.7.57	40.000	45.0.1.7	11.10.5	45.0.1.	242.5	OLIANA W /DADDED WAS
B4	42+00.0	3.9 RT	44+15.4	9.3 RT	42+00.0	15.0 LT	44+16.0	15.0 LT	216.0	CHAIN W/BARBED WIRE
B4	44+15.4	9.3 RT	44+20.6	9.4 RT	44+16.0	15.0 LT	44+21.0	10.0 LT	7.1	CHAIN W/BARBED WIRE
B4	44+20.6	9.4 RT	45+35.3	11.9 RT	44+21.0	10.0 LT	+45.5	10.0 LT	133.3	CHAIN W/BARBED WIRE

NOTES:

- 1. PROVIDE TEMPORARY FENCING PER SECTION 70.24 FOR ALL FENCES REMOVED OR AS DIRECTED BY THE ENGINEER.
- 2. STAKE RESET FENCE LAYOUT IN THE FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.08 PAY ITEM.

70.08

REMOVE	REMOVE AND RESET GATE										
	EXISTING LOCATION			PROPOSED LOCATION							
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	REMARKS	
B2	21+18.1	28.7 RT	21+26.6	29.1 RT	21+18.2	28.5 RT	21+26.7	28.9 RT	8.5	CHAIN, LT GATE SECTION	
B2	21+53.1	27.6 RT	21+61.8	21.8 RT	21+50.8	29.2 RT	21+62.0	21.6 RT	13.5	CHAIN, RT GATE SECTION	

NOTE:

1. STAKE RESET GATE LAYOUT IN THE FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 70.08 PAY ITEM.

RE	CORD DRAWING				
1.	DATA PROVIDED BY:		TITLE:		BAS
	THIS WILL SERVE TO CERTIFY THAT THESE	RECORD DRAWINGS ARE A	TRUE AND ACCURATE	REPRESENTATION	TOP
	OF THE PROJECT AS CONSTRUCTED.				PRO
	CONTRACTOR:				STC
	BY:	TITLE:	DATE:		WA.
2.	DATA TRANSFERRED BY:		TITLE:		GAS
	COMPANY:		DATE:		TEL
3.	BASED ON PERIODIC FIELD OBSERVATIONS	BY THE ENGINEER (OR AN	INDIVIDUAL UNDER HIS	HER DIRECT	DES
	SUPERVISION), THE CONTRACTOR-PROVIDED	DATA APPEARS TO REPRE	SENT THE PROJECT AS		QUA
	DATA TRANSFER CHECKED BY:		TITLE:		PRE
	COMPANY:		DATE:		MIII

DATA	DRAWN BY	CHECKED									
BASE	TS	MJ									i
TOPOGRAPHY	BW	BW									
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					11
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
TELEPHONE	RK	JK									ENG
ELECTRIC	JH	TK									394
DESIGN	RB	JK	ASBUILT								394 A
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						1
PRELIMINARY/FINAL	RB	JK	INSPECTOR								1
MUNICIPAL/STATE	RB	JK									
PLAN C	HECK		CONSTRUCTION RECORD		VERTICAL DATUM			·	REVISIONS	The second second	

ENGINEERING GROUP LIC
3940 ARCITIC BLVD. SUITE 300
ANOHORAGE, ALASKA 9950.3
PHONE: GROUP 3522-2522





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

DEMOLITION SUMMARY TABLES

SCALE HOR. N/A GRID SWI629, SWI630 B6 Of B7 STATUS 65% SHEET B6

70.11						
REMOVE A	ND SALVAG	E SIGN				9
SHEET	APPX STATION	APPX OFFSET (FT)	SIGN TYPE	LEGEND	SIGN POST	REMARKS
			R1-1	STOP		
B1	10+50	27.4 LT	D3-1D	SPENARD RD	PERFORATED STEEL TUBE	
			D3-1D	W 30TH AVE		
B1	12+92	17.7 RT	R2-1	MAXIMUM SPEED 20	PERFORATED STEEL TUBE	
			R1-1	STOP		
B2	18+31	22.2 RT	D3-1D	NORTH STAR ST	PERFORATED STEEL TUBE	
			D3-1D	W 30TH AVE		
B2	19+60	20.0 LT	SPECIAL	NO PARKING 8AM TO 5PM	PERFORATED STEEL TUBE	
BZ BZ	19+60	20.0 L1	SPECIAL	ARROWS	PERFORATED STEEL TOBE	
B2	21+37	21.7 LT	R2-1	MAXIMUM SPEED 20	PERFORATED STEEL TUBE	
B2	21+89	22.3 LT	SPECIAL	NO PARKING 8AM TO 5PM	PERFORATED STEEL TUBE	
DZ.	21+69	22.5 L1	SPECIAL	ARROWS	PERFORATED STEEL TOBE	
			R1-1	STOP		
B2	22+83	19.6 RT	D3-1D	ARCTIC BLVD	PERFORATED STEEL TUBE	
			D3-1D	W 30TH AVE		
B4	102+33	25.1 LT	SPECIAL	NO PARKING BETWEEN SIGNS	PERFORATED STEEL TUBE ON WOODEN BOLLARD	WOODEN BOLLARD TO REMAIN
В4	102+58	25.5 LT	SPECIAL	NO PARKING BETWEEN SIGNS	PERFORATED STEEL TUBE ON WOODEN BOLLARD	WOODEN BOLLARD TO REMAIN

NOTE: WORK TO REMOVE AND SALVAGE EXISTING SIGNS & POSTS SHALL BE INCIDENTAL TO SECTION 70.11 STANDARD SIGN PAY ITEM.

70	4	4	
70	. І	- 1	

REMOVE A	REMOVE AND RELOCATE SIGN (18)												
SHEET	APPX STATION	APPX OFFSET (FT)	SIGN TYPE	LEGEND	SIGN POST	REMARKS							
B1	15+83	23.8 LT	R1-1	STOP	PERFORATED STEEL TUBE	ON PARCEL 173							
B2	18+58	26.4 RT	SPECIAL	MAIN ENTRANCE CUSTOMER PARKING	PERFORATED STEEL TUBE	ON PARCEL 183							
B2	10+56	20.4 KT	SPECIAL	MAIN ENTRANCE CUSTOMER PARKING	PERFORATED STEEL TOBE	ON PARCEL 163							
B2	18+93	27.8 RT	R1-1	STOP	PERFORATED STEEL TUBE	ON PARCEL 183							
B2	21+55	31.5 RT	R1-1	STOP	PERFORATED STEEL TUBE	ON PARCEL 183							

NOTE: INSTALL RELOCATED SIGN IN SAME APPROXIMATE STATION AND OFFSET AS THE EXISTING LOCATION. STAKE RELOCATED SIGN LOCATION FOR ENGINEER'S APPROVAL PRIOR TO INSTALLING. THIS WORK SHALL BE INCIDENTAL TO THIS PAY ITEM.

70.13

REMOVE BOLL	ARD		@
SHEET	STATION	OFFSET (FT)	REMARKS
B1	16+26.9	19.3 RT	STEEL
B1	16+23.1	19.9 RT	STEEL

70.17

RELOCATE MAILBOX								
E	XISTING LOCATION	N	NEW LC	CATION				
	APPX	APPX	APPX	APPX				
SHEET	STATION	OFFSET (FT)	STATION	OFFSET (FT)	REMARKS			
B1	13+14.5	17.8 RT	13+14.5	23.5 RT	PARCEL 189			

NOTE: SEE SHEET D5 FOR MAILBOX INSTALLATION DETAILS.

70.22

70.22				
REMOVAL/DIS	SPOSAL A	ND/OR SALV	AGE OF OBSTRUCTIONS	@
SHEET	APPX STATION	APPX OFFSET (FT)	OBSTRUCTION ITEM	ACTION
B1	15+28	16.7 RT	CONCRETE PLANTER	PLACE ON PARCEL 186
B1	15+69	19.4 LT	CONCRETE PEDESTAL	PLACE ON PARCEL 173
В3	31+23	9.8 RT	JERSEY BARRIER	SALVAGE AND DELIVER TO MOA ST MAINTENANCE
В3	31+33	10.7 RT	JERSEY BARRIER	SALVAGE AND DELIVER TO MOA ST MAINTENANCE
В3	31+42	10.7 RT	JERSEY BARRIER	SALVAGE AND DELIVER TO MOA ST MAINTENANCE
В3	31+52	6.3 RT	JERSEY BARRIER	SALVAGE AND DELIVER TO MOA ST MAINTENANCE
В3	40+86	10.9 LT	TRASH	HAUL OFF AND DISPOSE TRASH

80.28

REMOVE LUMINARE POLE								
TENIOVE EON		(13)						
	APPX	APPX						
SHEET	STATION	OFFSET (FT)	REMARKS					
В3	33+56.4	27.4 RT						
В3	34+66.4	20.7 RT						
В3	B3 35+77.8							

ΚE	ECORD DRAWING		
1.	DATA PROVIDED BY:		BAS
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOP
	OF THE PROJECT AS CONSTRUCTED.		PRO
	CONTRACTOR:		STC
	BY: TITLE:	DATE:	WA
2.	DATA TRANSFERRED BY:		GAS
	COMPANY:	DATE:	TEL ELE DES
	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN	INDIVIDUAL LINDED LUS (UED DIDECT	ELE
	SUPERVISION). THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRI	FSENT THE PROJECT AS CONSTRUCTED	DES
	DATA TRANSFER CHECKED BY:	TITLE	QU
	DATA TRANSFER CHECKED BT:		PRE

DATA	DRAWN BY	CHECKED BY									Г
BASE	TS	MJ									1
TOPOGRAPHY	BW	BW									ı.
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	п
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					П
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					Н
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					П
TELEPHONE	RK	JK									ŀ
ELECTRIC	JH	TK									I.
DESIGN	RB	JK	ASBUILT								1
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						ı
PRELIMINARY/FINAL	RB	JK	INSPECTOR								ı
MUNICIPAL/STATE	RB	JK									L
PLAN CHECK			CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS		Г







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

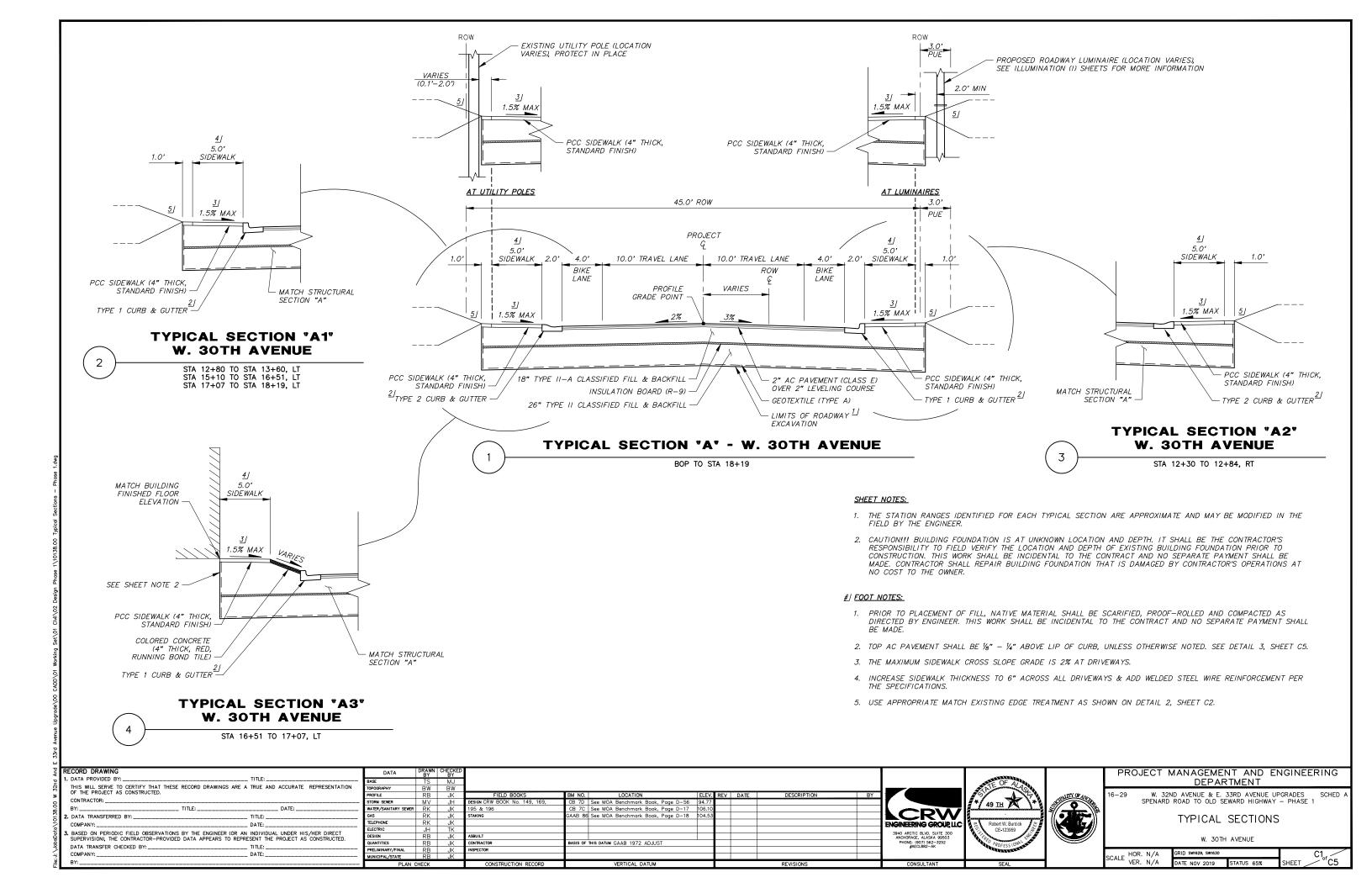
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

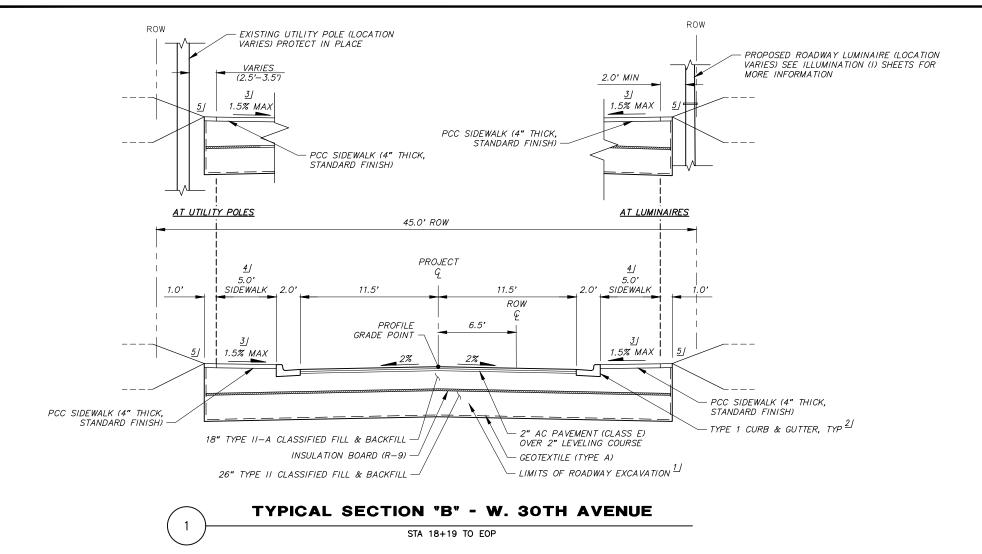
DEMOLITION SUMMARY TABLES

SCALE HOR. N/A GRID SW1629, SW1630

SCALE VER. N/A DATE NOV 2019 STATUS 65% SHEET B7

B7





SHEET NOTES:

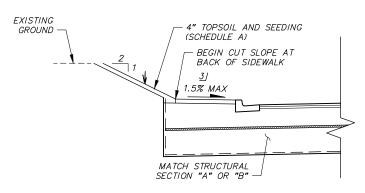
1. THE STATION RANGES IDENTIFIED FOR EACH TYPICAL SECTION ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.

| FOOT NOTES:

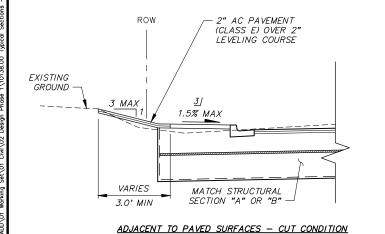
- 1. PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- 2. TOP AC PAVEMENT SHALL BE 1/8" 1/4" ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 3, SHEET C5.
- 3. THE MAXIMUM SIDEWALK CROSS SLOPE GRADE IS 2% AT DRIVEWAYS.
- 4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS ALL DRIVEWAYS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
- 5. USE APPROPRIATE MATCH EXISTING EDGE TREATMENT AS SHOWN ON DETAIL 2, THIS SHEET.
- 6. THE TYPICAL AND MAXIMUM CUT/FILL SLOPES ARE 2 (HORIZONTAL): 1 (VERTICAL). FILL SLOPES MAY VARY ALONG ROADWAY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. SEE DETAIL 2, SHEET C5. SEE ROADWAY SHEETS FOR LOCATIONS. THE ENGINEER MAY ADJUST THE TYPICAL SLOPES IN THE FIELD.

MATCH EXISTING EDGE TREATMENT NOTE:

1. MIRROR FOR RIGHT SIDE.



ADJACENT TO GRASSED AREAS WITH NO SHOULDER - CUT CONDITION STA 15+05 TO STA 15+43, LT



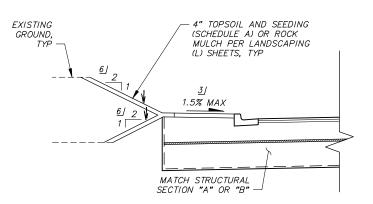
2" AC PAVEMENT
(CLASS E) OVER 2"
LEVELING COURSE

EXISTING
GROUND

1.0% MIN
1.5% MAX

VARIES
3.0' MIN

MATCH STRUCTURAL
SECTION "A" OR "B"



ADJACENT TO PAVED SURFACES - FILL CONDITION

ADJACENT TO GRASSED/ROCK MULCH AREAS - CUT/FILL CONDITION

W. 30TH AVENUE - TYPICAL MATCH EXISTING EDGE TREATMENT

RI	ECORD DRAWING		Ī
1.	DATA PROVIDED BY:		r
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A OF THE PROJECT AS CONSTRUCTED.	TRUE AND ACCURATE REPRESENTATION	ľ
	CONTRACTOR:		r
	BY: TITLE:	DATE:	ľ
2.	DATA TRANSFERRED BY:	_ TITLE:	Ĺ
	COMPANY:	DATE:	L
3.	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRE		F
	DATA TRANSFER CHECKER BY	TITLE:	L

DATE:

COMPANY:

DATA	DRAWN BY	CHECKED									1
BASE	TS	MJ									i
TOPOGRAPHY	BW	BW									
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					II.
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
TELEPHONE	RK	JK									ENGI
ELECTRIC	JH	TK									
DESIGN	RB	JK	ASBUILT								3940 AN
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						F
PRELIMINARY/FINAL	RB	JK	INSPECTOR								1
MUNICIPAL/STATE	RB	JK									
PLAN (CHECK		CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS		







PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SC SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

TYPICAL SECTIONS

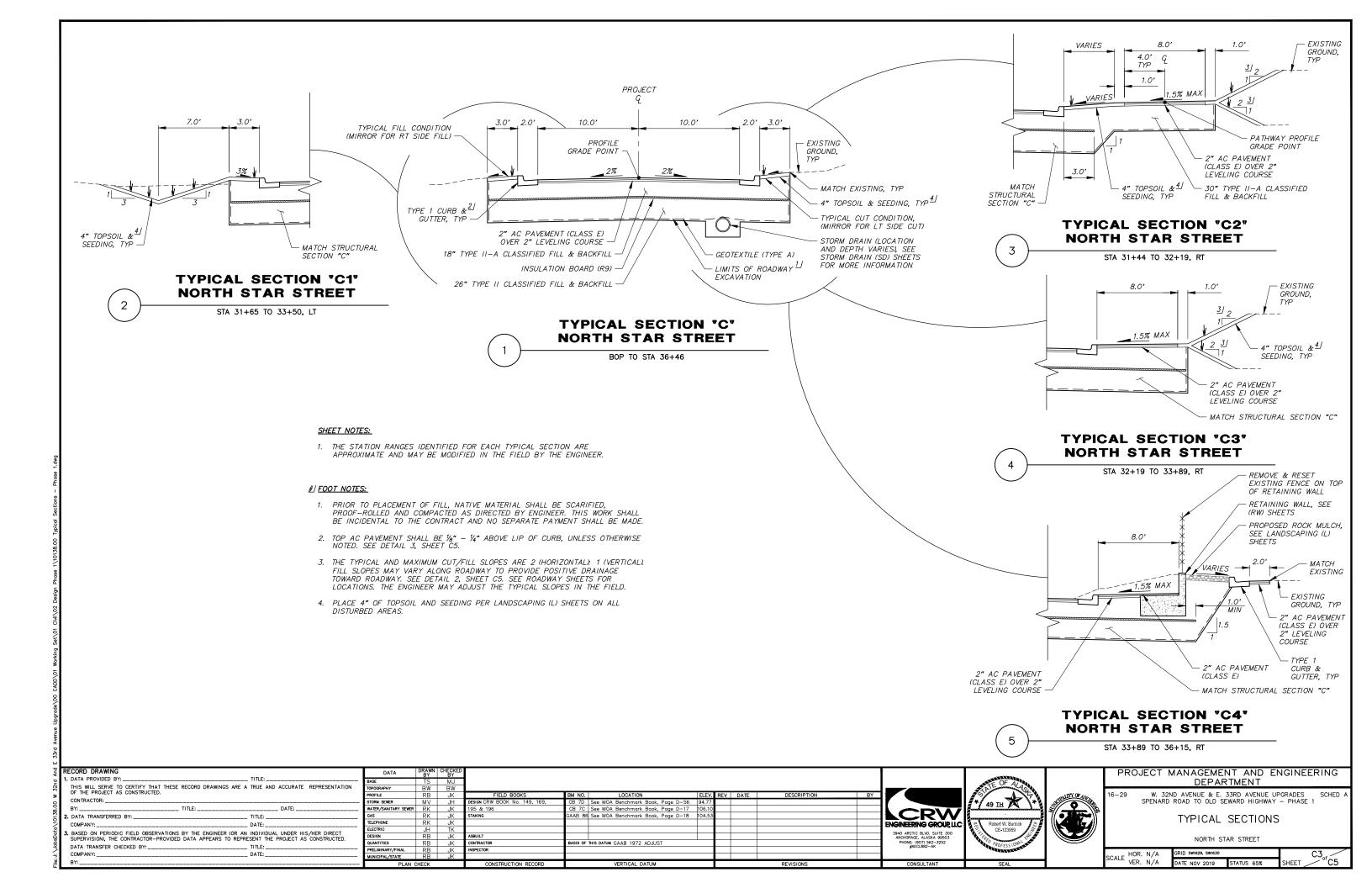
W. 30TH AVENUE

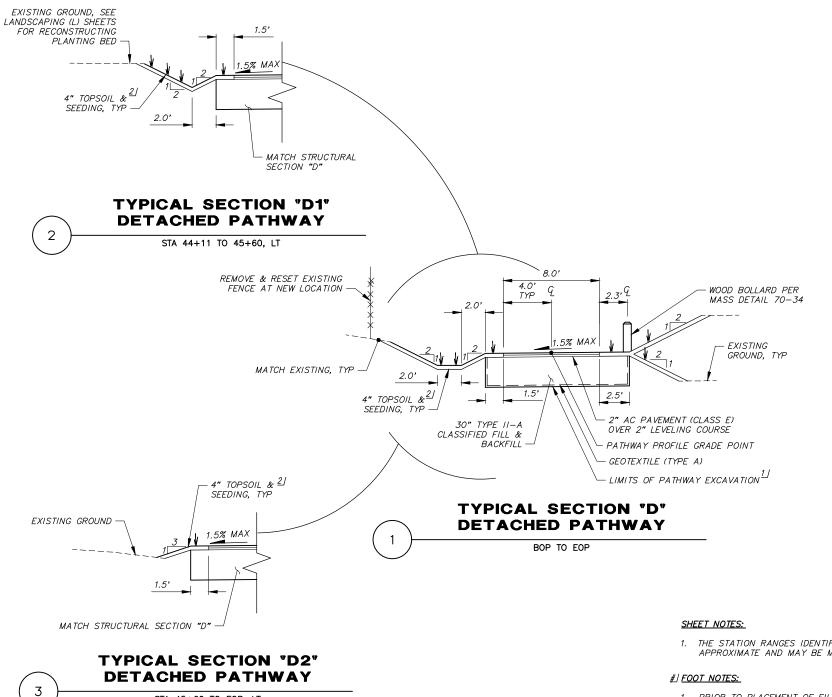
LE HOR. N/A GRID SW1629, SW1630

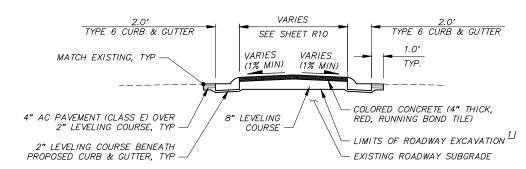
C2
of C5

SHEET

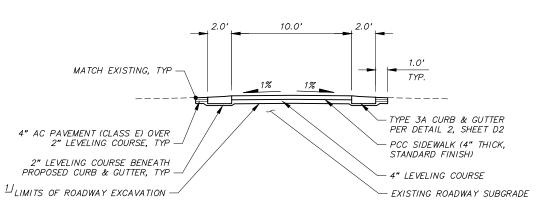
C5







TYPICAL RAISED MEDIAN SECTION



TYPICAL PEDESTRIAN REFUGE ISLAND SECTION

1. THE STATION RANGES IDENTIFIED FOR EACH TYPICAL SECTION ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.

- 1. PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- 2. PLACE 4" OF TOPSOIL AND SEEDING PER LANDSCAPING (L) SHEETS ON ALL DISTURBED AREAS.

5	R	ECORD DRAWING		
	١.	DATA PROVIDED BY:	TITLE.	
٦	١.			BASE
32nd		THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE OF THE PROJECT AS CONSTRUCTED.	E A TRUE AND ACCURATE REPRESENTATION	TOPOGR
≥				PROFIL
0		CONTRACTOR:		STORM
J: \JobsData\10138.00		BY: TITLE:	DATE:	WATER,
5	2.	DATA TRANSFERRED BY:	TITLE:	GAS
윈		COMPANY:	DATE:	TELEPH
ē	١_			ELECTR
å	ا.	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR A SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO REF		DESIGN
န္				QUANT
彡		DATA TRANSFER CHECKED BY:		PRELIM
÷.		COMPANY:	DATE:	MUNICI
ë		BY:		morno

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SE	TS	MJ									
POGRAPHY	BW	BW									
OFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
ORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					1 <i>7</i>
TER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					11 .
s	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
LEPHONE	RK	JK									ENGIN
ECTRIC	JH	TK									
SIGN	RB	JK	ASBUILT								3940 ANC
JANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						PH
RELIMINARY/FINAL	RB	JK	INSPECTOR								1
JNICIPAL/STATE	RB	JK									
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5





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

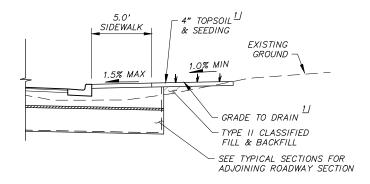
W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

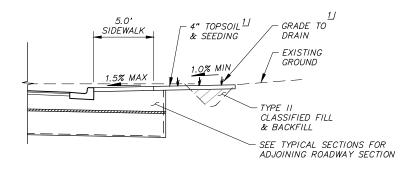
TYPICAL SECTIONS

PATHWAY & REFUGE ISLAND

SCALE HOR. N/A GRID SW1629, SW1630 C4 of C5

TYPICAL SECTION "E" DRIVEWAY 3 **PAVED OR CONCRETE**





SPECIAL FILL GRADING DETAILS

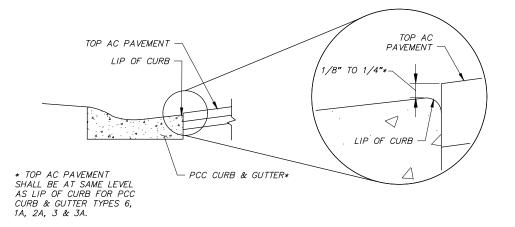
2

SHEET NOTES:

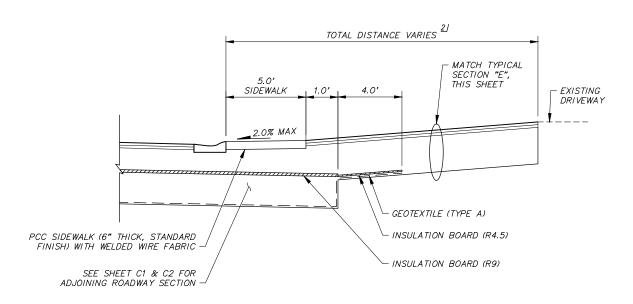
1. SEE SHEETS C1-C2 FOR ADJOINING ROADWAY SECTION.

#/FOOT NOTES:

- 1. PLACE 4" OF TOPSOIL AND SEEDING PER LANDSCAPING (L) SHEETS ON ALL DISTURBED AREAS.
- 2. SEE RECONSTRUCT DRIVEWAY SUMMARY TABLE ON THE ROADWAY SUMMARY TABLE (T) SHEETS, DRIVEWAY RECONSTRUCTION PLANS & DRIVEWAY DETAILS FOR DRIVEWAY RECONSTRUCTION
- 3. INSTALL INSULATION ADJACENT TO DRIVEWAY AND TRANSITION TO DRIVEWAY SECTION PER DETAIL 4, THIS SHEET



CURB AND GUTTER & AC PAVEMENT EDGE DETAIL



TYPICAL DRIVEWAY CONNECTION SECTION

- 1	1					
	RECORD DRAWING	DATA	DRAWN	CHECKED		
	1. DATA PROVIDED BY: TITLE: TITLE:	BASE	TS	MJ		
22	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION	TOPOGRAPHY	BW	BW		
≥	OF THE PROJECT AS CONSTRUCTED.	PROFILE	RB	JK	FIELD BOOKS	BM NC
٥ ا	CONTRACTOR:	STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 70
Š.	BY: DATE:	WATER/SANITARY SEWER	RK	JK	195 & 196	CB 70
21	2. DATA TRANSFERRED BY:	GAS	RK	JK	STAKING	GAAB 8
5	COMPANY: DATE:	TELEPHONE	RK	JK		
ဌ	3 RASED ON DEPLODIC FIELD ORSEDVATIONS BY THE ENGINEER (OR AN INDIVIDIAL LINDER HIS /HER DIRECT	ELECTRIC	JH	TK		
š	 BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. 	DESIGN	RB	JK	ASBUILT	
8	DATA TRANSFER CHECKED BY: TITLE:	QUANTITIES	RB	JK	CONTRACTOR	BASIS C
ز:		PRELIMINARY/FINAL	RB	JK	INSPECTOR	1
?. [33	MUNICIPAL/STATE	RB	JK J		

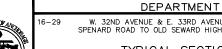
DATA	BY	BY									ı
ASE	TS	MJ									i
OPOGRAPHY	BW	BW									
ROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
TORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
VATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					I L
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
ELEPHONE	RK	JK									ENG
LECTRIC	JH	TK									
DESIGN	RB	JK	ASBUILT								394 A
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						
RELIMINARY/FINAL	RB	JK	INSPECTOR								ı
MUNICIPAL/STATE	RB	JK									i
PLAN (CHECK		CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS		

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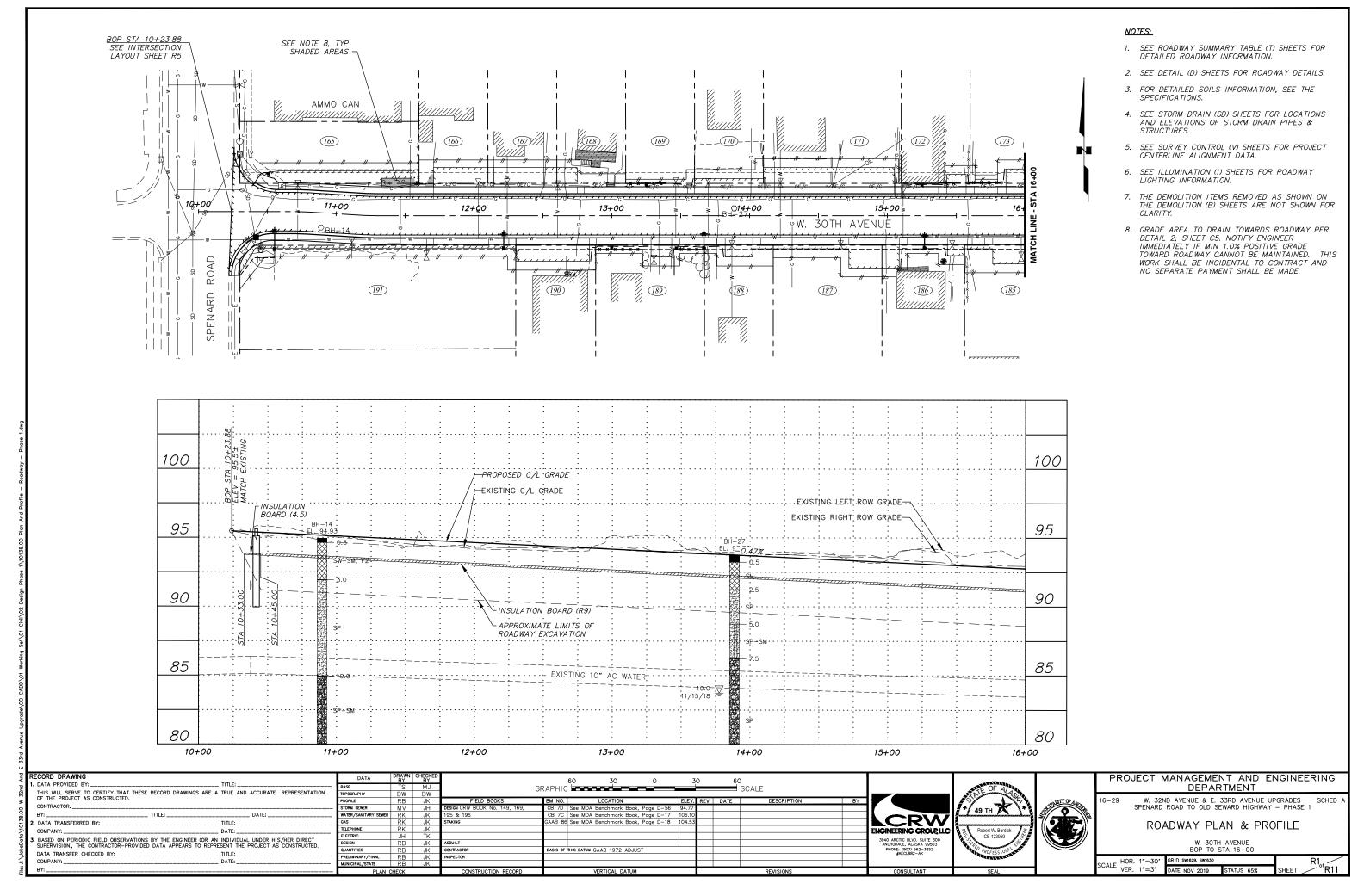
W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED . SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

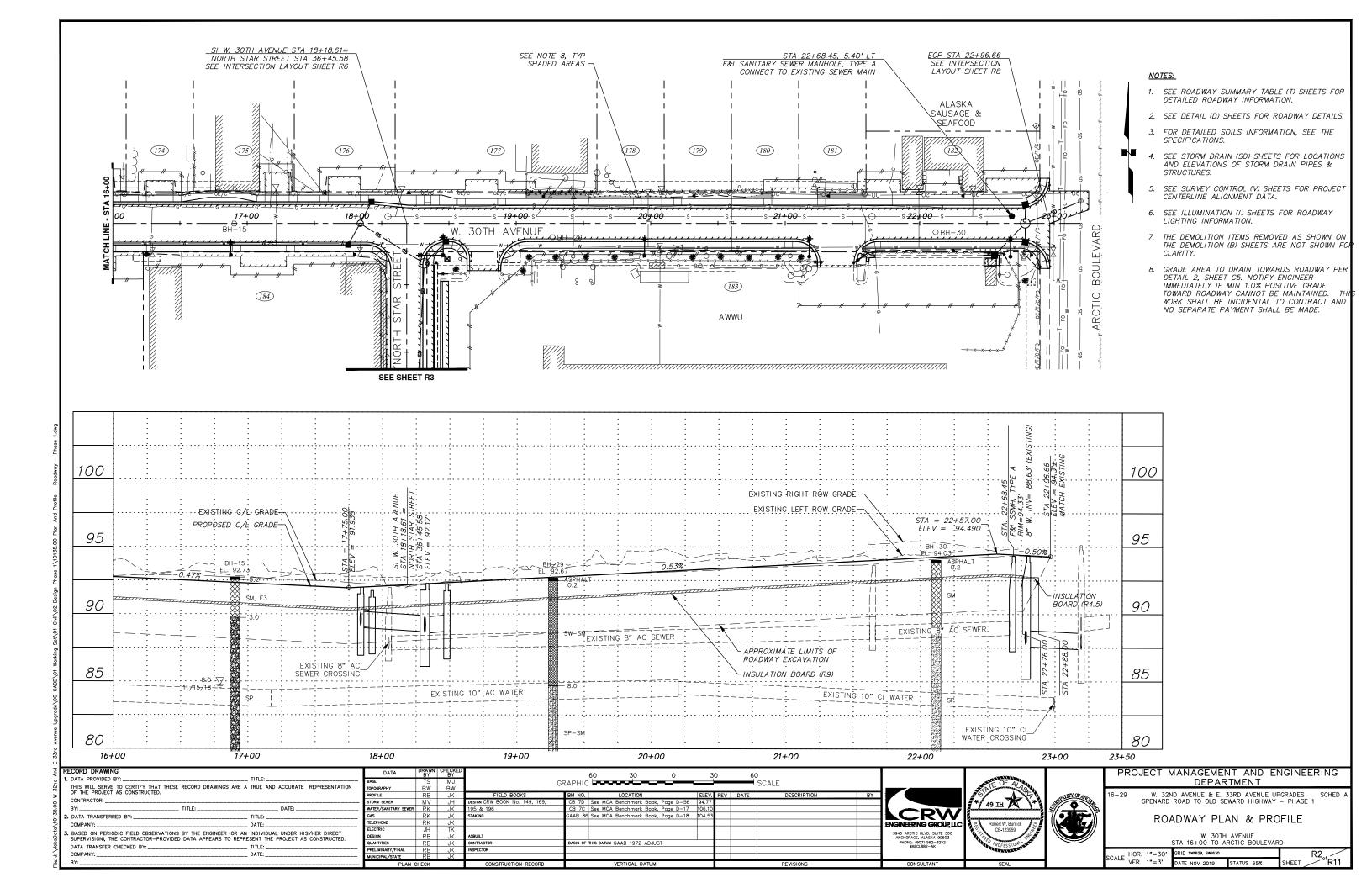
PROJECT MANAGEMENT AND ENGINEERING

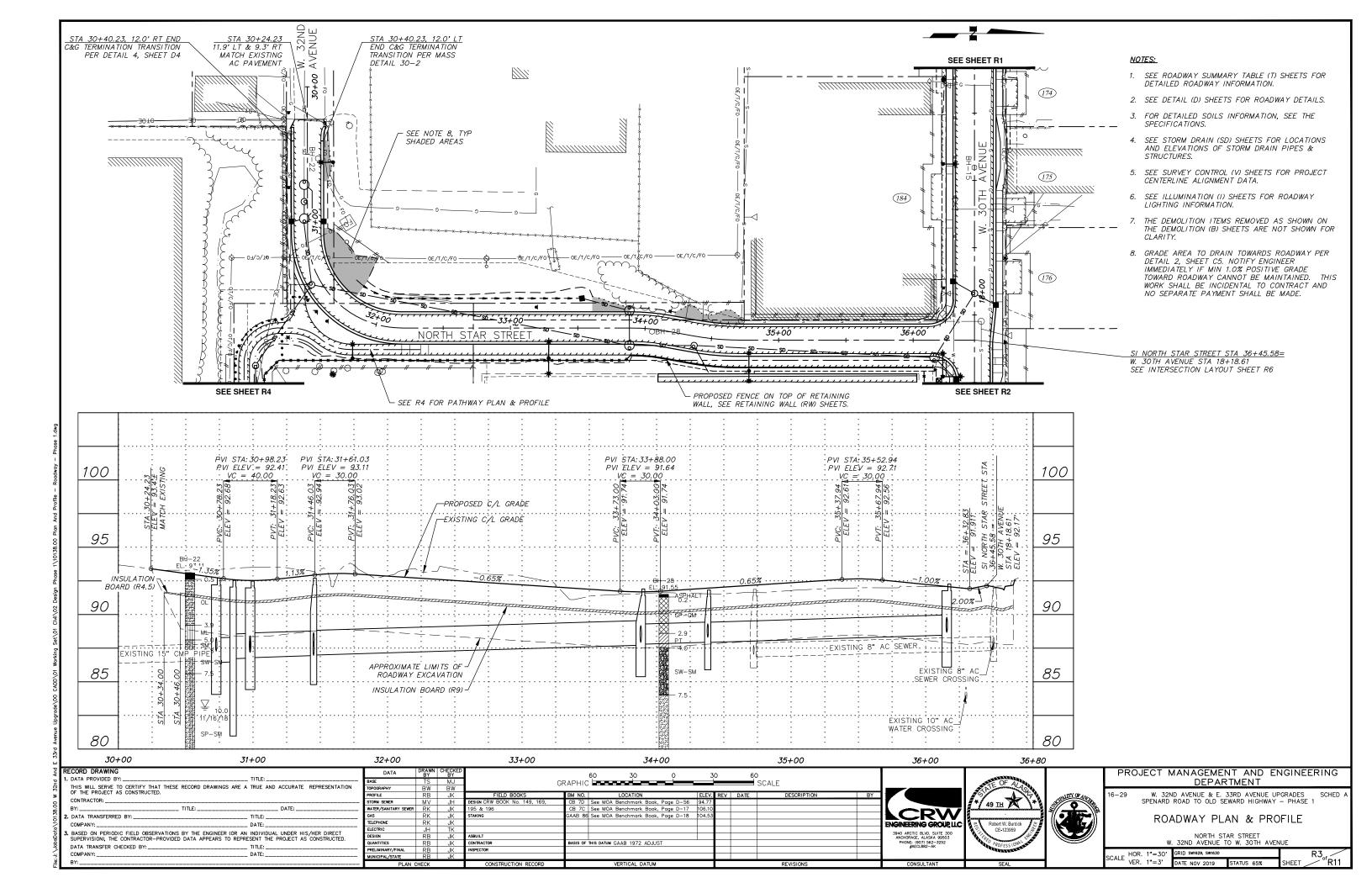
TYPICAL SECTIONS

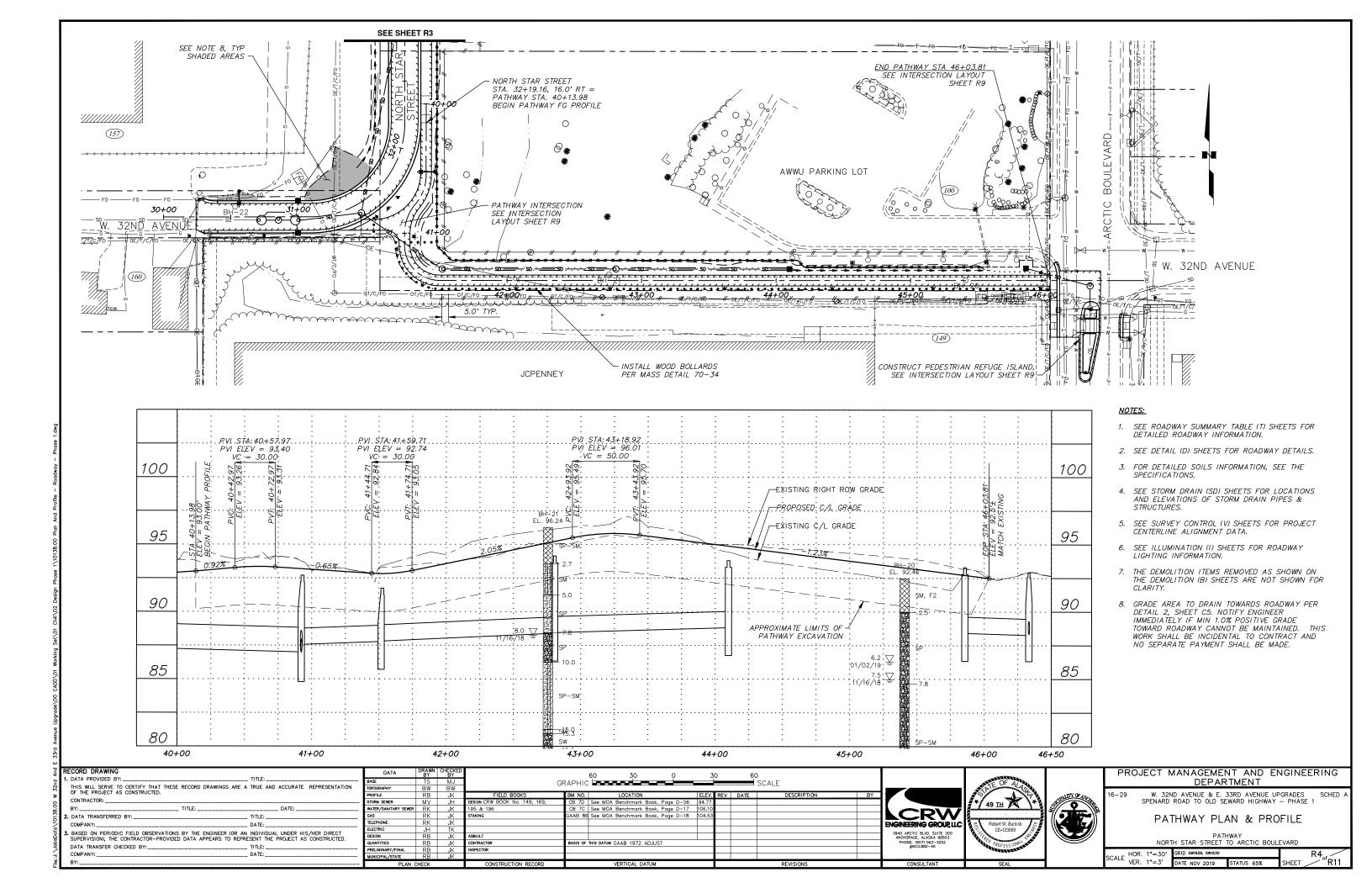
DRIVEWAYS & MISC DETAILS

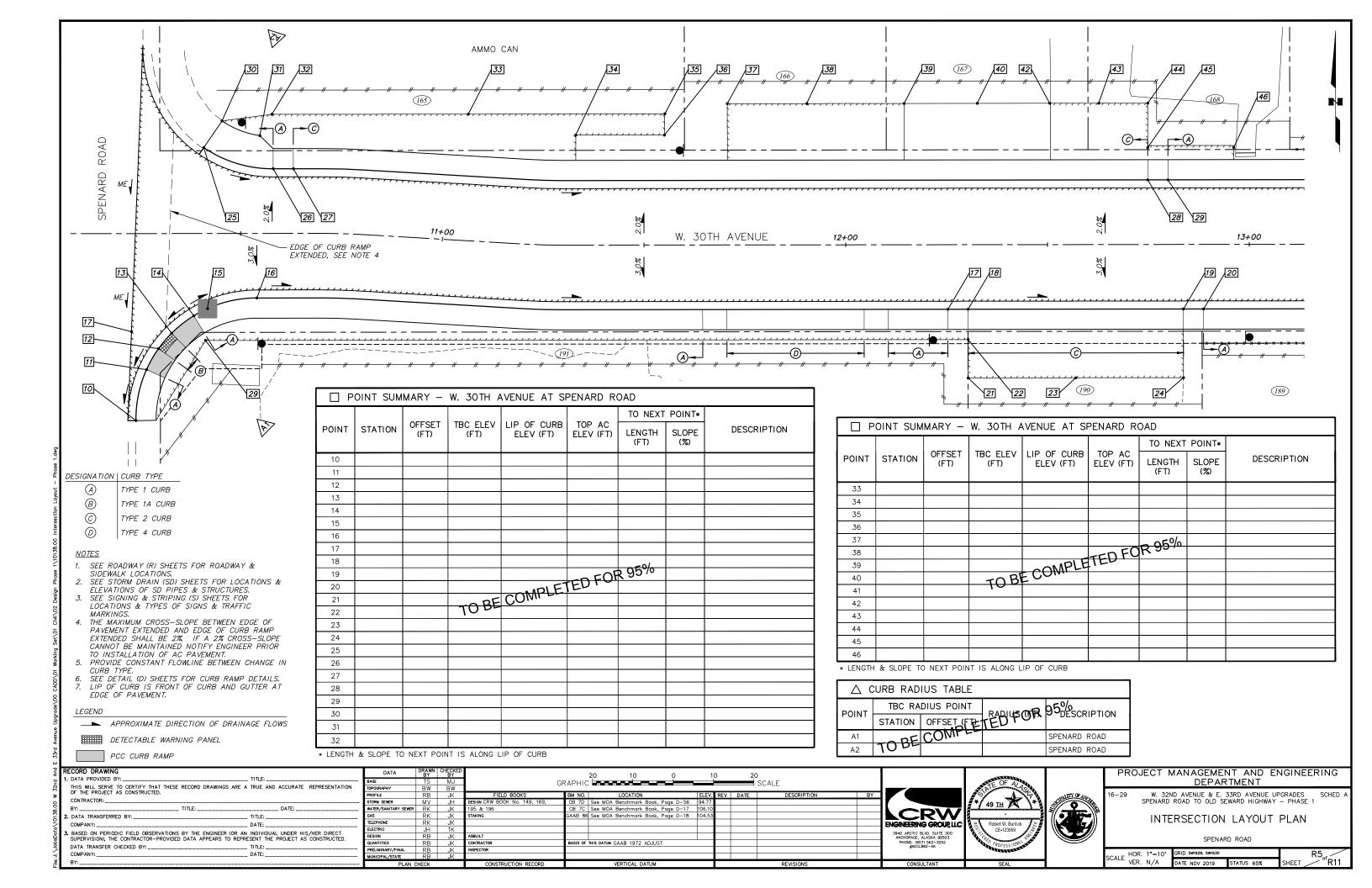
C5_{of}C5 GRID SW1629, SW1630 SCALE HOR. N/A ATE NOV 2019

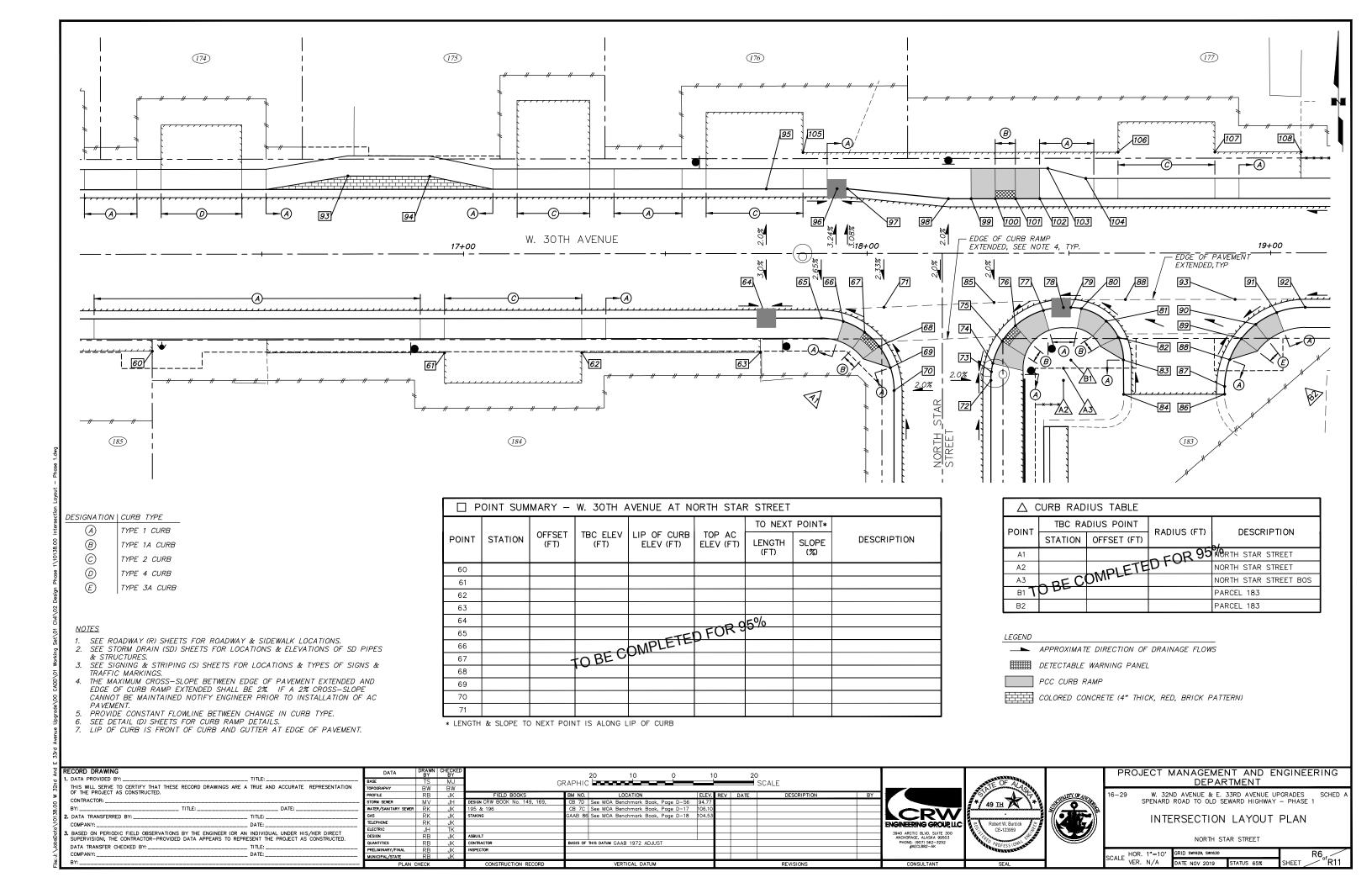












☐ POINT SUMMARY — W. 30TH AVENUE AT NORTH STAR STREET

RECORD DRAWING

1. DATA PROVIDED BY: TITLE:

DATE:

TITLE:

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DATA	DRAWN BY	CHECKED				
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ILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELI
M SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.
R/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106
	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104
PHONE	RK	JK				
TRIC	JH	TK				
3N	RB	JK	ASBUILT			
ITITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST	
IMINARY/FINAL	RB	JK	INSPECTOR			
CIDAL /STATE	PR	IV.				

ENGINEERING GROUP LIC 3940 ARCITIC BLVD. SUITE 300 ANCHORACE, ALASKA 999.03 PHONE (907) 562–2522 PHONE (307) 562–2522

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Robert W. Burdick
CE-123999

AOFESSION

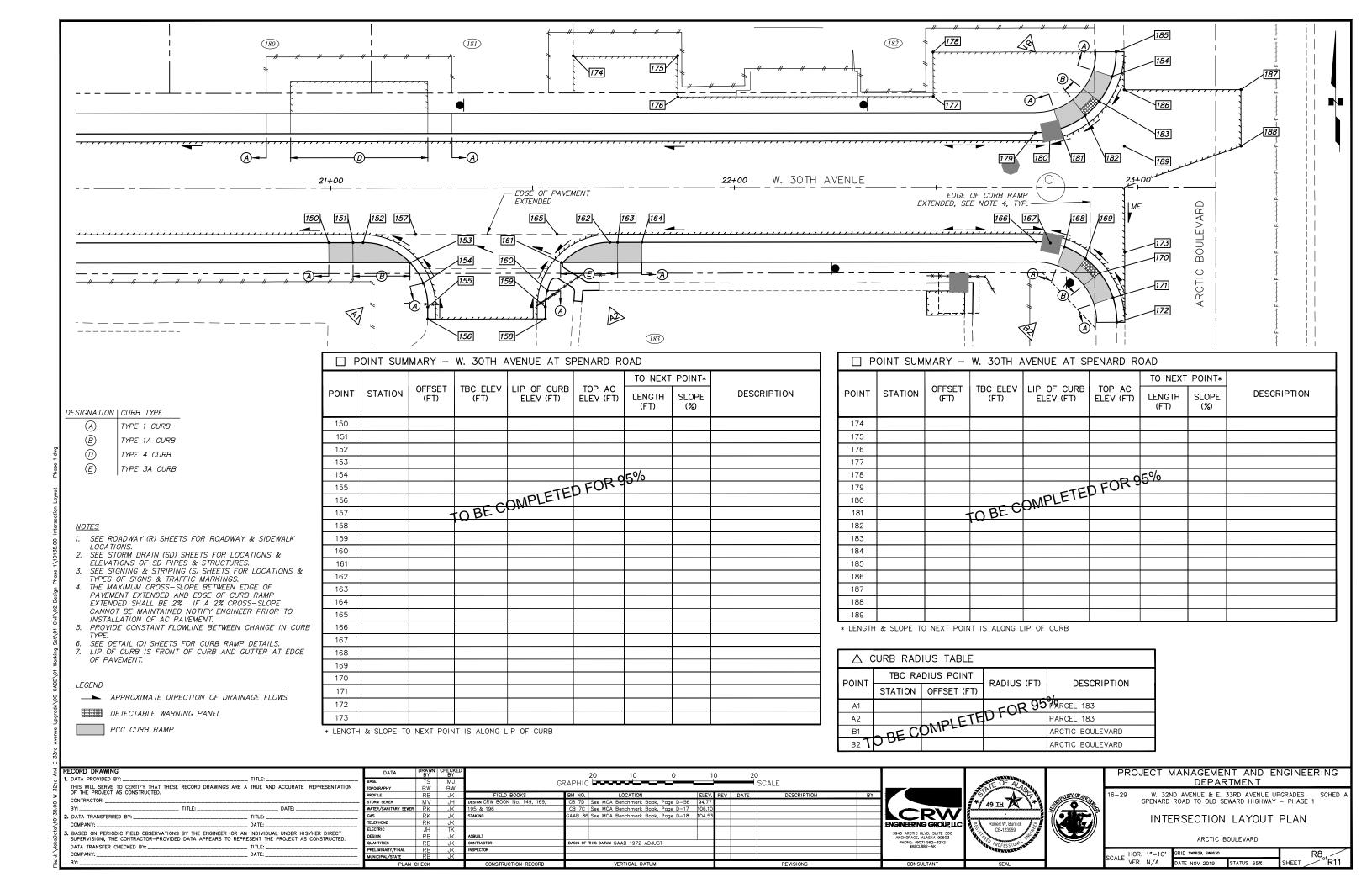
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

INTERSECTION LAYOUT TABLE

SCALE HOR. N/A GRID SW1629, SW1630

SCALE VER. N/A DATE NOV 2019 STATUS 65% SHEET R7 of R11



□ P	OINT SUM	MARY -	PATHWAY	AT NORTH ST	TAR STREE	Т		
						TO NEXT	POINT*	
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	LENGTH (FT)	SLOPE (%)	DESCRIPTION
200								
201								
202								
203								
204				OMPLETE!	~~ 0	5%		
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* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

- 1. SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK
- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
 SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
 SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
 PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE
- 5. SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
 6. LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

■ APPROXIMATE DIRECTION OF DRAINAGE FLOWS

DETECTABLE WARNING PANEL

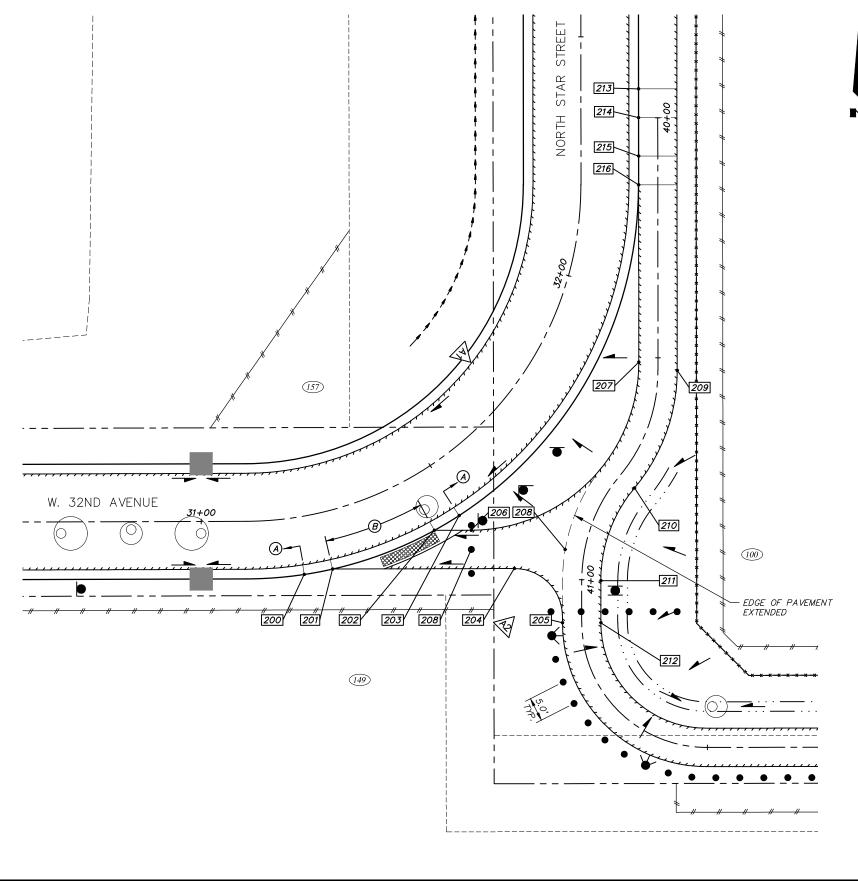
PCC CURB RAMP

DESIGNATION | CURB TYPE

TYPE 1 CURB

 $\bigcirc B$

TYPE 1A CURB



	ECORD DRAWING		
1.	DATA PROVIDED BY:	TITLE:	BAS
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOP
	OF THE PROJECT AS CONSTRUCTED.		PRO
	CONTRACTOR:		STC
	BY: TITLE:	DATE:	WAT
2.	DATA TRANSFERRED BY:	TITLE:	GAS
	COMPANY:	DATE:	TEL
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ŧΥ	BW	BW	9.	., .,								00/122	
	RB	JK	FIELD BOOKS	BM NO.		LOCATION			ELEV.	REV	DATE		DESCRIPTION
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NITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA	Benchmark	Book,	Page D-17	106.10				
	RK	JK	STAKING	GAAB 86	See MOA	Benchmark	Book,	Page D-18	104.53				
	RK	JK											
	JH	TK											
	RB	JK	ASBUILT										
S	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM	GAAB 1972	ADJUS1	Т					
RY/FINAL	RB	JK	INSPECTOR										
/CTATE	PR	II/											





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED . SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

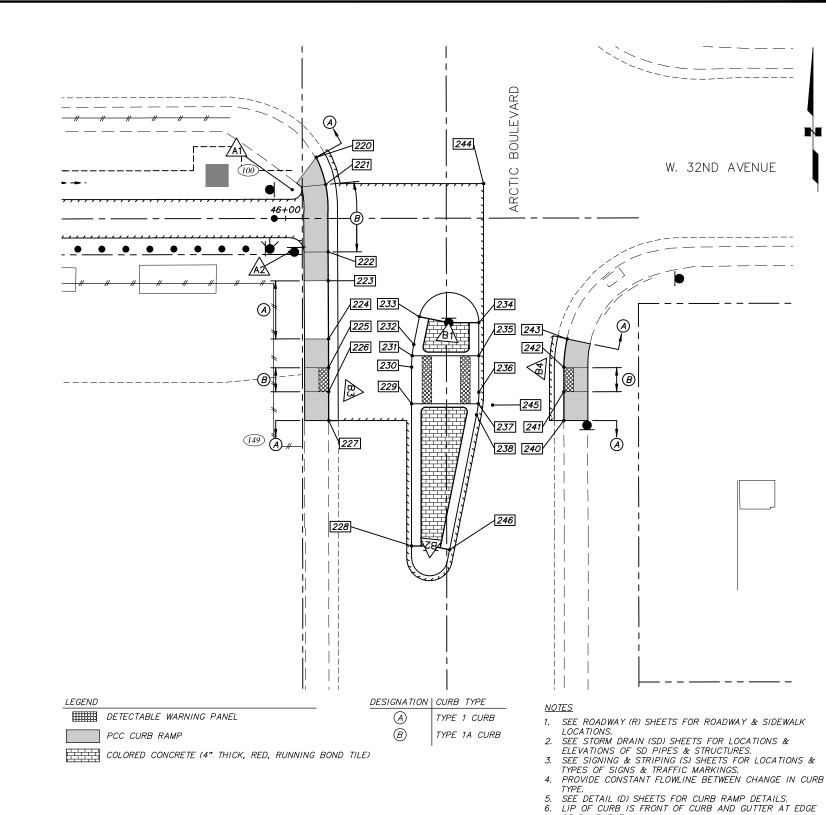
INTERSECTION LAYOUT PLAN

PATHWAY AT NORTH STAR STREET

GRID SW1629, SW1630 HOR. 1"=10' R9_{of}R11 DATE NOV 2019 STATUS 65%

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

\triangle	URB RAD	IUS TABLE		
POINT	TBC RA	DIUS POINT	RADIUS (FT)	DESCRIPTION
POINT	STATION	OFFSET (FT)		-1
A1			- EOR 95	PATHWAY
A2		OMPLETE	יי טייט.	PATHWAY
B1	a DE C	OML		REFUGE ISLAND NOSE
в2 🕇				REFUGE ISLAND NOSE
В3				REFUGE ISLAND
B4				REFUGE ISLAND



RECORD DRAWING

I. DATA PROVIDED BY:
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR:
BY:
TITLE:
DATE:
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2. DATA TRANSFERRED BY:
COMPANY:
DATE:

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3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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ENGINEERING GROUP LLC
3840 ARCTIC BLVD. SUITS
AND GROUP LLC
3840 ARCTIC BLVD. SUITS
ARCTIC SIGN 79 562-252
PONE: 1907 1962-252
ARCTIC SIGN 79 562-252

Robert W. Burdok CE-12399

OF PAVEMENT.

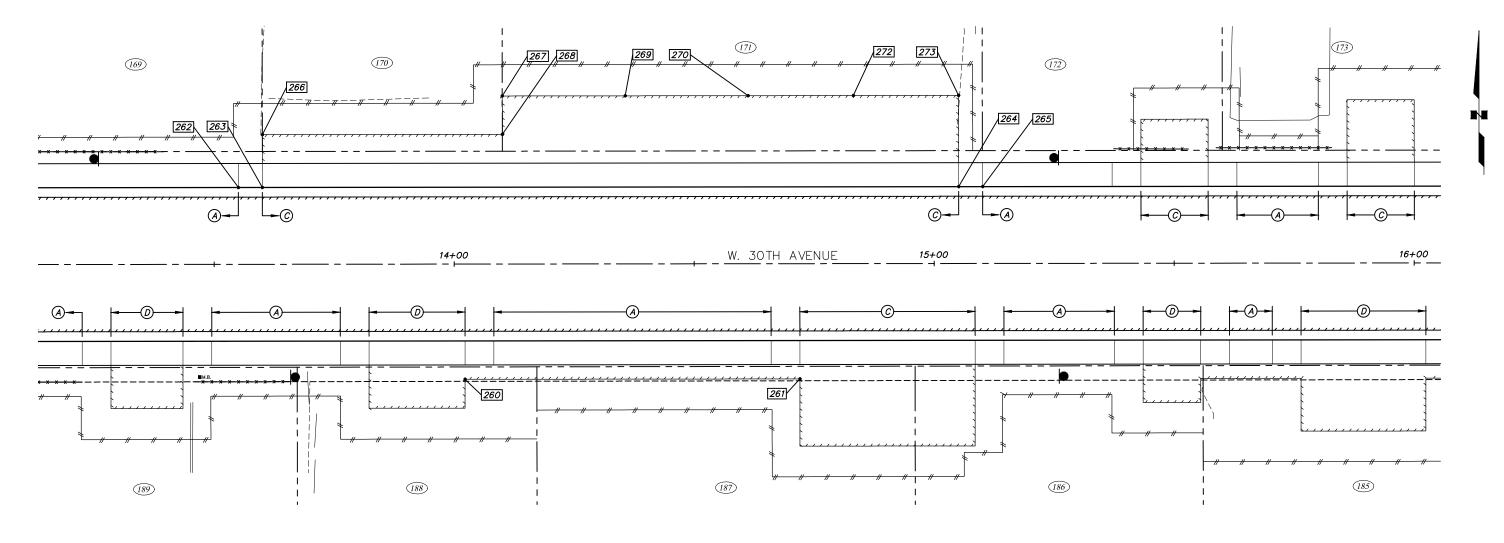
PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED . SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

INTERSECTION LAYOUT PLAN

PATHWAY AT ARCTIC BOULEVARD

HOR, 1"=10" | GRID SM628, SM630 | STATUS 65% | SHEET | OF R11



DESIGNATION	CURB	TYPE

- TYPE 1 CURB
- $\bigcirc B$ TYPE 1A CURB
- **©** TYPE 2 CURB
 - TYPE 4 CURB

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
 SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS &

- 2. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.

 3. SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.

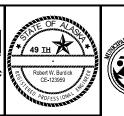
 4. THE MAXIMUM CROSS—SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS—SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO
- INSTALLATION OF AC PAVEMENT. 5. PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB
- 6. SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS. 7. LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

□ P	OINT SUM	MARY -	30TH AVE	NUE DRIVEWA	YS			
						TO NEXT	POINT*	
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	LENGTH (FT)	SLOPE (%)	DESCRIPTION
260								
261								
262								
263								
264				OMPLETE'	-50	5%		
265				-TE	h FOK 3	,		
266			- 0	OMPLEIL				
267		-	to BE U	O.				
268								
269								
270								
271								
272								
273								

* LENGTH & SLOPE TO NEXT POINT IS ALONG LIP OF CURB

RI	CORD DRAWING		
1.	DATA PROVIDED BY:	_ TITLE:	BASE
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOPOG
	OF THE PROJECT AS CONSTRUCTED.		PROFIL
	CONTRACTOR:		STORM
	BY: TITLE:		WATER
١,	DATA TRANSFERRED BY:		GAS
١-	COMPANY:		TELEPI
			ELECT
3.	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN SUPERVISION). THE CONTRACTOR-PROVIDED DATA APPEARS TO REPR	I INDIVIDUAL UNDER HIS/HER DIRECT	DESIG
			QUANT
	DATA TRANSFER CHECKED BY:		PRELIF
	COMPANY:	_ DATE:	MUNIC
1	BY:		

DATA	DRAWN BY	CHECKED			20 10 0	10	0	20			
BASE	TS	MJ	G	RAPHIC	: Learner - Learner			SCALE			
TOPOGRAPHY	BW	BW	· · ·	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,							
PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					INCRW
TELEPHONE	RK	JK									ENGINEERING GROUP LLC
ELECTRIC	JH	TK									
DESIGN	RB	JK	ASBUILT								3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						PHONE: (907) 562-3252 #AECL882-AK
PRELIMINARY/FINAL	RB	JK	INSPECTOR								#AECE882-AR
MUNICIPAL/STATE	RB	JK			<u>'</u>						
PLAN (CHECK		CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS		CONSULTANT





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED . SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

DRIVEWAY RECONSTRUCTION PLAN

SCALE HOR. 1"=10' GRID SW1629, SW1630

VER. N/A DATE NOV 2019 R11, / SHEET

RECONSTRUCT DRIVEWAY

SHEET	PARCEL		ERLINE RENCE	DRIVEWAY WIDTH AT CURB OR EDGE OF	CURB CUT TYPE	CURB RETURN	SKEW ANGLE	LANDING LENGTH (FT)	LANDING GRADE	TOTAL DISTANCE	EXISTING GRADE	PROPOSED GRADE	SURFACE TYPE ON PROPERTY	L1 (FT)	L2 (FT)	CONSTRUCT PER DETAIL	REMARKS
		STATION	OFFSET	PAVEMENT (FT)	001 111 2	RADII (FT)	(DEGREES)	LENOTH	ONABL	(FT)	ONADE	ONABL	T KOI EKIT				
R1	165	11+07.00	LT	70	2	N/A	-90	5.0	1.5%	VARIES	4.4%	3.8%	ASPHALT	6.0	-	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	191	11+87.53	RT	34	4	N/A	90	5.0	1.5%	7.5	3.6%	1.5%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	166	11+90.37	LT	39.4	2	N/A	-90	5.0	1.5%	16.0	5.8%	2.6%	ASPHALT	_		DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	167	12+32.63	LT	50	2	N/A	-90	5.0	1.5%	16.0	7.9%	8.4%	ASPHALT/CONCRETE	_	_	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	190	12+56.97	RT	53.4	2	N/A	90	5.0	1.5%	17.0	3.3%	1.8%	ASPHALT	-	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	168	12+66.62	LT	14.8	2	N/A	-90	5.0	1.5%	16.0	6.4%	3.4%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R5
R1	170	13+35.98	LT	50	2	N/A	-90	5.0	1.5%	11.0	6.8%	7.5%	ASPHALT	6.0		DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	189	13+85.52	RT	15	4	N/A	90	5.0	2.0%	14.0	8.4%	7.2%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	188	13+92.25	RT	20	4	N/A	90	5.0	2.0%	14.0	3.2%	5.2%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	171	14+57.59	LT	100	2	N/A	-90	5.0	2.0%	19.0	8.0%	9.9%	ASPHALT	_	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	187	14+90.26	RT	36.5	2	N/A	90	8.0	1.5%	22.0	0.5%	4.3%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	186	15+48.96	RT	12	2	N/A	90	5.0	1.5%	13.0	3.7%	7.4%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	172	15+51.52	LT	14	2	N/A	-90	5.0	2.0%	14.0	10.9%	10.2%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	185	15+89.41	RT	26	4	N/A	90	12.0	2.0%	19.0	1.8%	6.0%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R1	173	15+93.05	LT	14	2	N/A	-90	5.0	2.0%	18.0	7.4%	9.6%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R2	174	16+35.07	LT	20	4	N/A	-90	5.0	1.5%	16.0	1.8%	6.7%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	184	17+12.25	LT	34	2	N/A	90	5.0	2.0%	16.0	2.9%	7.5%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	175	17+23.78	LT	18	2	N/A	-90	5.0	2.0%	22.0	9.3%	9.6%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	176	17+72.13	LT	24	2	N/A	-90	5.0	2.0%	19.0	6.0%	8.1%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	177	18+72.18	LT	24	4	N/A	-90	5.0	2.0%	19.0	12.1%	11.8%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	183 WEST	18+76.00	RT	21	N/A	13/20	90	8.5	2.0%	23.5	3.2%	3.9%	ASPHALT	6.0	6.0	DETAIL 1, SHEET D3	SEE INTERSECTION LAYOUT SHEET R6
R2	178	20+05.04	LT	24	4	N/A	90	N/A	N/A	N/A	-	_	_	_	-		CURB CUT ONLY — FUTURE DEVELOPMENT
R2	180	21+07.04	LT	34	4	N/A	-90	5.0	1.5%	13.0	5.2%	7.0%	ASPHALT	6.0	6.0	DETAIL 2, SHEET D3	SEE INTERSECTION LAYOUT SHEET R8
R2	183 EAST	21+38.44	RT	25	N/A	16	90	8.5	2.0%	21.0	6.5%	7.8%	ASPHALT	6.0	6.0	DETAIL 1, SHEET D3	SEE INTERSECTION LAYOUT SHEET R8

RECONSTRUCT DRIVEWAY NOTES:

1. "LANDING LENGTH" BEGINS AT THE BACK OF CURB & GUTTER.
2. "LANDING GRADE" IS THE GRADE OF THE LANDING FROM THE BACK OF CURB & GUTTER TO THE END OF LANDING.
3. "SKEW ANGLE" ("+" IS CLOCKWISE AND "-" IS COUNTER CLOCKWISE) IS MEASURED FROM PROJECT CENTERLINE WITH O DEGREES ALIGNED ALONG INCREASING STATIONS.
4. "TOTAL DISTANCE" IS THE LIMIT OF RECONSTRUCTION BEGINNING AT THE BACK OF CURB & GUTTER.
5. "PROPOSED GRADE" IS APPROXIMATE GRADE FROM THE END OF THE LANDING TO THE LIMIT OF RECONSTRUCTION. ACTUAL CONSTRUCTION GRADE MAY VARY.

30.02

P.C.C.	CURB AND GUTTE	IRB AND GUTTER (TYPE 1, STEEL CURB FACING)							
SHEET	BEGIN STATION	OFFSET (FT)	END STATION	OFFSET (FT)	LENGTH (FT)	REMARKS			
R1	17+95.08	16.00 LT	18+20.08	13.50 RT	25.1				

P.C.C. CURB AND GUTTER (TYPE 1, STEEL CURB FACING) NOTES:

1. SEE DETAIL 2, SHEET D4 FOR STEEL CURB FACING DETAIL.

30.02

SHEET	STATION TO STATION	OFFSET (FT)	LENGTH (FT)	REMARKS
R1	10+41 TO 16+00	LT	560	INCLUDES DRIVEWAYS
R1	10+24 TO 16+00	RT	594	INCLUDES DRIVEWAYS
R2	16+00 TO 22+95	LT	706	INCLUDES DRIVEWAYS
R2	16+00 TO 22+95	RT	739	INCLUDES DRIVEWAYS & PARCEL 183
R3	30+30 TO 36+12	LT	563	
R3	30+30 TO 36+12	RT	600	
R3	34+46 TO 36+03	RT	161	AWWU PARKING LOT
R4	46+07	12.7 LT TO 42.1 RT	55	ARCTIC BOULEVARD SIDEWALK RECONSTRUCTION
R4	46+30	21.8 RT TO 68.3 RT	89	PEDESTRIAN REFUGE ISLAND
R4	46+58	25.2 RT TO 42.2 RT	17	ARCTIC BOULEVARD SIDEWALK RECONSTRUCTION

PCC CURB & GUTTER (ALL TYPES) NOTES:

- 1. SEE INTERSECTION LAYOUT SHEETS AND DRIVEWAY RECONSTRUCTION SHEETS R5-R11 FOR LOCATIONS AND TYPES OF CURB AND GUTTER.
- 2. SEE 20.28 RECONSTRUCT DRIVEWAY TABLE FOR LOCATIONS OF DRIVEWAY CURB CUTS.

RI	ECORD DRAWING		
1.	DATA PROVIDED BY:	TITLE:	SE
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE	A TRUE AND ACCURATE REPRESENTATION TOP	OGRA
	OF THE PROJECT AS CONSTRUCTED.		OFILE
	CONTRACTOR:	STO	ORM S
	BY: TITLE:		
2.	DATA TRANSFERRED BY:		s
	COMPANY:	DATE:	EPHC
١.		FIF	CTRI
Э.	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR A SUPERVISION). THE CONTRACTOR-PROVIDED DATA APPEARS TO REP	DESCRIPTION OF PROJECT AS CONSTRUCTED DES	SIGN
		QUA	ANTII
	DATA TRANSFER CHECKED BY:	PRE	ELIMI
	COMPANY:	DATE:	NICIF

DATA	DRAWN BY	CHECKED									
SE	TS	MJ									ı
POGRAPHY	BW	BW									
OFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
ORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
TER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					ш
s	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
LEPHONE	RK	JK									EN
ECTRIC	JH	TK									l
SIGN	RB	JK	ASBUILT								1
IANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						1
ELIMINARY/FINAL	RB	JK	INSPECTOR								1
INICIPAL/STATE	RB	JK									<u>L</u>
PLAN (CHECK		CONSTRUCTION RECORD		VERTICAL DATUM				REVISIONS		





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

ROADWAY SUMMARY TABLES

ıF	HOR.	N/A	GRID SW1629, SW1630	T1./	
LE	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET ° T3

30.03

SHEET APPX BEDIN STA	P.C.C.	SIDEWALK						
PR	SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	6" THICK, AREA (SY)	REMARKS
Fit 10-40.3 21.5 LT 10-57.8 15.0 LT 14	R1	10+24.0	46.3 RT	10+26.6	33.7 RT	7		
Fit 10-57.6 16.0 LT 12-72.9 16.0 LT 18.0 34000. 165-167 JB INDERVAY 18.1 12-10.5 18.0 RT 15.0 RT 15.0 RT 3.5 20000. 190 DRIVENAY 18.1 RT 12-10.5 18.0 RT 15.0 RT 16.0 RT 17.0 RE	R1	10+38.3	20.4 RT	11+64.5	16.0 RT	70		
RED 11-84-5 18-0 RT 12-10-5 18-0 RT 28 PARCEL 191 DENEMAY 181 17-20-5 18-0 RT 19-20-5 18-0 RT 19-20-	R1	10+40.9	21.3 LT	10+57.9	16.0 LT	14		
RY 12-10.5 160 RY 12-25.3 160 RY 35 PARCEL 190 DRIVENAY RY 12-25.3 160 RY 12-25.3 160 RY 12-25.5 160 RY 12-25.5 160 RY 13-25.5 160 RY 13-25.5 160 RY 13-25.5 160 RY 13-40.5 160 RY 14-40.3 160 RY 13-40.5 160 RY	R1	10+57.9	16.0 LT	12+79.9	16.0 LT		180	PARCEL 165-167 DRIVEWAY
Fit 12-25.3 10.0 RT 12-88.7 15.0 RT 15-85.1 15.0 LT 42.5 15.0 RT 15.0 LT 15-85.1 15.0 LT 1	R1	11+64.5	16.0 RT	12+10.5	16.0 RT		26	PARCEL 191 DRIVEWAY
RT 124799 16.0 LT 154951 16.0 LT 144951 16.0 LT 18	R1	12+10.5	16.0 RT	12+25.3	16.0 RT	8		
R1	R1	12+25.3	16.0 RT	12+88.7	16.0 RT		35	PARCEL 190 DRIVEWAY
RI	R1	12+79.9	16.0 LT	13+55.1	16.0 LT	42		
R1	R1	12+88.7	16.0 RT	13+22.5	16.0 RT	19		
Fill 134-95.1 150. LT	R1	13+22.5	16.0 RT	13+49.5	16.0 RT		15	PARCEL 189 DRIVEWAY
R1	R1	13+49.5	16.0 RT	13+76.3	16.0 RT	15		
RT	R1	13+55.1	16.0 LT	15+10.1	16.0 LT		86	PARCEL 170 & 171 DRIVEWAY
RT	R1	13+76.3	16.0 RT	14+08.3	16.0 RT		18	PARCEL 188 DRIVEWAY
RI	R1	14+08.3	16.0 RT	14+66.0	16.0 RT	32		
R1	R1	14+66.0	16.0 RT	15+14.5	16.0 RT		27	PARCEL 187 DRIVEWAY
RE	R1	15+10.1	16.0 LT	15+37.1	16.0 LT	15		
RI	R1	15+14.5	16.0 RT	15+37.5	16.0 RT	13		
R1	R1	15+37.1	16.0 LT	15+63.1	16.0 LT		14	PARCEL 172 DRIVEWAY
R1	R1	15+37.5	16.0 RT	15+61.5	16.0 RT		13	PARCEL 186 DRIVEWAY
R1/R2	R1	15+61.5	16.0 RT	15+70.4	16.0 RT	5		
R1/R2	R1	15+63.1	16.0 LT	15+80.1	16.0 LT	9		
R2 16+06.1 16.0 LT 16+19.1 16.0 LT 7 R2 16+19.1 16.0 LT 16+19.1 16.0 LT 7 R2 16+19.1 16.0 LT 16+19.1 16.0 LT 18.0 LT	R1/R2	15+70.4	16.0 RT	16+08.4	16.0 RT		21	PARCEL 185 DRIVEWAY
R2 16+08.4 16.0 RT 16+89.3 16.0 RT 45 R2 16+19.1 16.0 LT 16+51.1 16.0 LT 18 PARCEL 174 DRIVEWAY R2 16+51.1 16.0 LT 17+07.3 16.0 LT 31 26 PARCEL 184 DRIVEWAY R2 16+89.3 16.0 RT 17+35.3 16.0 LT 17 17 PARCEL 175 DRIVEWAY R2 17+07.3 16.0 LT 17+37.3 16.0 LT 17 9 17 PARCEL 175 DRIVEWAY R2 17+37.3 16.0 LT 17+94.0 16.8 RT 32 9 20 PARCEL 175 DRIVEWAY R2 17+37.3 16.0 LT 17+94.1 16.0 LT 9	R1/R2	15+80.1	16.0 LT	16+06.1	16.0 LT		14	PARCEL 173 DRIVEWAY
R2 16+08.4 16.0 RT 16+89.3 16.0 RT 45 R2 16+19.1 16.0 LT 16+51.1 16.0 LT 18 PARCEL 174 DRIVEWAY R2 16+51.1 16.0 LT 17+07.3 16.0 LT 31 26 PARCEL 184 DRIVEWAY R2 16+89.3 16.0 RT 17+35.3 16.0 LT 17 17 PARCEL 175 DRIVEWAY R2 17+07.3 16.0 LT 17+37.3 16.0 LT 17 9 17 PARCEL 175 DRIVEWAY R2 17+37.3 16.0 LT 17+94.0 16.8 RT 32 9 20 PARCEL 175 DRIVEWAY R2 17+37.3 16.0 LT 17+94.1 16.0 LT 9								
R2 16+19.1 16.0 LT 16+51.1 16.0 LT 31 18 PARCEL 174 DRIVEWAY R2 16+51.1 16.0 LT 17+07.3 16.0 LT 31 26 PARCEL 184 DRIVEWAY R2 16+89.3 16.0 RT 17+35.3 16.0 LT 17 PARCEL 175 DRIVEWAY R2 17+07.3 16.0 LT 17+94.0 16.8 RT 32 R2 17+37.3 16.0 LT 17+94.1 16.0 LT 9 R2 17+54.1 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 17-90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+94.1 16.0 LT 17-90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+94.1 16.0 LT 15.0 LT 20 PARCEL 176 DRIVEWAY R2 18+42.8 13.5 LT 18+53.0 14.8 RT 5 R2 18+43.7 14.2 RT 18+53.0 14.8 RT 5 R2 18+56.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2	R2	16+06.1	16.0 LT	16+19.1	16.0 LT	7		
R2 16+51.1 16.0 LT 17+07.3 16.0 LT 31 26 PARCEL 184 DRIVEWAY R2 16+89.3 16.0 RT 17+35.3 16.0 RT 26 PARCEL 184 DRIVEWAY R2 17+07.3 16.0 LT 17+37.3 16.0 LT 17 PARCEL 175 DRIVEWAY R2 17+35.3 16.0 LT 17+94.0 16.8 RT 32 32 R2 17+37.3 16.0 LT 17+94.1 16.0 LT 9 9 9 R2 17+54.1 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 9 R2 18+42.6 13.5 LT 18+56.2 13.5 LT 9 9 9 R2 18+43.7 14.2 RT 18+53.0 14.8 RT 5 9 9 9 R2 18+95.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 170 DRIVEWAY 12.5 LT 14.8 RT 5 9	R2	16+08.4	16.0 RT	16+89.3	16.0 RT	45		
R2 16+89.3 16.0 RT 17+35.3 16.0 RT 26 PARCEL 184 DRIVEWAY R2 17+07.3 16.0 LT 17+37.3 16.0 LT 17 PARCEL 175 DRIVEWAY R2 17+35.3 16.0 RT 17+94.0 16.8 RT 32 32 R2 17+37.3 16.0 LT 17+94.1 16.0 LT 9 9 R2 17+37.3 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+94.1 16.0 LT 18+25.8 13.5 LT 25 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 20 PARCEL 176 DRIVEWAY R2 18+42.8 13.5 LT 18+56.2 13.5 LT 9 20 PARCEL 176 DRIVEWAY R2 18+43.7 14.2 RT 18+56.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+96.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 19+87.0 13.5 LT 20+93.5 13.5 LT 20 PARCEL 178 DRIVEWAY	R2	16+19.1	16.0 LT	16+51.1	16.0 LT		18	PARCEL 174 DRIVEWAY
R2 17+07.3 16.0 LT 17+37.3 16.0 LT 17 PARCEL 175 DRIVEWAY R2 17+35.3 16.0 RT 17+94.0 16.8 RT 32 R2 17+37.3 16.0 LT 17+54.1 16.0 LT 9 R2 17+54.1 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 20 PARCEL 176 DRIVEWAY R2 18+42.8 13.5 LT 18+56.2 13.5 LT 9 20 PARCEL 176 DRIVEWAY R2 18+43.7 14.2 RT 18+56.2 13.5 LT 9 20 PARCEL 177 DRIVEWAY R2 18+56.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+56.2 13.5 LT 19+87.0 13.5 LT 20 PARCEL 177 DRIVEWAY R2 19+87.0 13.5 LT 20+93.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 20 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 L	R2	16+51.1	16.0 LT	17+07.3	16.0 LT	31		
R2 17+35.3 16.0 RT 17+94.0 16.8 RT 32	R2	16+89.3	16.0 RT	17+35.3	16.0 RT		26	PARCEL 184 DRIVEWAY
R2 17+37.3 16.0 LT 17+54.1 16.0 LT 9 R2 17+54.1 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 20 PARCEL 176 DRIVEWAY R2 18+42.8 13.5 LT 18+56.2 13.5 LT 9 40 40 40 40 40 46+03.9 40	R2	17+07.3	16.0 LT	17+37.3	16.0 LT		17	PARCEL 175 DRIVEWAY
R2 17+54.1 16.0 LT 17+90.1 16.0 LT 20 PARCEL 176 DRIVEWAY R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 25 25 25 26 27 28 28 28 28 28 28 28 28 29 20	R2	17+35.3	16.0 RT	17+94.0	16.8 RT	32		
R2 17+90.1 16.0 LT 18+25.8 13.5 LT 25 R2 18+42.8 13.5 LT 18+56.2 13.5 LT 9 R2 18+43.7 14.2 RT 18+53.0 14.8 RT 5 R2 18+56.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+92.2 13.5 LT 19+87.0 13.5 LT 53 20 PARCEL 177 DRIVEWAY R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 20 PARCEL 178 DRIVEWAY R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 20 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 83 24 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 RT 22+80.8 14.7 LT 83 83 84 84 85 84 85 86 86 86 86 86 86 86 87 87 87	R2	17+37.3	16.0 LT	17+54.1	16.0 LT	9		
R2 18+42.8 13.5 LT 18+56.2 13.5 LT 9 R2 18+43.7 14.2 RT 18+53.0 14.8 RT 5 R2 18+56.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+92.2 13.5 LT 19+87.0 13.5 LT 53 R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 26 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 83 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 21-30.0 25.1 RT 3 3 R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 35.5 LT 3 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 3 3 R10 46+03.9 13.0 RT 46+03	R2	17+54.1	16.0 LT	17+90.1	16.0 LT		20	PARCEL 176 DRIVEWAY
R2 18+43.7 14.2 RT 18+53.0 14.8 RT 5 R2 18+56.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+92.2 13.5 LT 19+87.0 13.5 LT 53 53 R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 20 PARCEL 178 DRIVEWAY R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 26 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 3 R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 13.0 RT 13.0 RT 13.0 RT <	R2	17+90.1	16.0 LT	18+25.8	13.5 LT	25		
R2 18+56.2 13.5 LT 18+92.2 13.5 LT 20 PARCEL 177 DRIVEWAY R2 18+92.2 13.5 LT 19+87.0 13.5 LT 53 53 R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 20 PARCEL 178 DRIVEWAY R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 26 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 83 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 3 22+24.0 22+81.8 14.8 RT 58 3	R2	18+42.8	13.5 LT	18+56.2	13.5 LT	9		
R2 18+92.2 13.5 LT 19+87.0 13.5 LT 53 R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 26 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 22+80.8 14.7 LT 83 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 RT 22+80.8 14.7 LT 83 3	R2	18+43.7	14.2 RT	18+53.0	14.8 RT	5		
R2 19+01.5 14.8 RT 20+99.5 13.5 RT 110 20 PARCEL 178 DRIVEWAY R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 26 PARCEL 180 DRIVEWAY R2 20+84.0 13.5 LT 21+30.0 13.5 LT 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 83 R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 8 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 3 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 3 3 R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13.0 RT 13 BIKE RAMP	R2	18+56.2	13.5 LT	18+92.2	13.5 LT		20	PARCEL 177 DRIVEWAY
R2 19+87.0 13.5 LT 20+23.0 13.5 LT 20 PARCEL 178 DRIVEWAY R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 R2 20+84.0 13.5 LT 21+30.0 13.5 LT 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 13.0 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13.0 RT 13 BIKE RAMP	R2	18+92.2	13.5 LT	19+87.0	13.5 LT	53		
R2 20+23.0 13.5 LT 20+84.0 13.5 LT 34 R2 20+84.0 13.5 LT 21+30.0 13.5 LT 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 83 83 83 83 84 84 84 84 85	R2	19+01.5	14.8 RT	20+99.5	13.5 RT	110		
R2 20+84.0 13.5 LT 21+30.0 13.5 LT 26 PARCEL 180 DRIVEWAY R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 83 83 83 83 84 83 83 83 84 84 84 84 83 83 84 84 84 84 83 84	R2	19+87.0	13.5 LT	20+23.0	13.5 LT		20	PARCEL 178 DRIVEWAY
R2 21+30.0 13.5 LT 22+80.8 14.7 LT 83 R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	20+23.0	13.5 LT	20+84.0	13.5 LT	34		
R2 21+77.0 13.5 RT 22+81.8 14.8 RT 58 R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	20+84.0	13.5 LT	21+30.0	13.5 LT		26	PARCEL 180 DRIVEWAY
R2 22+93.7 27.4 LT 22+94.6 33.5 LT 3 R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	21+30.0	13.5 LT	22+80.8	14.7 LT	83		
R2 22+93.8 27.4 RT 22+94.7 33.5 RT 3 R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	21+77.0	13.5 RT	22+81.8	14.8 RT	58		
R10 46+03.9 13.0 RT 46+03.9 25.1 RT 7 R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	22+93.7	27.4 LT	22+94.6	33.5 LT	3		
R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP	R2	22+93.8	27.4 RT	22+94.7	33.5 RT	3		
R10 46+06.4 12.7 LT 16+08.9 13.0 RT 13 BIKE RAMP								
	R10	46+03.9	13.0 RT	46+03.9	25.1 RT	7		
R10 46+26.3 28.6 RT 16+40.2 28.6 RT 15 PEDESTRIAN REFUGE ISLAND	R10	46+06.4	12.7 LT	16+08.9	13.0 RT		13	BIKE RAMP
	R10	46+26.3	28.6 RT	16+40.2	28.6 RT	15		PEDESTRIAN REFUGE ISLAND

30.04

P.C.C.	P.C.C. CURB RAMP (6" THICK) & DETECTABLE WARNINGS									
CHEET	APPX	OFFSET	CURB RAMP	DETECTABLE	CURB RAMP	REMARKS				
SHEET	STATION	(FT)	AREA (SY)	WARNING AREA (SF)	TYPE					
R1	10+32	27.2 RT	9	10	PARALLEL	SPENARD ROAD				
R2	18+01	21.9 RT	7	9	PARALLEL	NORTH STAR STREET				
R2	18+34	14.5 LT	14	10	PARALLEL	W 30TH AVENUE				
R2	18+36	20.4 RT	11	9	PARALLEL	NORTH STAR STREET				
R2	18+61	18.7 RT	7		PARALLEL	PARCEL 183 WEST				
R2	18+94	19.4 RT	7		PARALLEL	PARCEL 183 WEST				
R2	21+14	14.8 RT	9		UNIDIRECTIONAL	PARCEL 183 EAST				
R2	21+63	14.8 RT	9	0	UNIDIRECTIONAL	PARCEL 183 EAST				
R2	22+88	20.1 LT	9	10	PARALLEL	ARCTIC BOULEVARD				
R2	22+88	20.1 RT	9	10	PARALLEL	ARCTIC BOULEVARD				
R10	46+08	33.6 RT	9	10	PARALLEL	ARCTIC BOULEVARD				
R10	46+29	33.6 RT	N/A	20	N/A	PEDESTRIAN REFUGE ISLAND				
R10	46+37	33.6 RT	N/A	20	N/A	PEDESTRIAN REFUGE ISLAND				
R10	46+59	33.7 RT	9	10	PARALLEL	ARCTIC BOULEVARD				

PCC CURB RAMP & DETECTABLE WARNING NOTES:

1. SEE INTERSECTION LAYOUT SHEETS R5-R11 FOR FOR LOCATIONS OF CURB RAMPS AND DETECTABLE WARNINGS.

30.10

COLORE	COLORED CONCRETE (4" THICK, RED, RUNNING BOND TILE PATTERN)										
SHEET	APPX BEGIN STA	APPX OFFSET (FT)	APPX END STA	APPX OFFSET (FT)	4" THICK, AREA (SY)	REMARKS					
R2	16+51.1	16.0 LT	17+07.3	16.0 LT	14						
R10	46+30.1	68.3 RT	46+33.0	39.4 RT	6	REFUGE ISLAND					
R10	46+33.5	27.9 RT	46+33.5	21.8 RT	22	REFUGE ISLAND					

1. DATA PROVIDED BY: __ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: ______ TITLE: _____ DATE: ____ 2. DATA TRANSFERRED BY: ______ ______ TITLE: _____ DATA TRANSFER CHECKED BY: ____

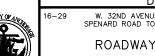
_ DATE:_

COMPANY: _

DATA	DRAWN BY	CHECKED								
E	TS	MJ								
OGRAPHY	BW	BW								
FILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	
RM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77				
ER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10				
	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53				
PHONE	RK	JK								
CTRIC	JH	TK								
IGN	RB	JK	ASBUILT							
NTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST					
LIMINARY/FINAL	RB	JK	INSPECTOR							
IICIPAL/STATE	RB	JK								







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

ROADWAY SUMMARY TABLES

SCALE HOR. N/A VER. N/A

GRID SW1629, SW1630

DATE NOV 2019 STATUS 65%

SANITARY SEWER MANHOLE NOTES:

1. SEE MASS DETAIL 50-01.

50.06

REMOVE A	REMOVE AND REPLACE MANHOLE CONE SECTION OR MANHOLE COVER AND FRAME									
SHEET	STATION	OFFSET (FT)	CONE SECTION	COVER AND FRAME	REMARKS					
R2	18+05	5.0 LT		X						
R2	21+65	5.2 LT	X							
R3	34+72	14.3 LT		X						

SANITARY SEWER MANHOLE CONE/RING NOTES:

1. SEE MASS DETAILS 50-25 AND 50-26.

2. COORDINATE W/ ENGINEER IN FIELD TO VERIFY WHETHER CONE SECTION OR MANHOLE COVER AND FRAME REPLACEMENT IS REQUIRED.

55.07 & 55.08

ADJUST S	ADJUST STORM DRAIN MANHOLE CONE OR RING									
SHEET	STATION	OFFSET (FT)	CONE	RING	REMARKS					
R2	23+20	16.6 LT		X						
R4	30+34	12.7 RT		X						
R4	46+10	21.9 RT		X						
R4	46+33	20.3 RT	X							

ADJUST STORM DRAIN MANHOLE RING NOTES:

1. SEE MASS DETAIL 55-17 AND 55-18.

60.04

FURNISH /	AND INSTALL	FIRE HYDRAN	T ASSEMBLY SINGLE PUMPER
SHEET	STATION	OFFSET (FT)	REMARKS
B1	16+25	23.0 RT	CONNECT TO EXISTING HYDRANT LEG

70.18

CHAIN LINK FENCE (6', 9 GAUGE)

CHAIN	HAIN LINK FENCE (6, 9 GAUGE)									
		PROPOSED	LOCATION							
	APPROX BEGIN	APPROX BEGIN	APPROX END	APPROX END	LENGTH	REMARKS				
SHEET	STATION	OFFSET (FT)	STATION	OFFSET (FT)	(FT)					
R4	45+00.0	10.0 LT	45+54.4	10.0 LT	54.4	INSTALL BARBED WIRE PER MASS WITH VINYL SLATS TO MATCH COLOR OF RESET FENCE				

60.03 & 60.05

REMOVE	AND REPL	ACE VALVE	BOX TOP	SECTION OR ADJUST KEY	Y BOX
SHEET	STATION	OFFSET (FT)	KEY BOX	VALVE BOX TOP SECTION	REMARKS
R1	10+38	20.4 RT		X	
R1	10+45	26.5 RT		X	
R1	10+72	19.5 LT	Х		
R1	11+13	20.3 RT	Х		
R1	12+03	23.5 RT	Х		
R1	12+24	23.5 RT	Х		
R1	12+31	28.7 RT	Х		
R1	12+61	22.1 LT	Х		
R1	13+31	31.9 RT	Х		
R1	13+46	24.4 LT	Х		
R1	13+70	23.8 LT	Х		
R1	13+87	21.4 RT	Х		
R1	14+22	23.8 LT	Х		
R1	14+66	21.4 LT	Х		
R1	15+11	16.7 LT	Х		
R1	15+47	21.3 RT	Х		
R1	15+97	29.7 RT	X		
R2	16+30	21.3 RT	X		
R2	17+18	14.4 RT		X	
R2	17+21	24.9 LT	Х		
R2	17+84	21.0 RT	Х		
R2	19+43	21.4 LT	Х		
R2	20+07	18.2 RT	Х		
R2	20+20	17.8 RT		X	
R4	46+23	16.7 RT		X	
R4	46+25	38.6 RT		X	
R4	46+26	28.2 RT		X	

SPECIAL FILL GRADING TABLE									
SHEET	APPROX BEGIN STATION	APPROX END STATION	OFFSET	REMARKS					
R1	11+33	11+57	21.0 LT						
R1	11+64	11+71	21.0 LT						
R2	19+08	20+15	18.5 LT						
R2	20+70	20+90	18.5 LT						
R2	21+24	21+60	18.5 LT						
R3	30+40	30+70	12.0 LT						
R3	31+05	31+75	12.0 LT						
R3	33+61	33+88	12.0 LT	_					
R3	34+20	34+74	12.0 LT						

SPECIAL FILL GRADING NOTES:

- 1. SPECIAL FILL GRADING SHALL BE PER DETAIL 2, SHEET C5.
- 2. LOCATIONS ARE APPROXIMATE, CONTRACTOR SHALL MODIFY LOCATIONS IN THE FIELD PER THE DIRECTION OF THE ENGINEER OR AS NECESSARY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

BASE
NTATION TOPOGRA
PROFILE
STORM S
WATER/S
GAS
TELEPHO
ELECTRIC
LOTED DESIGN
QUANTITI
PRELIMIN
MUNICIPA

DATA	BY	BY		
	TS	MJ		
PHY	BW	BW		
	RB	JK	FIELD BOOKS	BM NO. LOCATION ELEV. REV DATE DESCRIPTION
SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-56 94.77
SANITARY SEWER	RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-17 106.10
	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53
NE	RK	JK		
C	JH	TK		
	RB	JK	ASBUILT	
TES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST
NARY/FINAL	RB	JK	INSPECTOR	
AL/STATE	RB	JK		
PLAN (CHECK		CONSTRUCTION RECORD	VERTICAL DATUM REVISIONS

CRW
ENGINEERING GROUR LLC Robert W. Burdick CE-123959 3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECL882-AK

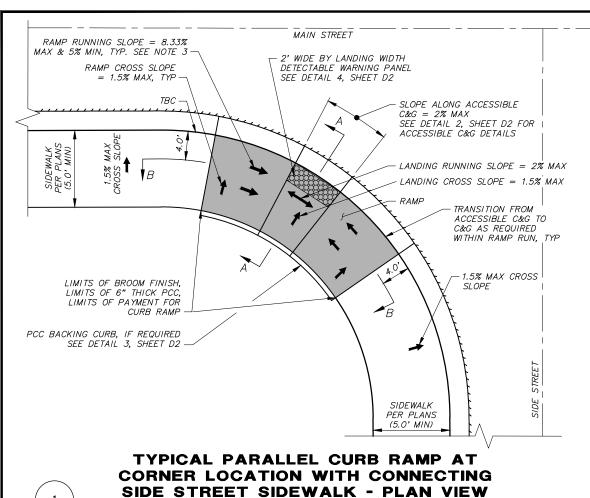




W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

ROADWAY SUMMARY TABLES

GRID SW1629, SW1630 SCALE HOR. N/A DATE NOV 2019 STATUS 65%



PCC BACKING CURB. IF REQUIRED SEE DETAIL 3. SHEET D2 LIMITS OF BROOM FINISH, RAMP CROSS SLOPE = 1.5% LIMITS OF 6" THICK PCC, MAX, TYP LIMITS OF PAYMENT FOR CURB RAMP RAMP RUNNING SLOPE = 8.33% MAX AND 5% MIN, TYP. SEE NOTE 3 (5.0' MIN MA) OSS OPE 5% CRC TRO RAMPTRANSITION FROM ACCESSIBLE C&G TO C&G AS REQUIRED WITHIN RAMP LANDING CROSS SLOPE = 1.5% MAXLANDING RUNNING SEE DETAIL 2, SHEET D2 FOR ACCESSIBLE CURB DETAILS SLOPE = 2% MAX MAIN STREET 2' WIDE BY LANDING WIDTH DETECTABLE WARNING PANEL

SCALE: NTS

TYPICAL PARALLEL CURB RAMP AT **NON-CORNER LOCATION - PLAN VIEW**

SCALE: NTS

SHEET NOTES:

- SEE SHEETS R5-R10 FOR CURB RAMP TYPES, LOCATIONS, RAMP, LANDING AND FLARE LENGTHS AND ELEVATIONS. RAMP/FLARE/LANDING LENGTH FOR PARALLEL CURB RAMPS SHALL BE AS MEASURED 4' OFF BACK OF CURB.
- 2. NOTIFY ENGINEER PRIOR TO INSTALLATION OF CONCRETE IF MAXIMUM/MINIMUM SLOPES CANNOT BE MAINTAINED.
- 3. FOR PARALLEL CURB RAMPS, RAMPS SHALL BE 15 FEET MAXIMUM. RAMPS SHALL HAVE THE OUTSIDE EDGES AND JOINTS TRIMMED WITH A 1/4-INCH RADIUS EDGING
- 4. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
- 5. MINIMUM FLOWLINE SLOPE IN CURB RETURN IS 0.5%, UNLESS OTHERWISE NOTED.
- 6. PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- CONSTRUCT SIDEWALK ADJACENT TO CURB RAMP PER THE TYPICAL SECTIONS SHOWN ON THE "C" SHEETS.
- 8. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "P.C.C. CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
- FORM BACKING CURB AS DIRECTED BY THE ENGINEER TO MATCH EXISTING GROUND. PAYMENT FOR THIS CURB SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE. IF EXISTING GROUND BEHIND SIDEWALK IS GRAVEL OR GRASS, GRADE TO MATCH EXISTING GROUND. PAYMENT FOR GRADING SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK" AND NO ADDITIONAL PAYMENT SHALL BE MADE. 4" TOPSOIL AND SEEDING SHALL BE PLACED ON DISTURBED GRASS AREAS PER THE LANDSCAPING PLANS.
- 10. CONSTRUCT RAMPS AND LANDINGS WITH A BROOM FINISH RUNNING PERPENDICULAR TO THE DIRECTION OF TRAVEL.
- 11. INSTALL YELLOW ADA APPROVED DETECTABLE WARNINGS (DW) PANELS UNLESS OTHERWISE NOTED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND THESE DRAWINGS. SET DETECTABLE WARNINGS SO THAT THE FIELD AREA AT THE BASE OF THE DOMES IS FLUSH WITH THE SURROUNDING CONCRETE. THERE SHALL BE NO LIP AT THE EDGE OF THE DETECTABLE CURB WARNINGS. SEE DETAIL 4. SHEET D2.
- 12. DETECTABLE WARNINGS DOMES AT PARALLEL CURB RAMPS SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINATE DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
- 13. RAMP LOCATIONS MAY BE ADJUSTED TO ENSURE MINIMUM 48" CLEARANCE AROUND APPURTENANCES SUCH AS SIGNAL POLES, POWER POLES, LIGHT POLES, J-BOXES, SIGNS. CATCH BASINS AND MANHOLES. PRIOR TO PLACEMENT OF CONCRETE AND APPURTENANCES, THE RAMP LAYOUT AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
- 14. GAP BETWEEN DETECTABLE WARNING PANELS AND BACK OF CURB ONLY ALLOWABLE AT CENTER OF CURB RAMPS. CORNERS OF DETECTABLE WARNINGS SHALL BE FLUSH WITH BACK OF CURB. IF REQUIRED BY THE ENGINEER CONTRACTOR SHALL CUT DETECTABLE WARNING PANELS PER THE MANUFACTURER'S RECOMMENDATIONS. CUTTING DW PANELS SHALL BE INCIDENTAL TO 30.04 DETECTABLE WARNINGS PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.

TYPICAL PARALLEL CURB RAMP AT CORNER LOCATION WITHOUT CONNECTING SIDE STREET SIDEWALK - PLAN VIEW SCALE: NTS

2' WIDE BY LANDING WIDTH DETECTABLE

C&G DETAILS

WARNING PANEL SEE DETAIL 4, SHEET D2

SLOPE ALONG ACCESSIBLE C&G = 2% MAX

SEE DETAIL 2, SHEET D2 FOR ACCESSIBLE

- LANDING RUNNING SLOPE = 2% MAX

- LANDING CROSS SLOPE = 1.5% MAX

TRANSITION FROM ACCESSIBLE C&G TO C&G AS REQUIRED WITHIN

RAMP/FLARE RUN. (TYP)

STREET

SIDE

PCC BACKING CURB VARIES 0-6"± AS DIRECTED BY THE ENGINEER. SEE DETAIL 3, SHEET D2 & NOTE 9 THIS SHEET. MATCH-**EXISTING** → MAX 6" (SEE NOTE 14) DW CROSS SLOPE STREET 6" PCC LANDING ACCESSIBLE C&G IAW DETAIL 2, SHEET D2 SUBGRADE PER TYPICAL SECTIONS ON "C" SHEETS SIDEWALK PER PLAN (5.0' MIN) SECTION A-A

TBC 5% MIN 5% MIN 8.33% MAX 2% MAX LANDING 8.33% MAX RAMP LENGTH RAMP LENGTH VARIES SEE VARIES SEE 5' MIN FLOW LINE NOTF 1 NOTF 1

SECTION B-B SLOPE VARIES 2% MAX AS REQUIRED LANDING 5% MIN 8.33% MAX FLARE LENGTH RAMP I FNGTH VARIES SEE VARIES SEE FLOW LINE NOTE 1

SECTION C-C

TYPICAL CURB RAMP SECTIONS

SCALE: NTS

PROJECT MANAGEMENT AND ENGINEERING **DEPARTMENT** W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1 6-29 49 대 📉 ___ DATE: _ 95 & 196 : 7C See MOA Benchmark Book, Page D−17 CRW AB 86 See MOA Benchmark Book, Page D-18 10-TITLE: ROADWAY DETAILS DATE: ENGINEERING GROUP LLC Justin T. Keene CE-11775

. DATA PROVIDED BY: THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. . DATA TRANSFERRED BY: 3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECL882-AK . BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. CONTRACTOR SIS OF THIS DATUM GAAB 1972 ADJUST DATA TRANSFER CHECKED BY: __ DATE:

MAIN STREET

RAMP RUNNING SLOPE = 8.33%

RAMP CROSS SLOPE = 1.5% MAX, TYP

MAX & 5% MIN, TYP. SEE NOTE 3.

PCC BACKING CURB. IF REQUIRED

3

SEE DETAIL 3, SHEET D2

LIMITS OF BROOM FINISH, LIMITS

OF 6" THICK PCC, LIMITS OF

PAYMENT FOR CURB RAMP

CURB RAMPS

HOR, N/A

1

RECORD DRAWING

CONTRACTOR:

2

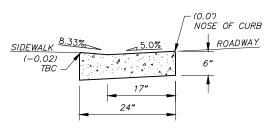
COMPANY:

SEE DETAIL 4, SHEET D2

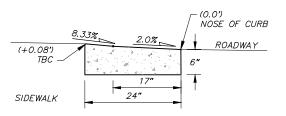
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<u>UNIDIRECTIONAL CURB RAMP NOTES:</u> 1. SEE SHEET NOTES ON SHEET D1.

UNIDIRECTIONAL CURB RAMP SCALE: NTS



PCC CURB AND GUTTER TYPE 1A FOR USE IN CURB RAMPS WITH TYPE 1 C&G.



PCC CURB AND GUTTER TYPE 3A FOR USE IN CURB RAMPS WITH TYPE 3 C&G.

ACCESSIBLE CURB & GUTTER NOTES:

- 1. TRANSITION CURBS TO MAINTAIN CONSTANT FLOWLINE ACROSS CURB RAMP AND AROUND CURB RETURN IAW PLANS.
- 2. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.

ACCESSIBLE CURB & GUTTER SECTIONS (TYPE 1A & TYPE 3A)

SCALE: NTS

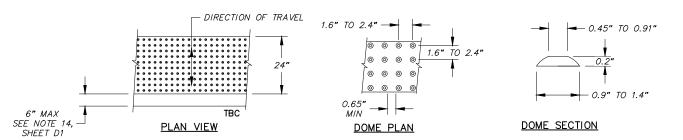
PCC BACKING CURB $-\frac{1}{2}$ " CHAMFER, TYP. LANDING SURFACE

BACKING CURB DETAIL NOTES:

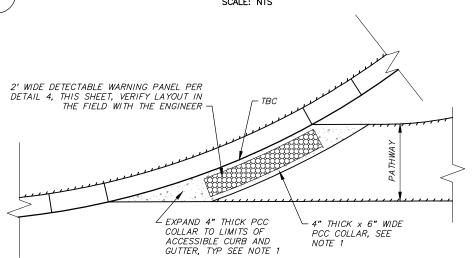
1. THE TOP OF BACKING CURB SHALL TRANSITION BACK TO TOP OF SIDEWALK AT TOP RAMP SECTION OF CURB RAMP.

MONOLITHIC

BACKING CURB DETAIL 3 SCALE: NTS



DETECTABLE WARNING PANEL SCALE: NTS



DETECTABLE WARNING PANEL IN PATHWAY NOTES:

1. ALL WORK & MATERIALS REQUIRED FOR INSTALLING DETECTABLE WARNING PANEL SHALL BE INCIDENTAL TO ITEM 30.04 DETECTABLE WARNINGS PAY ITEM AND NO SEPERATE PAYMENT SHALL BE MADE.

DETECTABLE WARNING PANEL IN PATHWAY

SCALE: NTS

RE	CORD DRAWING			
1.	DATA PROVIDED BY:		TITLE:	- BASE
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS OF THE PROJECT AS CONSTRUCTED.	ARE A	TRUE AND ACCURATE REPRESENTATION	TOPO
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э.	BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO	OFFICE	ENT THE DROJECT AS CONSTRUCTED	DESIG
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FILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
ORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
TER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					11 .
S	RK	JK	STAKING	GAAB 86	6 See MOA Benchmark Book, Page D-18	104.53					
EPHONE	RK	JK									ENGIN
CTRIC	JH	TK									3940
SIGN	RB	JK	ASBUILT								3940 ANC
ANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972 ADJUST						Pi
ELIMINARY/FINAL	RB	JK	INSPECTOR								1
NICIPAL/STATE	RB	JK									

CRW NEERING GROUP LLC

5





W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

ROADWAY DETAILS

CURB RAMPS

ALE	HOR.	N/A	GRID SW1629, SW1630			D2	./
	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET		¹ D5

TYPICAL DRIVEWAY CURB RETURN WITH ATTACHED SIDEWALK

SCALE: NTS

SHEET DRIVEWAY NOTES:

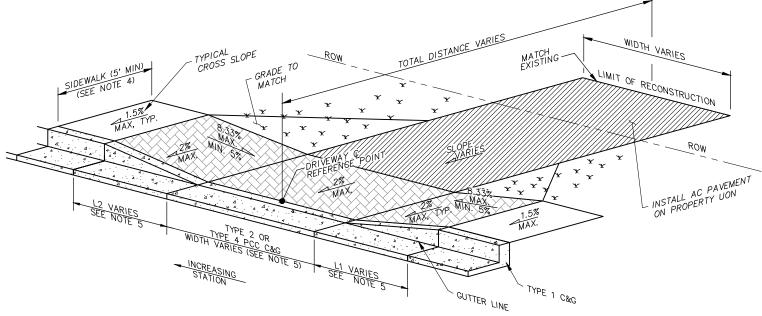
- 1. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
- 2. PAYMENT FOR PCC CURB & GUTTER (ALL TYPES) AND TRANSITION C&G SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER, (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
- 3. CENTER THE PROPOSED DRIVEWAY ENTRANCES ON DRIVEWAY CENTERLINE REFERENCE POINT AS SHOWN IN THE 20.28 RECONSTRUCT DRIVEWAY SUMMARY TABLES PROVIDED ON THE ROADWAY SUMMARY TABLE "T" SHEETS OR AS SHOWN ON THE DRIVEWAY RECONSTRUCTION PLANS.
- 4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS LANDINGS AND RAMP TRANSITIONS AND ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
- 5. SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLES ON THE ROADWAY SUMMARY TABLE "T" SHEETS AND DRIVEWAY RECONSTRUCTION PLANS, FOR INDIVIDUAL DRIVEWAY SPECIFICS.
- 6. WHERE INSULATION IS INSTALLED IN ROADWAY, INSTALL INSULATION UNDER DRIVEWAY PER DETAIL 4, SHEET C5.

SHEET DRIVEWAY LEGEND:

LIMITS OF 2" AC PAVING (CLASS E) FOR DRIVEWAY

LIMITS OF BROOM FINISH, LIMITS OF 6" THICK PCC, LIMITS OF PAYMENT FOR CURB RAMP, SEE NOTE 5

LIMITS OF PCC SIDEWALK (6" THICK, STANDARD FINISH), SEE NOTE 4



TYPICAL DRIVEWAY CURB-CUT WITH ATTACHED SIDEWALK

SCALE: NTS

RECORD DRAWING			
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	RB	JK	FIELD BOOKS	BM NO. LOCATION ELEV. REV DATE DESCRIPTION
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TARY SEWER	RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-17 106.10
	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53
	RK	JK		
	JH	TK		
	RB	JK	ASBUILT	
	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST
Y/FINAL	RB	JK	INSPECTOR	
STATE	RB	JK		
DI AN CHECK CONSTRI			CONSTRUCTION RECORD	VERTICAL DATUM







PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

6-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED / SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

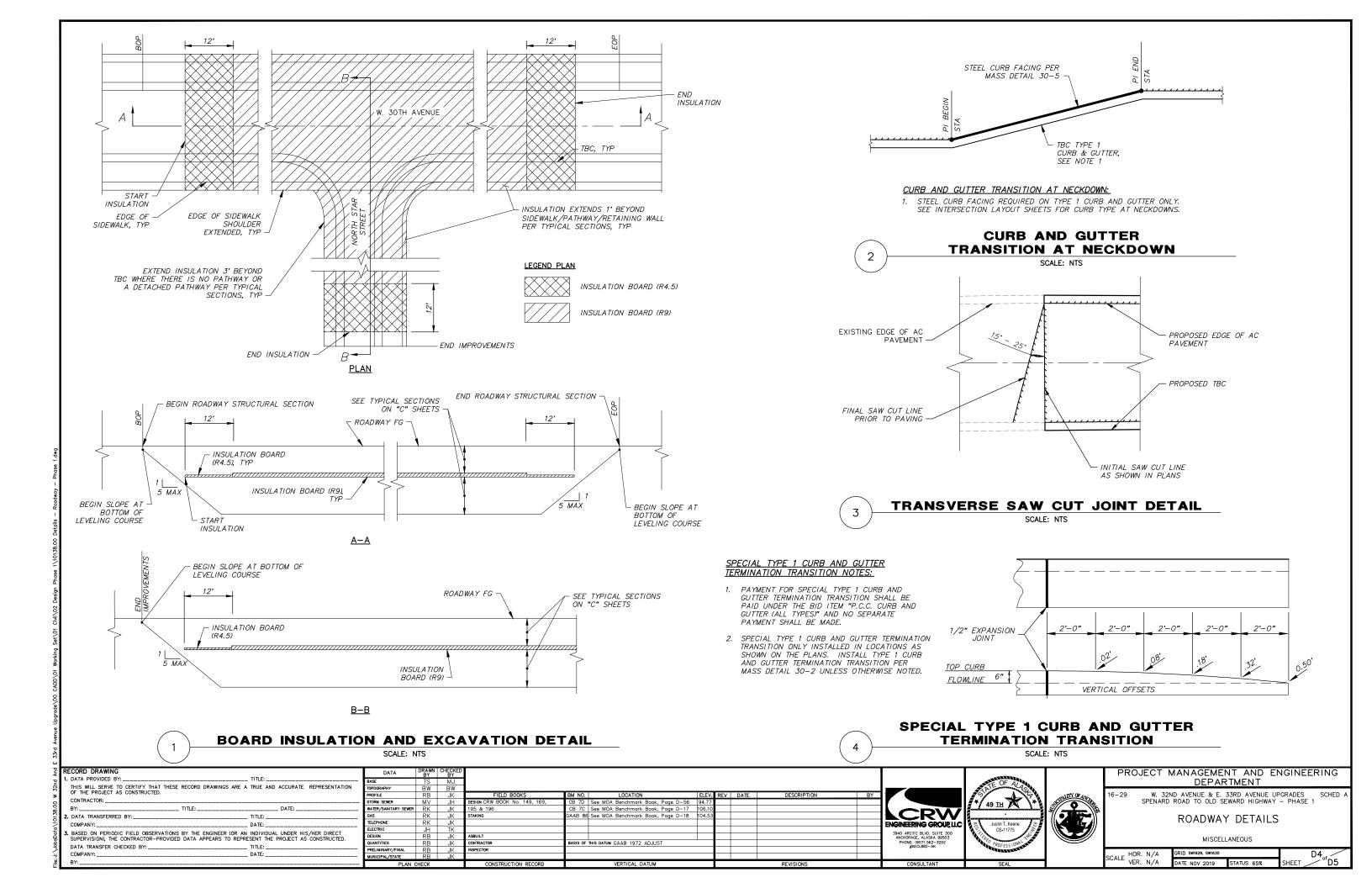
ROADWAY DETAILS

DRIVEWAYS

SCALE HOR. N/A GRID SW629, SW630

VER. N/A DATE NOV 2019 STATUS 65% SHEET D3

Of D5



TYPICAL WOOD POST MAILBOX INSTALLATION NOTES:

- SEE "RELOCATE MAILBOX" TABLE, DEMOLITION SHEETS & ROADWAY SHEETS FOR LOCATING MAILBOXES ALONG ROADWAY. LOCATIONS ARE APPROXIMATE, VERIFY LOCATION WITH ENGINEER PRIOR TO INSTALLATION.
- 2. MAILBOXES AND SUPPORTS SHALL CONFORM WITH U.S. POSTAL SERVICE REGULATIONS.
- 3. NEWSPAPER RECEPTACLES SHALL CONFORM TO THE SAME SETBACK AND SUPPORT REGULATIONS AS MAILBOXES. WHERE NEWSPAPER RECEPTACLES AND MAILBOXES ARE TO BE MOUNTED TOGETHER, THE NEWSPAPER RECEPTACLE SHALL BE MOUNTED BELOW THE BOTTOM SURFACE OF THE MAILBOX.
- 4. CONTRACTOR SHALL COORDINATE WITH THE MOA AND ENGINEER IN THE FIELD REGARDING MAILBOX SUBSTITUTIONS OR MAILBOX SIZING, PRIOR TO ORDERING MATERIALS.
- 5. CONTRACTOR SHALL INSTALL MAILBOX ADDRESS LABELS TO MATCH EXISTING LABELS. ADDRESS LABELS SHALL BE A MINIMUM OF 1" IN HEIGHT AND INSTALLED ON THE SIDE OF THE MAILBOX VISIBLE FROM ON COMING TRAFFIC. ADDRESS LABELS SHOULD BE CENTERED BOTH VERTICAL AND HORIZONTAL ON MAILBOX.
- 6. ALL WOOD SHALL BE PRESSURE TREATED WOOD SEALED WITH A SEMI-TRANSPARENT OIL BASED STAIN BROWN IN COLOR. SUBMIT COLOR SAMPLE FOR APPROVAL.

FOR MAILBOX LOCATIONS BEHIND SIDEWALK AS SHOWN ON THE PLANS

TYPICAL WOOD POST MAILBOX INSTALLATION

SCALE: NTS

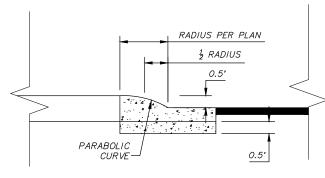
CURB NOSE MEDIAN DETAIL

SCALE: NTS

CURB TYPE PER PLAN FLOWLINE CURB NOSE MEDIAN PAINT, SEE NOTE 1 RADIUS PER PLAN CURB TYPE PER PLAN

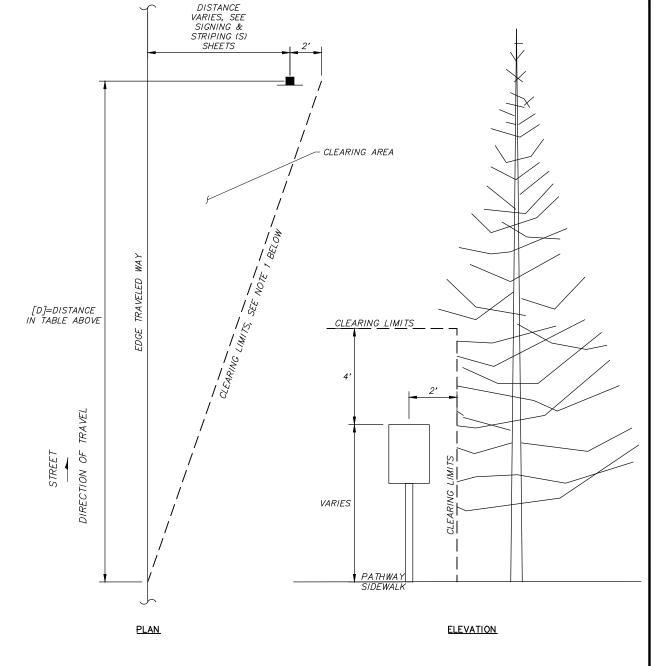
CURB NOSE MEDIAN DETAIL NOTE:

1. CURB NOSE MEDIAN SHALL BE PAINTED WITH YELLOW REFLECTIVE TRAFFIC PAINT. PAINTING & MATERIALS SHALL BE INCIDENTAL TO CURB NOSE MEDIAN PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.



SECTION A-A

CLEARING DISTANCE SPEED LIMIT DISTANCE[D] (MPH) (FT) 20 100 25 125 30 150 35 175



SIGN SIGHT DISTANCE CLEARING DETAIL NOTE:

1. MAINTAIN CLEARING LIMITS WITHIN AVAILABLE RIGHT-OF-WAY.

3 SIGN SIGHT DISTANCE CLEARING DETAIL SCALE: NTS

RECORD DRAWING 1. DATA PROVIDED BY: THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: BY: TITLE: DATE: 2. DATA TRANSFERRED BY: COMPANY: 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. DATA TRANSFER CHECKED BY: DATE: COMPANY: DATE:

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Æ	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION
SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77			
/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10			ĺ
	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53			
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ENGINEERING GROUP LIC

3940 ARCITIC BLVD. SUITE 30
ANCHORACE, ALASKA 99903
PHONE: 1907 1962–2952
ACCL882–AX



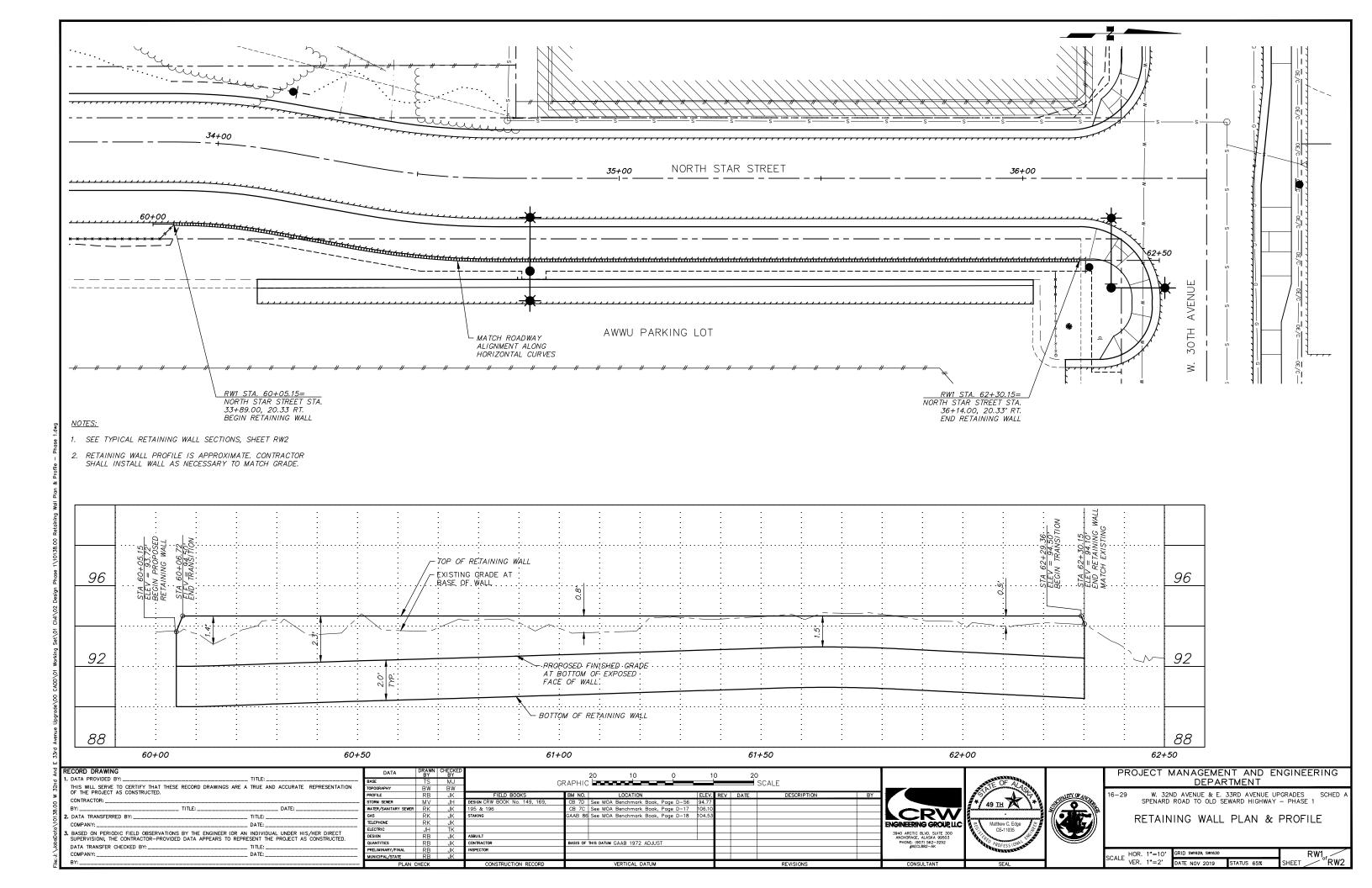


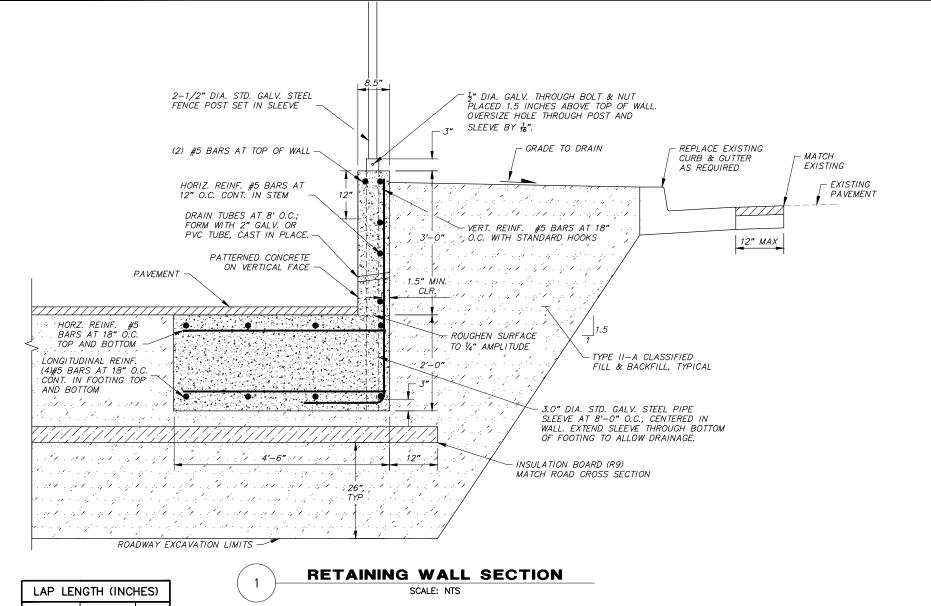
PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

ROADWAY DETAILS

MISCELL ANEOUS





BAR SIZE NORMAL TOP

#4 24 31

#5 30 38

#6 35 46

#7 51 67

59

66

74

#8

#9

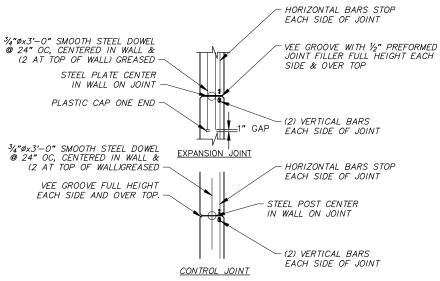
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76

86

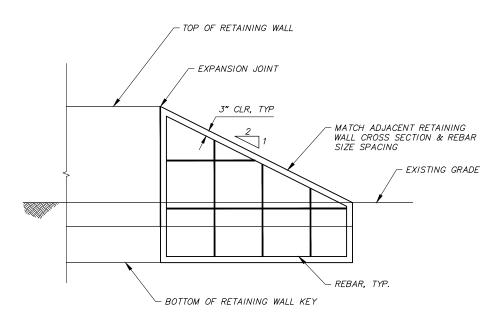
96

SCALE: NTS



NOTE: PROVIDE JOINTS @ EACH POST AT AND 20' MAX ON CENTER IN ALL CONCRETE RETAINING WALLS AND AT EDGES OF TRANSITIONS.
EVERY 4TH JOINT SHALL BE AN EXPANSION JOINT.

2 VERTICAL JOINTS PLAN VIEW SCALE: NTS



TRANSITION DETAIL

SCALE: NTS

3	RI	CORD DRAWING		
3	1.	DATA PROVIDED BY:	TITLE:	BASE
3		THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOPO
:		OF THE PROJECT AS CONSTRUCTED.		PROF
2		CONTRACTOR:		STOR
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ă	٠.	SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRE	ESENT THE PROJECT AS CONSTRUCTED.	DESI
3		DATA TRANSFER CHECKED BY:	TITLE	QUAN
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PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark Book, Page D-17	106.10					11 a
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark Book, Page D-18	104.53					
TELEPHONE	RK	JK									ENGIN
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PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

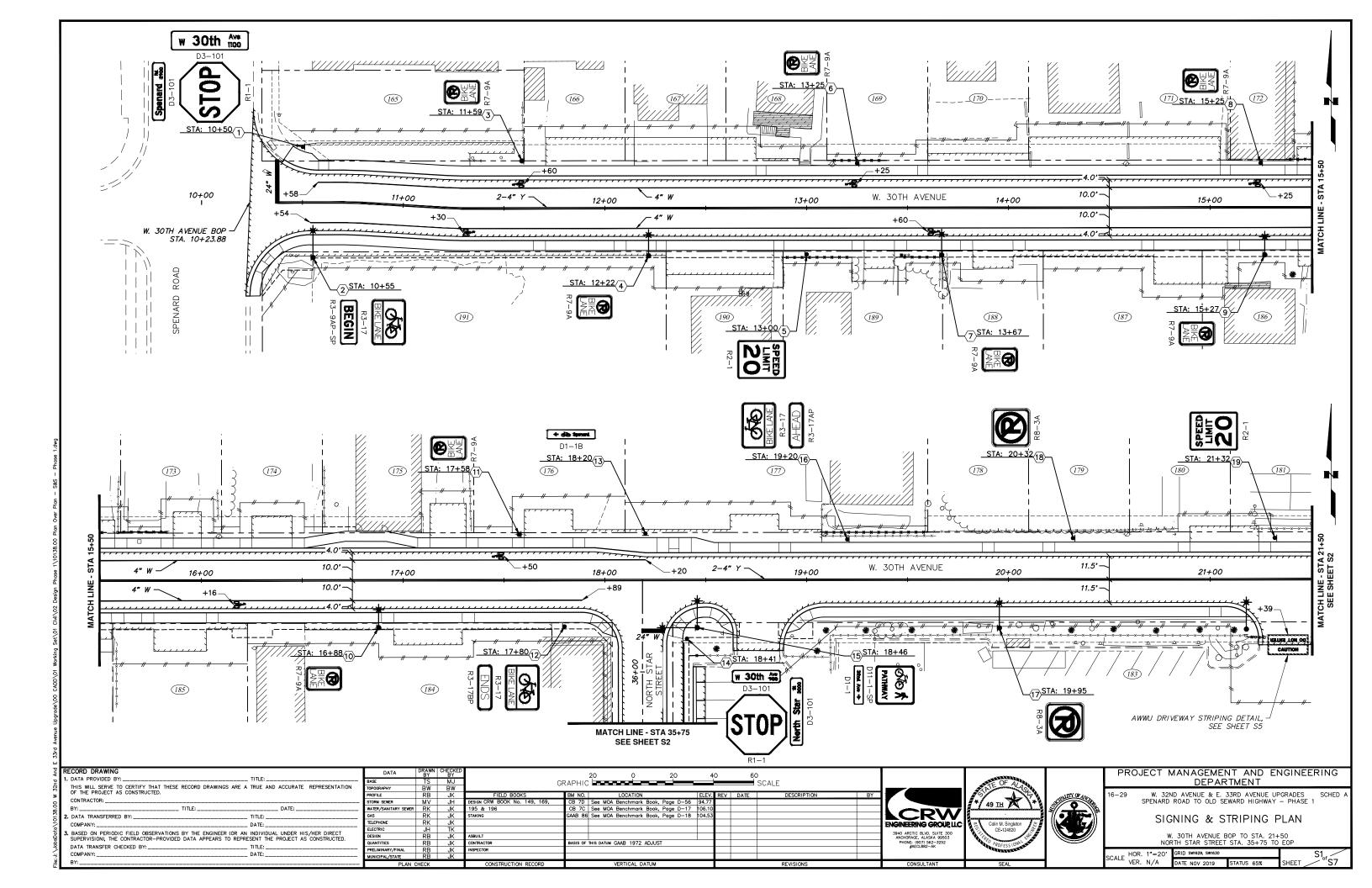
S-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

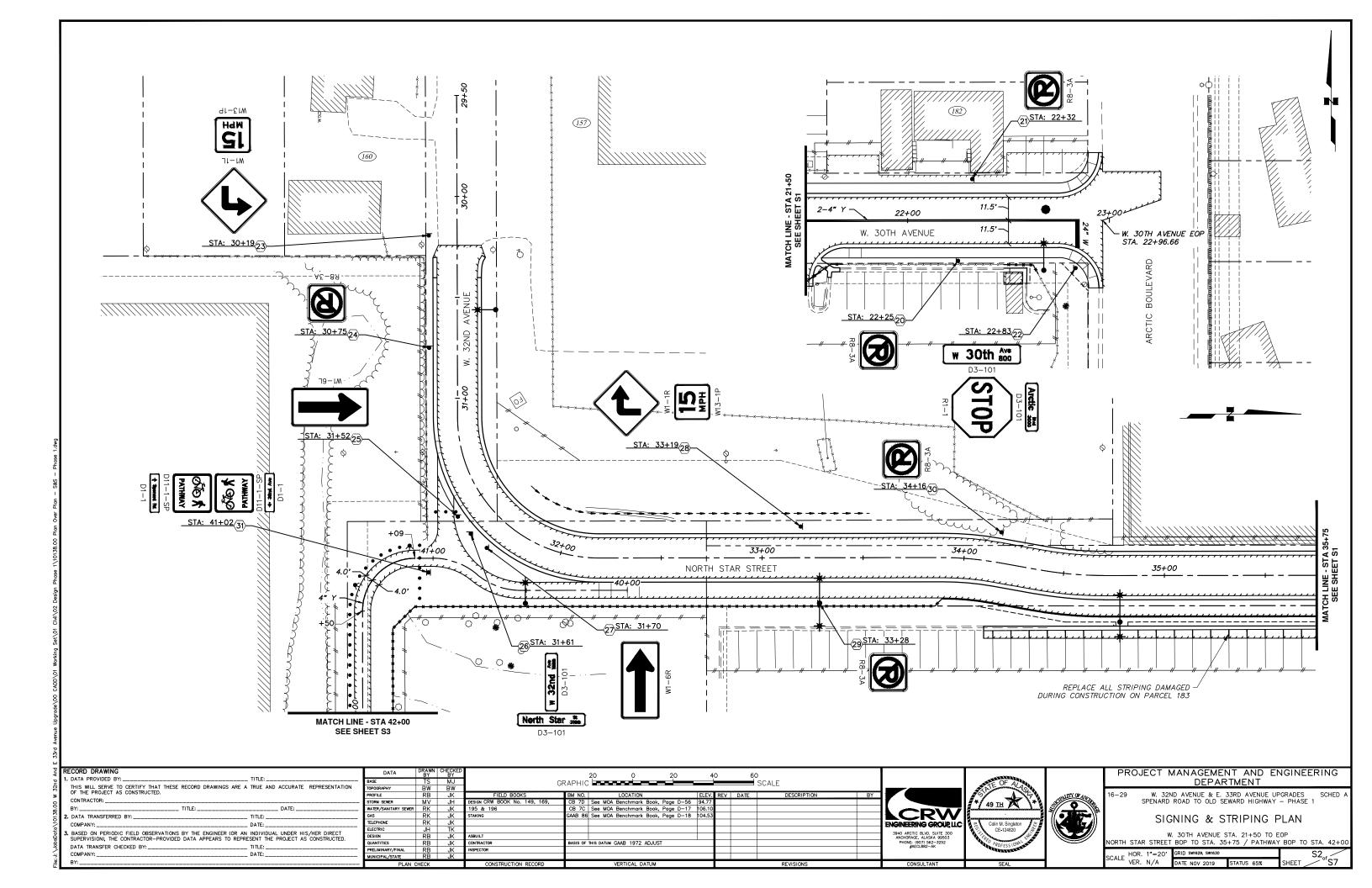
RETAINING WALL DETAILS

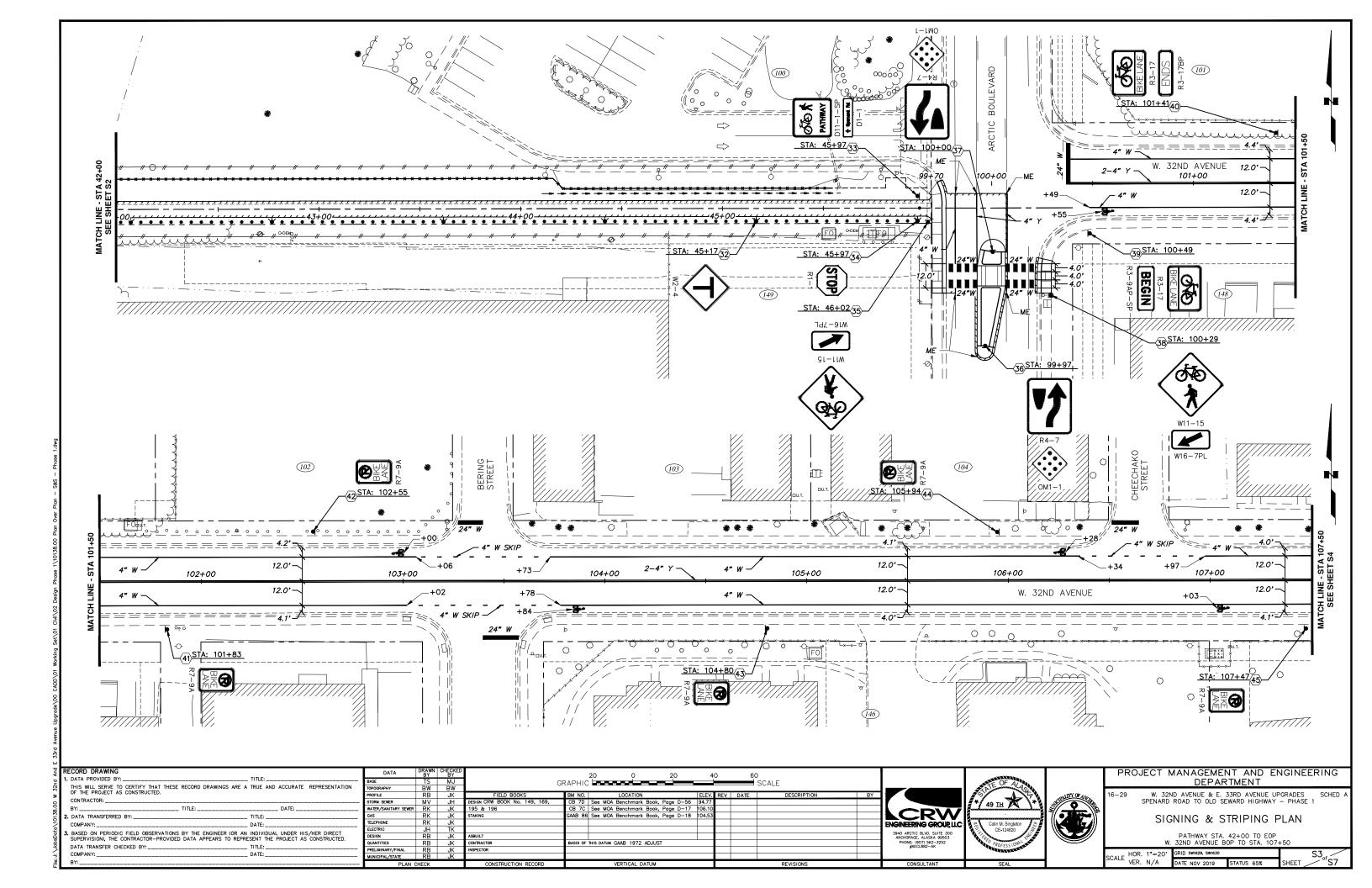
SCALE HOR. N/A GRID SW1629, SW1630

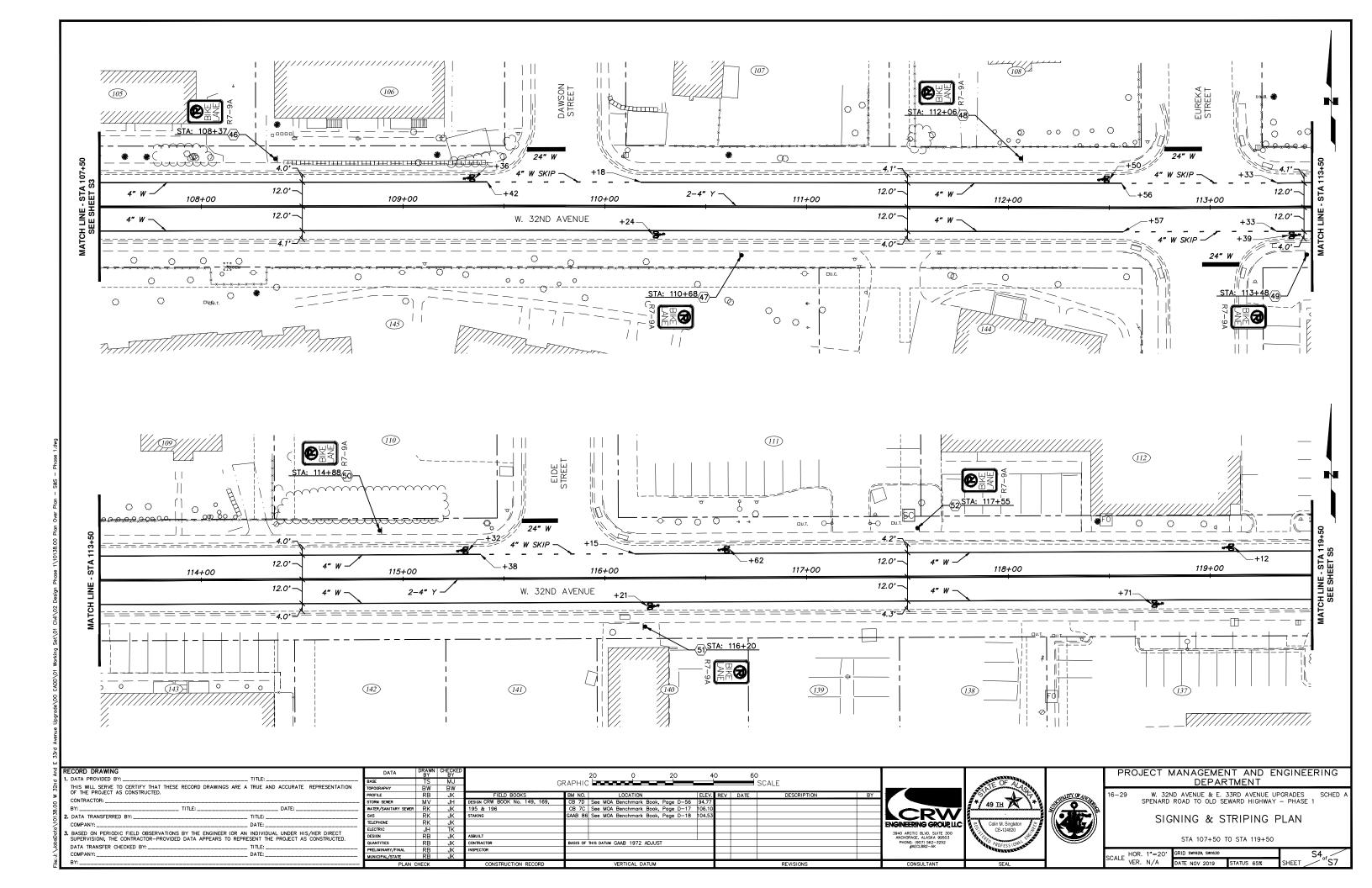
VER. N/A DATE NOV 2019 STATUS 65% SHEET RW2

Of RW2



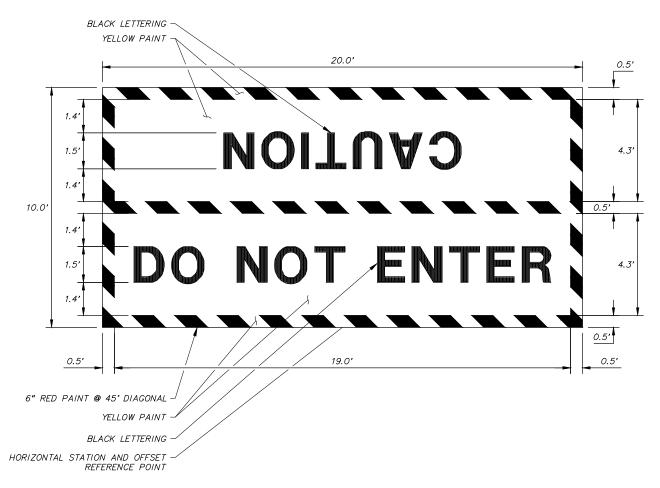




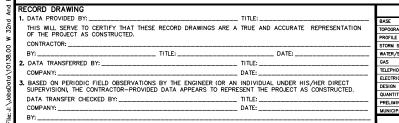


STRIPING NOTES:

- 1. ALL STRIPING SHALL CONFORM TO THESE CONTRACT DOCUMENTS AND STANDARD MASS DETAILS. ALL REVISIONS SHALL CONFORM TO THE LATEST EDITION OF THE ALASKA TRAFFIC MANUAL AND THE MUTCD.
- 3. ALL PAVEMENT MARKINGS SHALL BE INLAID METHYLACRYLATE TO A DEPTH OF 250 MILS.
- 4. DIMENSIONS REFERENCE CENTER OF STRIPE TO CENTER OF STRIPE OR CENTER OF STRIPE TO EDGE OF PAVEMENT.
- 5. "W" REFERENCES WHITE MARKINGS AND "Y" REFERENCES YELLOW MARKINGS.
- 6. INSTALL STOP BARS PER MASS STANDARD DETAIL 70-18.
- 7. 4" WHITE DASH MARKINGS ACROSS SIDE STREETS SHALL BE 2' LONG STRIP WITH 6' GAP.



AWWU DRIVEWAY STRIPING DETAIL



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PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

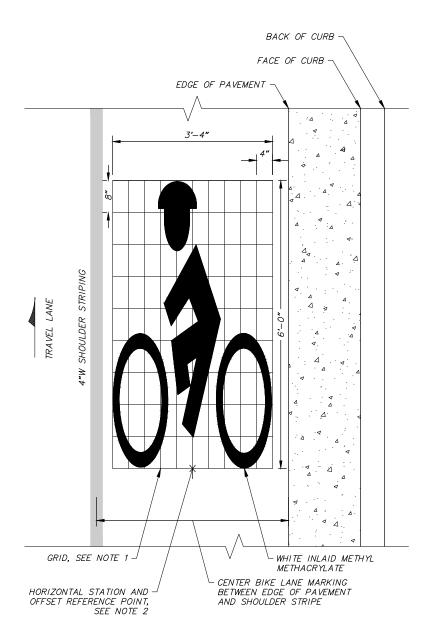
6-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SIGNING & STRIPING PLAN

W. 32ND AVENUE STA 119+50 TO EOP

HOR. 1"=20' GRID SW1629, SW1630 S5

S5 SHEET S7



BIKE LANE MARKING

MARKING NOTES:

- GRID IS SHOWN FOR MARKING STENCIL LAYOUT, DO NOT INCLUDE GRID WHILE INSTALLING BIKE LANE MARKINGS.
- SEE STRIPING PLANS FOR APPROXIMATE LOCATIONS AND ORIENTATION OF BIKE LANE MARKINGS.

	RECORD DRAWING		
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ı	3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AL		ELECTRIC
П	SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO REP		DESIGN
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s	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53	1
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IANTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST	1
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INICIPAL/STATE	RB	JK			
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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SIGN SCHEDULE SUMMARY

S6_{of} S7 GRID SW1629, SW1630 HOR, N/A DATE NOV 2019

					SIGN SCHEDULE SI	UMMARY (CONT'D)				
SHEET NO.	POST NO.	STATION	OFFSET	TYPE	LEGEND	WIDTH (INCHES)	HEIGHT (INCHES)	AREA (SF)	SIGN FACES	SIGN POST	COMMENTS
	32	45+16.71	6.3' RT	W2-4	'T' INTERSECTION	18	18	2.25	W	LIGHT POLE	
1	33	45+96.71	6.0' LT	D11-1-SP	PATHWAY	24	24	4.00	Е	2.5" PST	
	33	45+96.71	6.0 L1	D1-1	SPENARD RD - STRAIGHT ARROW	24	6	1.00	Е	2.5 P31	
	34	45+96.71	6.3' RT	R1-1	STOP	18	18	2.25	W	LIGHT POLE	
	35	46+01.83	7.0' RT	W11-15	BICYCLE AND PEDESTRIAN SYMBOL	30	30	6.25	N	2.5" PST	
	33	40+01.63	7.0 KT	W16-7PL	DIAGONAL ARROW	24	12	2.00	N	2.5 P31	
	36	99+96.68	80.5' RT	R4-7	KEEP RIGHT	24	30	5.00	S	2.5" PST	
	30	99+90.00	80.5 KT	OM1-1	OBJECT MARKER	18	18	2.25	N/S	2.5 F31	BACK TO BCK SIGNS
	37	100+00.48	33.9' LT	R4-7	KEEP RIGHT	24	30	5.00	N	2.5" PST	
	37	100+00.48	55.8	OM1-1	OBJECT MARKER	18	18	2.25	N/S	2.5 F31	BACK TO BCK SIGNS
S3	70	38 100+29.24	55.3' RT	W11-15	BICYCLE AND PEDESTRIAN SYMBOL	30	30	6.25	S	LIGHT POLE	
	56	100+29.24	00.0 1(1	W16-7PL	DIAGONAL ARROW	24	12	2.00	S	LIGHT FOLE	
	39	100+48.53	3 24.8' RT	R3-17	BIKE LANE	24	18	3.00	W	2.5" PST	
	39	100+46.55	24.0 101	R3-9AP-SP	BEGIN	24	8	1.33	W	2.5 F31	
	40	101+40.86	24.9' LT	R3-17	BIKE LANE	24	18	3.00	E	2.5" PST	
			24.9 L1	R3-17BP	ENDS	24	8	1.33	Е	2.5 1 51	
	41	101+83.00	23.7' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	42	102+55.39	24.6' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	2.5" PST	
	43	104+80.44	23.6' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	44	105+94.45	24.0' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	Е	2.5" PST	
	45	107+47.49	24.0' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	46	108+36.70	23.6' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	Е	2.5" PST	
	47	110+67.86	24.1' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	48	112+06.02	24.2' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	Е	2.5" PST	
S4	49	113+47.84	23.6' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	50	114+88.46	24.1' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	E	2.5" PST	
	51	116+19.84	24.1' RT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	W	2.5" PST	
	52	117+55.00	24.2' LT	R7-9A	NO PARKING SYMBOL, BIKE LANE	12	18	1.50	Е	2.5" PST	
	53	119+55.00	23.9' LT	R3-17	BIKE LANE	24	18	3.00	Е	2.5" PST	
S6	54	120+50 00	50.00 25.2' RT	W11-1	BICYCLE SYMBOL	30	30	6.25	W	2.5" PST	
	54	120+50.00	20.2 1(1	W16-1	SHARE THE ROAD	18	24	3.00	W	2.0 F31	

SIGNING NOTES:

- 1. THE STATIONS INDICATED IN THE SIGN SUMMARY ARE APPROXIMATE. INSTALL SIGNS AND SIGN FOUNDATIONS PER MASS STANDARD DETAILS. BEFORE INSTALLING ANY SIGN, STAKE THE LOCATION OF ALL SIGNS FOR THE ENGINEER'S REVIEW AND APPROVAL.
- 2. UNLESS OTHERWISE STATED, PROVIDE PERFORATED STEEL TUBE (PST) SIGN POSTS OF THE SIZE INDICATED IN THE SIGN SUMMARY.
- 3. INSTALL THE POSTS FOR STOP SIGNS AT LOCATIONS THAT CONFORM TO MASS STANDARD DETAILS 70-18.
- 4. ALL STOP SIGNS AND STREET NAME SIGNS SHALL REMAIN OPERATIONAL DURING CONSTRUCTION.
- 5. THE LETTERING FOR STREET NAME SIGNS (D3 SERIES) SHALL BE FEDERAL HIGHWAY ADMINISTRATION "FHWA 2000 SERIES D" LETTERING: A COMBINATION OF LOWER-CASE LETTERS WITH INITIAL UPPER-CASE LETTERS.
- 6. ALL STREET NAME SIGNS WILL BE SINGLE PANEL MOUNTED BACK TO BACK PLACED ON TOP OF THE POST AND BRACED PER MASS DETAIL 70-28, REGARDLESS OF SIGN HEIGHT.
- 7. INSTALL SIGN ON LIGHT POLE ACCORDING TO MASS STANDARD DETAIL 70-30.
- 8. SEE D SHEETS FOR SIGN SIGHT DISTANCE CLEARING DETAIL.

	CORD DRAWING		
٠.	DATA PROVIDED BY:	_ TITLE:	
	THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A	TRUE AND ACCURATE REPRESENTATION	TOPO
	OF THE PROJECT AS CONSTRUCTED.		PROF
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DGRAPHY	BW	BW			
FILE	RB	JK	FIELD BOOKS	BM NO. LOCATION ELEV. REV DATE DESCRIPTION	BY
RM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-56 94.77	
ER/SANITARY SEWER	RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-17 106.10	
	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53	
EPHONE	RK	JK			
CTRIC	JH	TK			
IGN	RB	JK	ASBUILT		
NTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST	
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IICIPAL/STATE	RB	JK			
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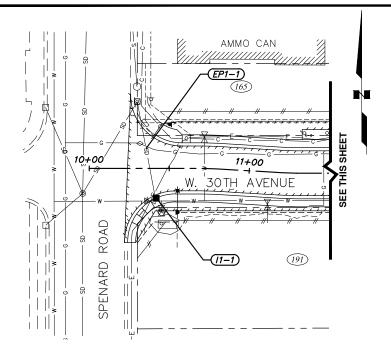


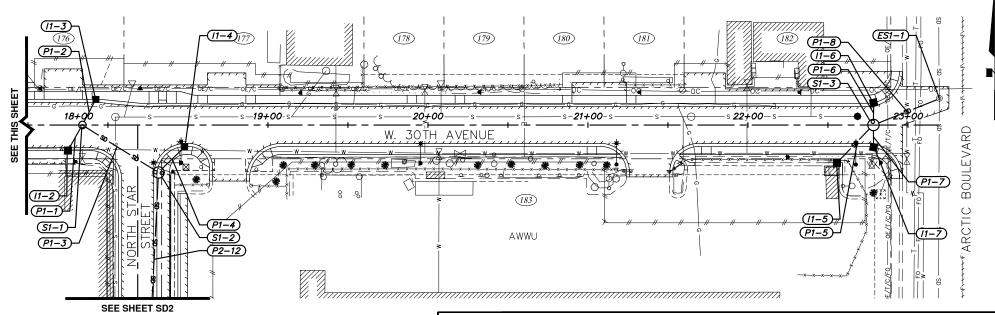
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

6-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED A SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

SIGN SCHEDULE SUMMARY

SCALE	HOR.	N/A	GRID SW1629, SW1630			S
SCALL	VER.	N/A	DATE NOV 2019	STATUS 65%	SHEET	

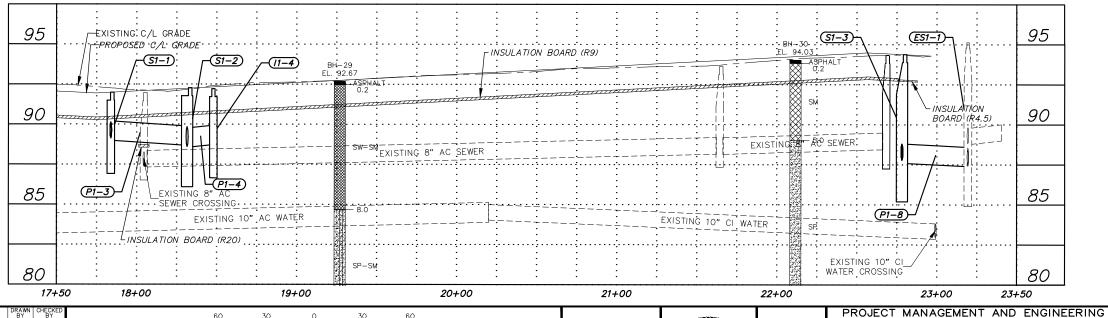




55.02	55.02 - STORM DRAIN												
PIPE	SIZE	PIPE	LENGTH	FROM	TO	INLET	OUTLET	SLOPE					
NAME	(IN.)	TYPE	(FT.)			ELEVATION	ELEVATION						
EP1-1	10	S	-	-	11-1	91.33	-	-					
P1-1	12	S	18.37	11-2	S1-1	89.18	89.13	0.35%					
P1-2	12	S	18.16	11-3	S1-1	89.18	89.13	0.35%					
P1-3	12	S	56.24	S1-1	S1-2	89.08	88.79	0.57%					
P1-4	12	S	23.16	11-4	S1-2	88.81	88.67	0.77%					
P1-5	12	S	34.81	11-5	S1-3	89.90	87.79	7.08%					
P1-6	12	S	13.91	11-6	S1-3	88.68	87.79	9.99%					
P1-7	12	S	13.87	11-7	S1-3	88.67	87.79	9.92%					
P1-8	12	S	44.45	S1-3	ES1-1	87.69	87.50	0.48%					

55.04, 55.	05 & 55.09	STORM	DRAIN STRU	CTURES			
STRUCTURE	TYPE OF	TYPE OF	STATION	OFFSET TO	TOP OF CASTING	CURB	COMMENTS
ID	STRUCTURE	CASTING		STRUCTURE C/L	ELEVATION	TYPE	
11-1	CB/CONNECT	CI	10+41.74	18.61' RT	95.61	1	CONNECT EXISTING PIPE EP1-1
11-2	CB (RED)	CI	17+75.00	16.00' RT	91.92	1	
S1-1	MH I (RED)	МН	17+83.99	CL	91.98	N/A	
11-3	CB (RED)	CI	17+92.50	16.00' LT	91.73	1	
S1-2	MH II (RED)	МН	18+31.63	29.75' RT	92.27	1	
11-4	CB (RED)	CI	18+48.05	13.50' RT	92.49	1	
I1-5	CB	CI	22+55.63	23.60' RT	95.20	1	
S1-3	MH II	МН	22+78.37	CL	94.38	N/A	
11-7	CB	CI	22+78.54	13.87' RT	94.27	1	
11-6	CB	CI	22+78.56	13.90' LT	94.20	1	
ES1-1	CONNECT	мн	23+19.56	16.63' LT	95.13	N/A	EXISTING MH

- 1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE
- 2. REFER TO SHEET SD4 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
- 3. REFER TO SHEETS SD4-SD6 FOR STORM DRAIN DETAILS.



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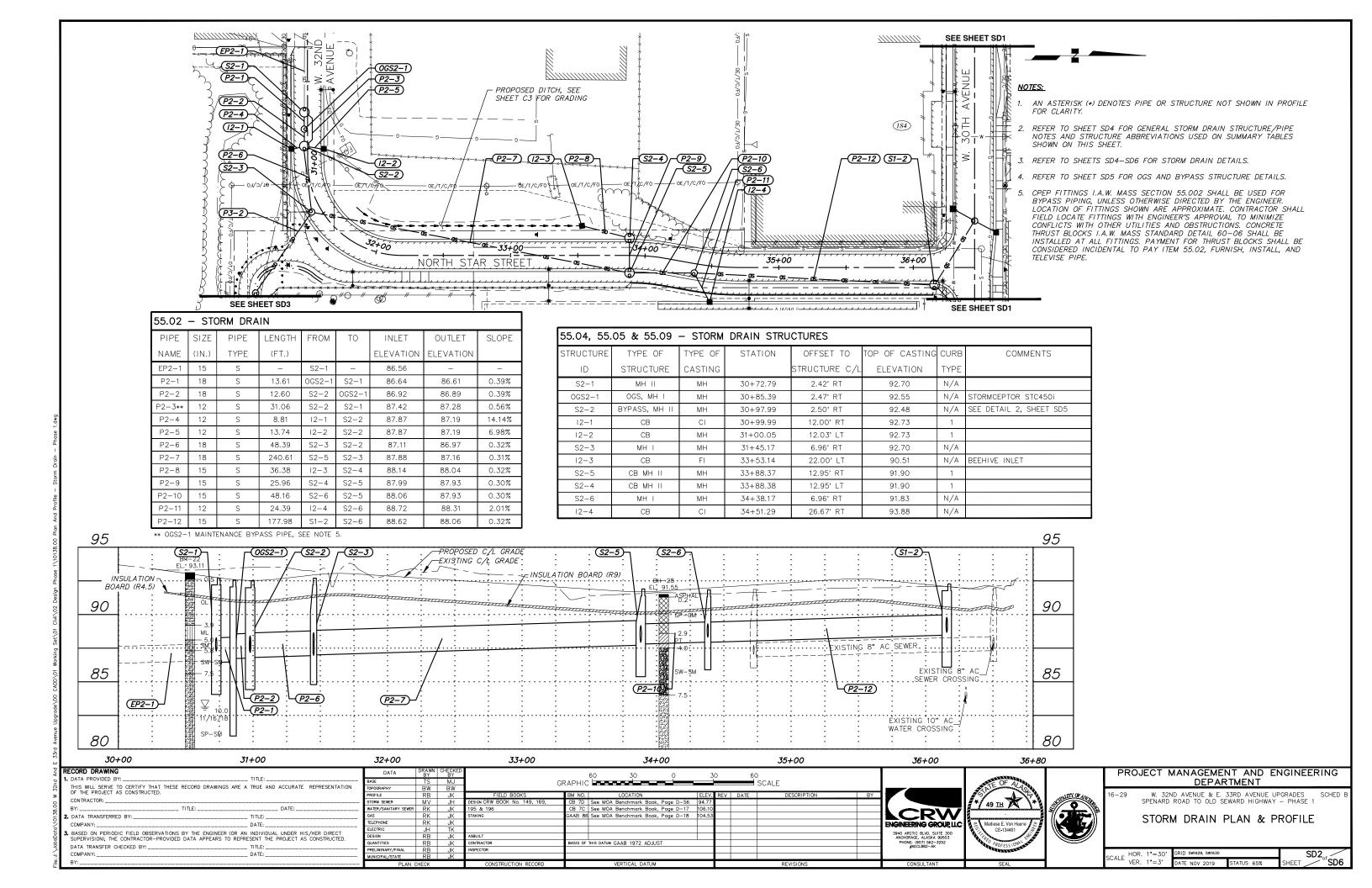
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PROFILE	RB	JK	FIELD BOOKS	BM NO. LOCATION ELEV. REV DATE DESCRIPTION BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-56 94.77	
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-17 106.10	
GAS	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53	INCRV
TELEPHONE	RK	JK			ENGINEERING GROUP LLC
ELECTRIC	Ŧ	TK			3940 ARCTIC BLVD. SUITE 300
DESIGN	RB	JK	ASBUILT		ANCHORAGE, ALASKA 99503
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST	PHONE: (907) 562-3252 #AECL882-AK
PRELIMINARY/FINAL	RB	JK	INSPECTOR		#AECLOB2-AK
MUNICIPAL/STATE	RB	JK			
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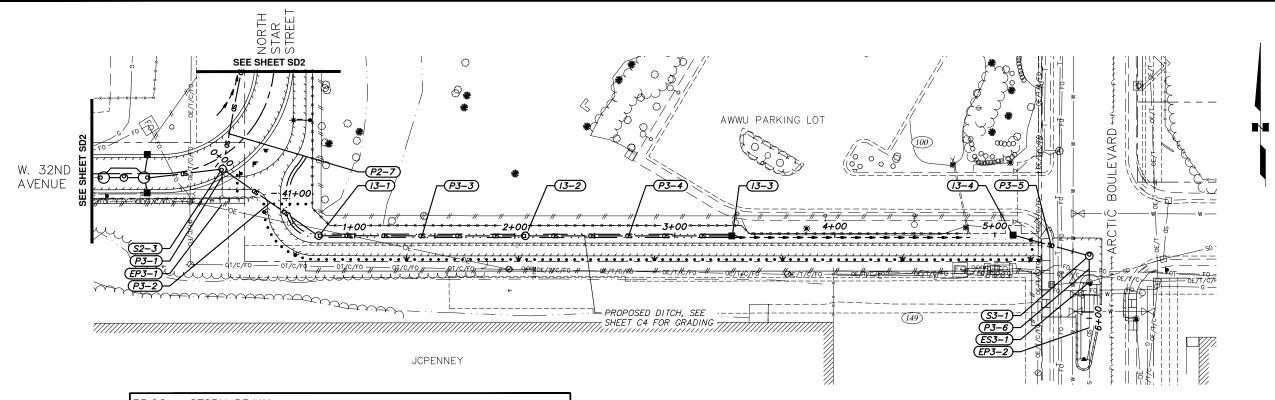




STORM DRAIN PLAN & PROFILE

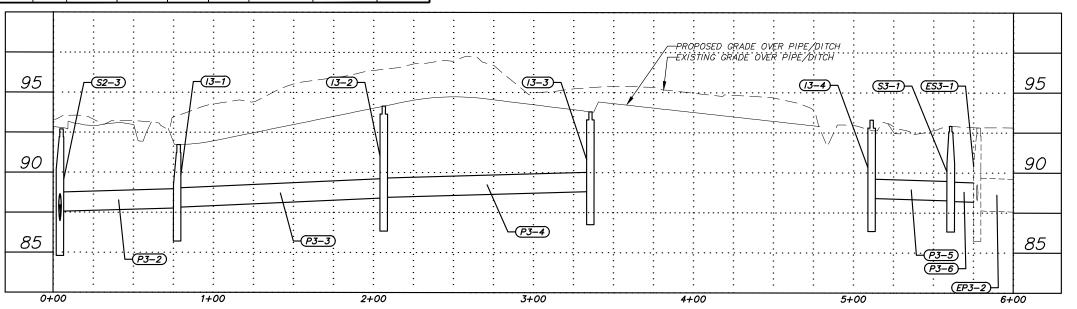
SD1_{of} SD6 HOR. 1"=30





55.02	55.02 - STORM DRAIN													
PIPE	SIZE	PIPE	LENGTH	FROM	ТО	INLET	OUTLET	SLOPE						
NAME	(IN.)	TYPE	(FT.)			ELEVATION	ELEVATION							
EP3-1	12	CMP	-	_	P3-1	-	86.98	-						
P3-1	12	S	7.92	EP3-1	S2-3	86.98	86.96	0.34%						
P3-2	12	S	73.28	13-1	S2-3	87.87	87.66	0.30%						
P3-3	12	S	129.14	13-2	13-1	88.50	87.92	0.46%						
P3-4	12	S	129.18	13-3	13-2	88.92	88.55	0.30%						
P3-5	12	S	49.36	13-4	S3-1	88.51	88.37	0.31%						
P3-6	12	S	16.43	S3-1	ES3-2	88.32	88.28	0.32%						
EP3-2	18	S	_	ES3-2	_	87.91	-	_						

55.04, 55.	55.04, 55.05 & 55.09 - STORM DRAIN STRUCTURES											
STRUCTURE	TYPE OF	TYPE OF	NORTHING	EASTING	TOP OF CASTING	CURB	COMMENTS					
ID	STRUCTURE	CASTING			ELEVATION	TYPE						
13-1	CB MH I	FI	330875.54	345604.89	91.76	N/A	BEEHIVE INLET					
13-2	CB MH I	FI	330875.76	345733.95	94.17	N/A	BEEHIVE INLET					
13-3	CB	FI	330875.99	345863.09	93.83	N/A	BEEHIVE INLET					
13-4	CB	FI	330876.79	346038.85	93.30	N/A	BEEHIVE INLET					
ES3-1	CONNECT	мн	330847.54	346086.49	92.82	N/A	EXISTING MH					
S3-1	MH I	МН	330863.96	346086.49	92.91	N/A						



NOTES:

- 1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARITY.
- 2. REFER TO SHEET SD4 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
- 3. REFER TO SHEETS SD4-SD6 FOR STORM DRAIN DETAILS.

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ı	1. DATA PROVIDED BY:		TITLE:		BASE
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ı	OF THE PROJECT AS CONSTRUCTED.				PROFILE
ı	CONTRACTOR:				STORM :
ı	BY:	TITLE:	DATE:		WATER/
ı	2. DATA TRANSFERRED BY:		TITLE:		GAS
	COMPANY:		DATE:		TELEPHO
ı	3. BASED ON PERIODIC FIELD OBSERVATI				ELECTRI
ı	SUPERVISION), THE CONTRACTOR-PROV				DESIGN
ı	DATA TRANSFER CHECKED BY:				QUANTI
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PROFILE	RB	JK	FIELD BOOKS	BM NO.	LOCATION		ELEV.	REV	DATE	DESCRIPTION	BY	
STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D	See MOA Benchmark	Book, Page D-56	94.77					17
WATER/SANITARY SEWER	RK	JK	195 & 196	CB 7C	See MOA Benchmark	Book, Page D-17	106.10					
GAS	RK	JK	STAKING	GAAB 86	See MOA Benchmark	Book, Page D-18	104.53					
TELEPHONE	RK	JK										ENGINE
ELECTRIC	JH	TK										3940 AR
DESIGN	RB	JK	ASBUILT									ANCHOR
QUANTITIES	RB	JK	CONTRACTOR	BASIS OF	THIS DATUM GAAB 1972	ADJUST						PHON
PRELIMINARY/FINAL	RB	JK	INSPECTOR									i
MUNICIPAL/STATE	RB	JK										1

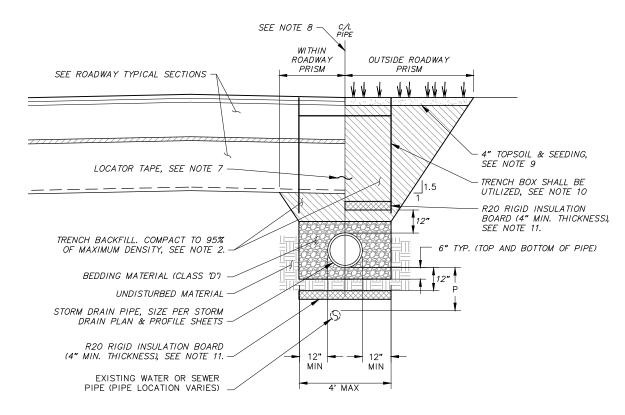






STORM DRAIN PLAN & PROFILE

OALE HOR	. 1"=30'	GRID SW1629, SW1630		SD3.
SCALE VER	. 1"=3'	DATE NOV 2019	STATUS 65%	SHEET / OT SD6



TYPICAL STORM DRAIN TRENCH SECTION SCALE: NTS

STORM DRAIN & SUBDRAIN TRENCH SECTION NOTES:

- 1. TRENCH EXCAVATION AND SHORING SHALL COMPLY WITH ALL LOCAL, STATE, AND OSHA REGULATIONS AND REQUIREMENTS. INDICATED TRENCH WALL SLOPES AND DIMENSIONS ARE FOR PAY QUANTITY DETERMINATIONS ONLY.
- 2. TRENCH BACKFILL SHALL BE NATIVE MATERIAL MEETING TYPE III CLASSIFICATION (MINIMUM) AS APPROVED BY THE ENGINEER. NATIVE MATERIAL NOT MEETING TYPE III CLASSIFICATION SHALL BE REMOVED AND REPLACED WITH TYPE II CLASSIFIED MATERIAL.
- 3. REMOVE AND DISPOSE OF ALL ORGANIC MATERIALS IN ACCORDANCE WITH MASS SECTION 20.13.
- 4. IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, CONTRACTOR SHALL SAW CUT AND REMOVE AN ADDITIONAL 12 INCHES FROM EXISTING PAVEMENT EDGE. THE ENGINEER MAY REQUIRE MORE THAN 12 INCHES ADDITIONAL CUT IF THE EXISTING PAVEMENT HAS BEEN LIFTED IN THE REMOVAL PROCESS, IF THE JOINT DOES NOT OCCUR ON UNDISTURBED MATERIAL, OR IF THE JOINT IS LOCATED WITHIN
- 5. WATER LINES CROSSING STORM DRAIN LINES REQUIRE A MINIMUM VERTICAL SEPARATION OF THREE (3) FEET. INSTALL R20 INSULATION BOARD WHEN 'P' IS LESS THAN 3', AS MEASURED FROM OUTSIDE OF PIPES & WITHIN BEDDING LIMITS, OR AS DIRECTED BY ENGINEER IN FIELD. EIGHTEEN (18) INCHES IS THE MINIMUM INSULATED SEPARATION DISTANCE. IF EIGHTEEN (18) INCHES CAN NOT BE OBTAINED, THE WATER LINE WILL HAVE TO BE RELOCATED.
- 6. WHERE WATER AND STORM DRAIN MAINS CROSS, STORM DRAIN MAIN JOINTS SHALL BE AT LEAST 10 FEET FROM WATER MAIN JOINTS.
- 7. INSTALL DETECTABLE LOCATOR TAPE THREE (3) FEET BELOW FINISH GRADE OR TWO (2) FEET DEEP IN THE STREET STRUCTURAL SECTION PER MASS SECTION 20.13.
- 8. LOCATION OF STORM DRAIN VARIES WITHIN ROADWAY. INSTALL STORM DRAIN AS SHOWN ON STORM DRAIN PLAN & PROFILE SHEETS.
- 9. PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
- 10. TRENCH BOX SHALL BE UTILIZED TO MINIMIZE TRENCH WIDTH AND REDUCE IMPACTS TO ADJACENT PROPERTIES AND RE-VEGETATION. CONTRACTOR SHALL TAKE EXTRA PRECAUTIONS TO AVOID IMPACTS TO TREE PROTECTION ZONES.
- 11. INSTALL R20 INSULATION BOARD:
 - · ABOVE SD PIPE WHEN COVER IS LESS THAN 4' IN AREAS OUTSIDE OF INSULATED ROADWAY SECTION, INSULATION PLACEMENT SHALL CONFORM TO MASS DETAIL 20-9.
 - BELOW SD PIPE WHEN 'P' IS LESS THAN 3', AS MEASURED FROM OUTSIDE OF PIPES & WITHIN BEDDING LIMITS, OR AS DIRECTED BY ENGINEER IN THE FIELD.

GENERAL STORM DRAIN STRUCTURE & PIPE NOTES:

HORIZONTAL AND VERTICAL CONTROL POINTS FOR STORM DRAIN STRUCTURES (REFERENCE POINTS CALLED OUT IN PLAN & PROFILE SHEETS) ARE:

HORZ CONTROL CENTER OF MH STRUCTURE REFERENCE ELEV. FG/TOP OF LID. TBC @ MID. PT. OF CURB INLET HOOD TYPE II MH CENTER OF MH TYPF II CRMH CENTER OF MH TBC @ MID. PT. OF CURB INLET HOOD CATCH BASIN CENTER OF CB CB W/ FIELD INLET CENTER OF CB FG/TOP OF FRAME TYPE'I CBMH W/FIELD INLET CENTER OF MH FG/TOP OF FRAME

PIPE LENGTHS ARE BASED ON THE HORIZONTAL DISTANCE BETWEEN THE CENTER OF CONNECTING STRUCTURES OR FITTINGS. PIPE SLOPES ARE CALCULATED USING THE ACTUAL LENGTH OF PIPE FROM THE INSIDE FACE OF

> HOR, NTS VER. NTS

DATE NOV 2019

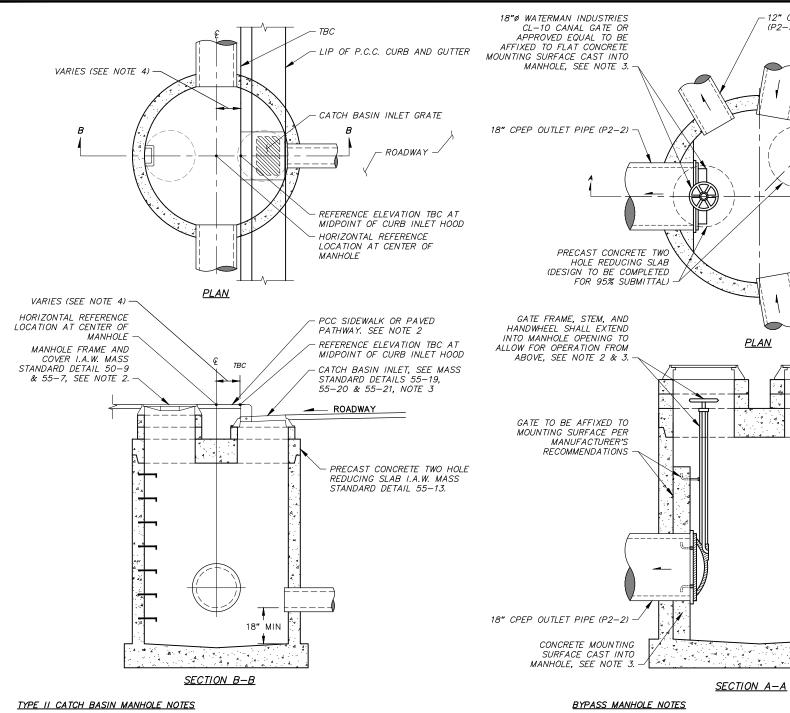
SHEET

STATUS 65%

°fSD6

- 3. UNLESS OTHERWISE NOTED ALL STORM DRAIN MAIN PIPE SHALL BE CPEP, TYPE S.
- 4. THE FOLLOWING ABBREVIATIONS USED ON THE STORM DRAIN STRUCTURE TABLES ON THE PLAN & PROFILES SHEETS ARE DESCRIBED BELOW:
 - CB CATCH BASIN
- CB (RED) REDUCED HEIGHT CATCH BASIN CB MH I CATCH BASIN MANHOLE, TYPE I
- CB MH II CATCH BASIN MANHOLE, TYPE II
- MH I STORM DRAIN MANHOLE, TYPE I
- MH II STORM DRAIN MANHOLF, TYPF II • OGS - OIL AND GRIT SEPARATOR
- CONNECT CONNECT TO STORM DRAIN MANHOLE
- C.I. CURB INLET
- MH MANHOLE FRAME AND LID

RECORD DRAWING	DATA	DRAWN	CHECKED	·	•		_				PROJECT MANAGEMENT AND ENGINEERING
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OF THE PROJECT AS CONSTRUCTED.	PROFILE	RB	JK	FIELD BOOKS	BM NO. LOCATION	ELEV. REV DATE	DESCRIPTION BY		_A_\\\\\	TRAITY OF AVA	16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED B
CONTRACTOR:	STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-5	94.77			* 49 <u>TH</u> *		SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1
BY: DATE: TITLE:	WATER/SANITARY SEWE	er RK	JK	195 & 196	CB 7C See MOA Benchmark Book, Page D-1	7 106.10			49 111		
2. DATA TRANSFERRED BY:	GAS	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18	3 104.53		INCRW	į		STORM DRAIN DETAILS
COMPANY: DATE:	TELEPHONE	RK	JK					ENGINEERING GROUP LLC	🙎 Marliese E. Von Huene 🛭 🛠		STOKWI DIKAHA DETAILS
3 BASED ON PERIODIC FIFLD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL LINDER HIS/HER DIRECT	ELECTRIC	JH	TK						© CE-134461		
5 BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER OF AN INDIVIDUAL UNDER HISTORICE OF SUPERVISION). THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.	DESIGN	RB	JK	ASBUILT				3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503			
DATA TRANSFER CHECKED BY:	QUANTITIES	RB	JK	CONTRACTOR	BASIS OF THIS DATUM GAAB 1972 ADJUST			PHONE: (907) 562-3252 #AECL882-AK	PROFFSSIONAL		
<i>y</i>	PRELIMINARY/FINAL	RB	JK	INSPECTOR				precedez-AK	A STREET, STRE		HOP NITS GRID SW1629, SW1630 SD4
COMPANY: DATE:	MUNICIPAL/STATE	l RR	l .lk								SOALE HOR. NTS GRID SW1629, SW1630 SD4.



- 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE 2015 MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS AS CURRENTLY AMENDED AND AS MODIFIED ON THIS DETAIL.
- 2. SET MANHOLE COVER 1/4-INCH BELOW PCC SIDEWALK OR PAVED PATHWAY FINISH GRADE OR PER MASS STANDARD DETAIL 55-10 FOR ALL OTHER LOCATIONS.
- 3. MH CENTER MAY BE ON ROADWAY SIDE OF CURB LINE IN SOME LOCATIONS. ALIGN CATCH BASIN INLET WITH CURB LINE.

DATE:

4. OFFSET FOR STANDARD INSTALLATION IS 0.95'.

RECORD DRAWING

CONTRACTOR:

COMPANY:

. DATA PROVIDED BY:

. DATA TRANSFERRED BY: _

TYPE II CATCH BASIN MANHOLE DETAIL

SCALE: NTS

BYPASS MANHOLE (\$2-2) DETAIL 2

CAST CONCRETE MOUNTING SURFACE INTO MANHOLE SUCH THAT BYPASS GATE

2. BYPASS GATE STEM SHALL BE NON-RISING TO POSITION HANDWHEEL AT CONVENIENT STATIC OPERATING ELEVATION FROM MANHOLE OPENING ABOVE.

3. BYPASS MANHOLE (STRUCTURE S2-2) SHALL BE PAID FOR UNDER PAY ITEM

4. CATCHBASIN LEADS (P2-4 & P2-5) AND BYPASS PIPE (P2-3) NOT SHOWN IN

<u>PLAN</u>

12" CPEP BYPASS PIPE (P2-3), SEE NOTE 4

12" CPEP CATCH BASIN

LEAD (P2-5), SEE NOTE 4

LADDER LOCATION AND

DETAIL 55-4

12" CPEP CATCH BASIN

LEAD (P2-4), SEE NOTE 4

SEE NOTE 1.

INSTALLATION PER MASS

18" CPEP INLET PIPE (P2-6)

MANHOLE FRAME AND COVER

I.A.W. MASS STANDARD DETAIL 50-9 & 55-7, TYP.

CONCRETE GRADE RING I.A.W. MASS STANDARD

PRECAST CONCRETE TWO

HOLE REDUCING SLAB. DESIGN TO BE PROVIDED

STORM DRAIN MANHOLE TYPE II. SEE MASS STANDARD DETAIL 55-5.

- 18" CPEP INLET PIPE (P2-6)

AT 95% DESIGN.

DETAIL 55-5, TYP.

SCALE: NTS

HANDWHEEL IS CENTERED IN ACCESS OPENING.

55.05 CONSTRUCT (TYPE II) BYPASS MANHOLE.

SECTION A-A FOR CLARITY.

PIPE (P2-1) 18" CPEP INLET PIPE (P2-2) LADDER LOCATION AND INSTALLATION I.A.W. MASS DETAIL 55-4, SEE NOTE 6. MANHOLE FRAME AND COVER I.A.W MASS STANDARD DETAIL PLAN 50-9 & 55-7 CONCRETE GRADE RINGS CONCRETE REDUCING 6" MIN. SLAB 25.5" STORMCEPTOR® INSERT, 4"Ø OIL SEE NOTE 1 INSPECTION PIPE REMOVABLE DROP TEE HANDLE 18" CPEP OUTLET -18" CPEP INLET PIPE PIPE (P2-1) (P2-2)4"Ø OUTLET RISER PERMANENT POOL ELEVATION - 12"ø REMOVABLE DROP TEE SEDIMENT STORAGE SUMP SECTION B-B

OIL & GRIT SEPARATOR NOTES

18" CPEP OUTLET

- 1. OIL AND GRIT SEPARATOR (STRUCTURE OGS2-1) SHALL BE STORMCEPTOR MODEL STC450i MANUFACTURED BY CONTECH ENGINEERED SOLUTIONS LLC OR APPROVED EQUAL.
- 2. ACCESS OPENING THROUGH REDUCING SLAB SHOULD BE POSITIONED OVER THE DROP TEE AND OIL PORT.
- 3. SEE STORM DRAIN PLAN & PROFILE SHEETS FOR INLET AND OUTLET PIPE INVERTS & ORIENTATION AND STRUCTURE INFORMATION.
- 4. LADDER RUNGS NOT SHOWN IN SECTION VIEW FOR CLARITY.

OIL AND GRIT SEPARATOR (OGS2-1) 3 SCALE: NTS

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. 7C | See MOA Benchmark Book, Page D-TITLE AB 86 See MOA Benchmark Book, Page D-18 10 DATE: . BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED. IS OF THIS DATUM GAAB 1972 ADJUS DATA TRANSFER CHECKED BY: ___

CRW FINGINFERING GROUP I C 3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252





PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

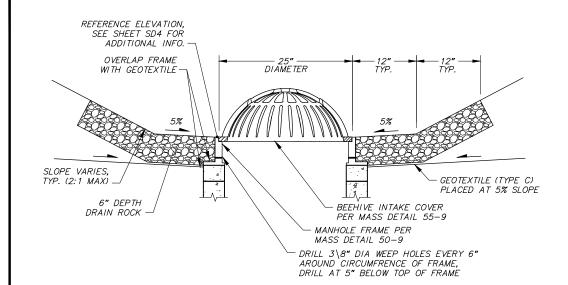
REDUCING SLAB ACCESS TO BE ORIENTATED OVER OIL INSPECTION PIPE AND DROP TEE

48" I.D. MANHOLE STRUCTURE

W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

STORM DRAIN DETAILS

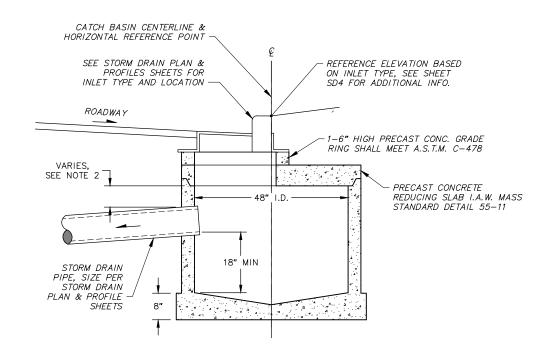
GRID SW1629, SW1630 SD5 HOR, NTS ATE NOV 2019



FIELD INLET NOTES

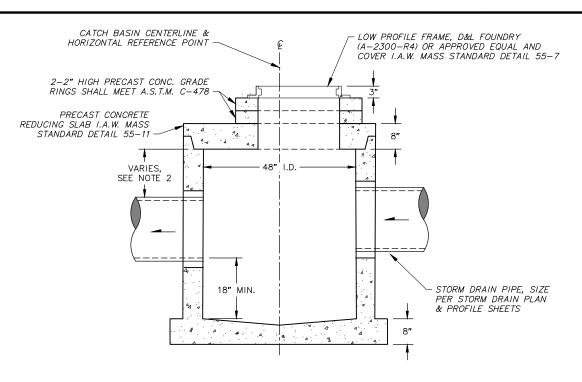
1. DRAIN ROCK AND GEOTEXTILE SHALL BE INCIDENTAL TO PAY ITFM 55.09.

FIELD INLET DETAIL SCALE: NTS



REDUCED HEIGHT CATCH BASIN

SCALE: NTS

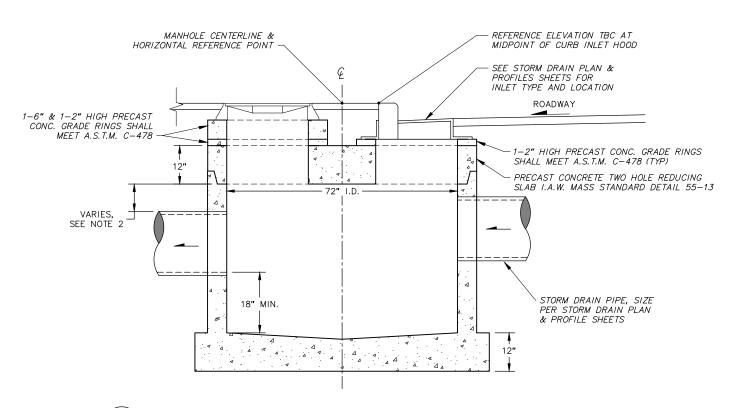


REDUCED HEIGHT TYPE 1 MANHOLE

SCALE: NTS

REDUCED HEIGHT STRUCTURE NOTES

- 1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE 2015
 MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS (MASS) AS CURRENTLY AMENDED AND AS MODIFIED ON THESE DETAILS.
- 2. BASE SECTION HEIGHT BETWEEN TOP OF PIPE AND REDUCING SLAB SHALL BE REDUCED AS NECESSARY TO FACILITATE THE CONSTRUCTION OF THE STORM DRAIN AS SHOWN ON THE PLAN AND PROFILE SHEETS.
- 3. REDUCED HEIGHT CATCH BASIN SHALL BE PAID FOR UNDER PAY ITEM 55.09, CONSTRUCT CATCH BASIN
- 4. REDUCED HEIGHT MANHOLE AND CATCHBASIN MANHOLE SHALL BE PAID FOR UNDER PAY ITEM 55.05, CONSTRUCT (TYPE I) MANHOLE & CONSTRUCT (TYPE II) CATCH BASIN MANHOLE.



REDUCED HEIGHT TYPE 2 CATCH BASIN MANHOLE

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STORM SEWER	MV	JH	DESIGN CRW BOOK No. 149, 169,	CB 7D See MOA Benchmark Book, Page D-56 94.77	I 7
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GAS	RK	JK	STAKING	GAAB 86 See MOA Benchmark Book, Page D-18 104.53	
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CRV SINEERING GROUP, LIC 400 ARCTIC BLVD, SUITE 300 ANCHORACE, ALASKA 99503 PHONE: 1907) 362–3252 PHONE: 1907) 362–3252	49 IH And Mariese E. Von Huene CE-134461 Profess IOM	
CONSULTANT	SEAL	

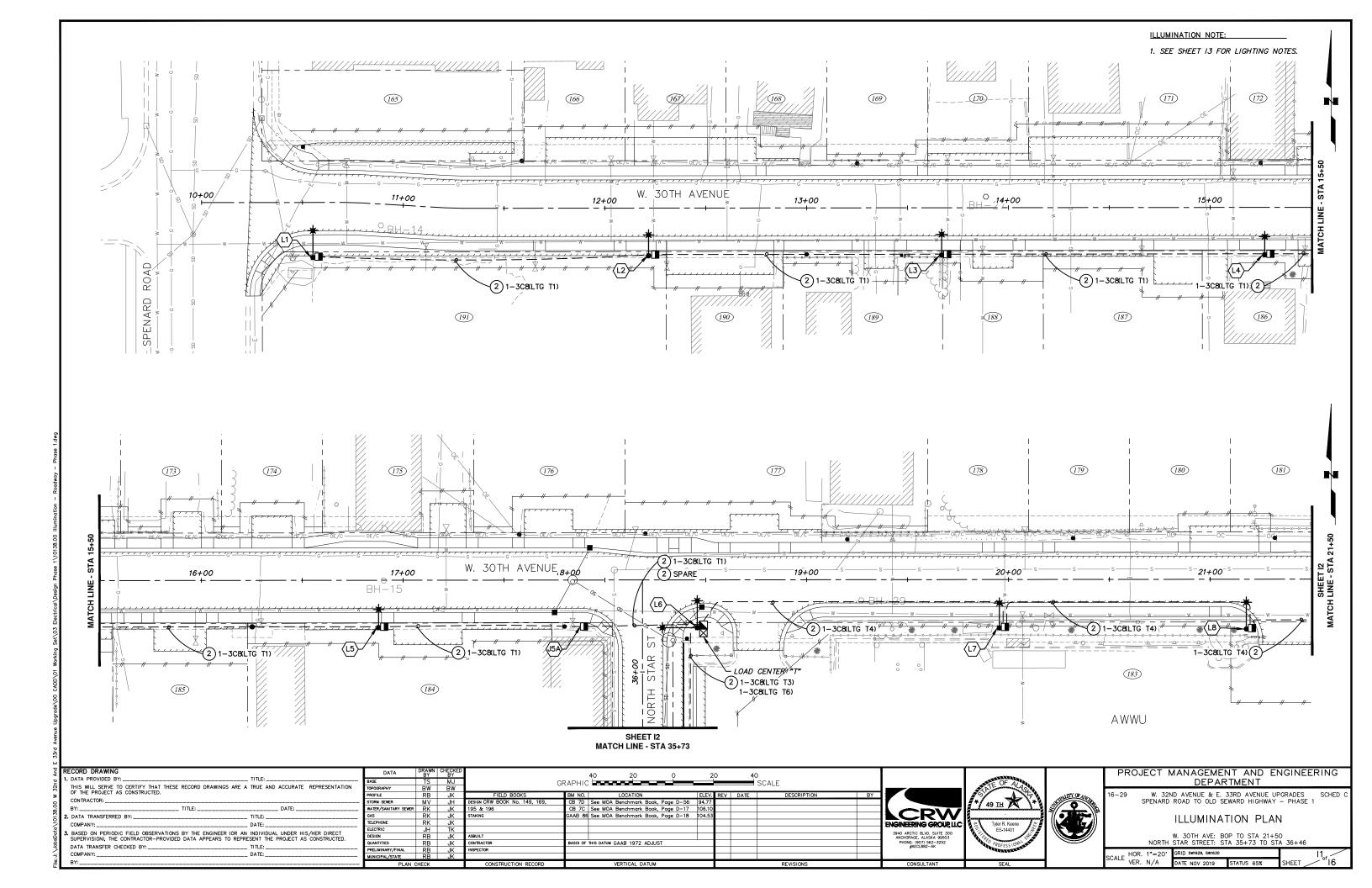


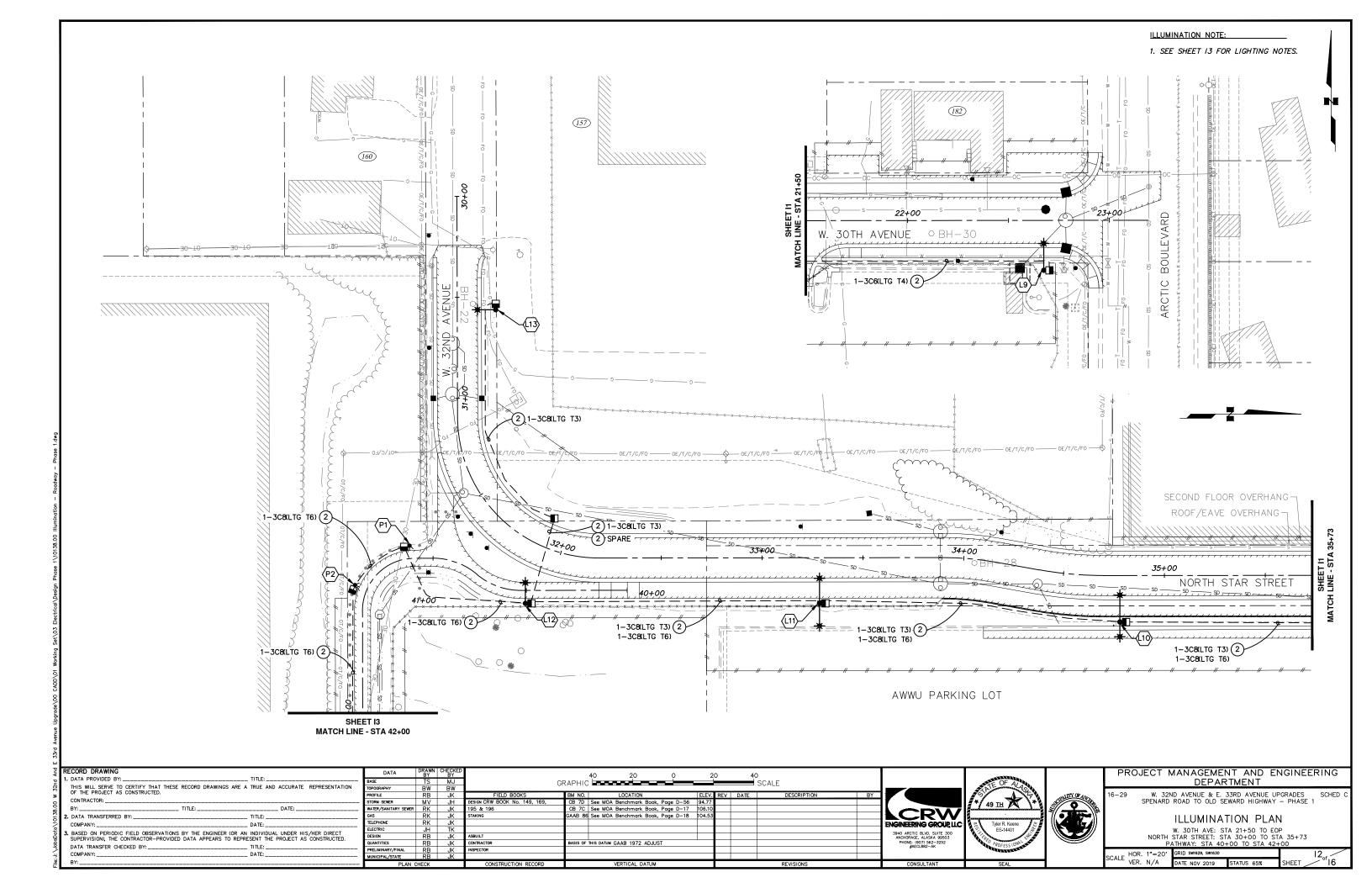
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 16-29

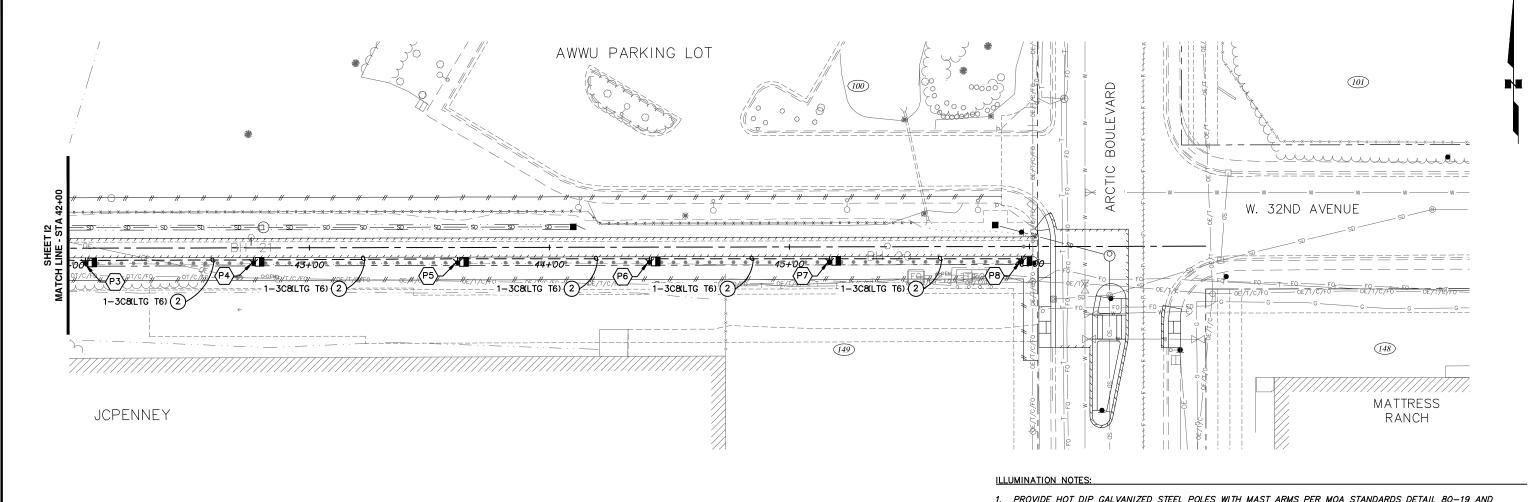
W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED (
SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

STORM DRAIN DETAILS

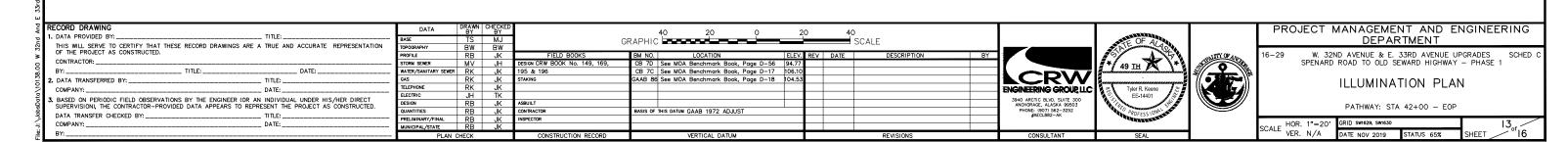
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- 1. PROVIDE HOT DIP GALVANIZED STEEL POLES WITH MAST ARMS PER MOA STANDARDS DETAIL 80-19 AND 80-20, RESPECTIVELY.
- 2. ALL LUMINAIRE POLE FOUNDATIONS SHALL BE DRIVEN PILE UNLESS OTHERWISE NOTED ON THE DRAWINGS. DUE TO LIMITED ROW, LUMINAIRE POLE FOUNDATION SHALL BE LOCATED A MINIMUM OF 6 INCHES INSIDE OF ROW. WHEN POLE LOCATION IS WITHIN 10' OF A UTILITY, EXCAVATE A HOLE TO 12" BELOW ANTICIPATED UTILITIES DEPTH WITH A VACTOR TRUCK BEFORE DRIVING PILE. SEE MASS DETAIL 80-13.
- 3. INSTALL THE POLES WITH FIXED BASES PER MOA DETAIL 80-21.
- 4. LUMINAIRES APPROVED FOR SUBSTITUTION SHALL PROVIDE THE LIGHT LEVELS AND UNIFORMITIES INDICATED IN THE LIGHT LEVELS TABLE.
- 5. PROVIDE THE POLE SHAFT LENGTHS AND MAST ARM LENGTHS SHOWN IN THE POLE SCHEDULE.
- 6. PROVIDE RIGID METAL CONDUIT (RMC) WITH A BARE, STRANDED #8 AWG COPPER GROUND FOR ALL RACEWAYS.
- 7. PROVIDE ONE SPARE 2" RMC WITH PULL ROPE BETWEEN THE JUNCTION BOXES ADJACENT TO EVERY ROAD CROSSING.
- 8. PROVIDE A 3 CONDUCTOR CABLE FOR EACH BRANCH CIRCUIT. SIZE AS SHOWN ON THE DRAWINGS.
- 9. INSTALL THE JUNCTION BOX WITHIN 3' OF THE POLE OR LOAD CENTER. DO NOT INSTALL JUNCTION BOXES IN SIDEWALKS, PATHWAYS OR TRAILS. JUNCTION BOXES INSTALLED BEHIND SIDEWALKS, PATHWAYS OR TRAILS SHALL HAVE A MINIMUM SETBACK OF 2' AND BE PLACED BEHIND OR ON THE DOWN TRAFFIC SIDE OF FOUNDATIONS.
- 10. IN THE DRAWINGS, EACH JUNCTION BOX HAS THE SAME IDENTIFYING NUMBER AS THE LIGHT POLE OR LOAD CENTER NEXT TO IT. FOR JUNCTION BOXES LOCATED BETWEEN POLES, THE IDENTIFYING NUMBER INCLUDES THE SMALLER OF THE TWO POLE NUMBERS BETWEEN WHICH THE JUNCTION BOX IS LOCATED AND AN "A" SUFFIX.
- 11. INSTALL PEDESTRIAN LIGHT COLUMNS ON CONCRETE FOUNDATIONS PER DETAILS ON SHEET 16.
- 12. COORDINATE WITH CEA FOR REMOVAL OF UTILITY OWNED LUMINAIRES WITHIN THE PROJECT AREA.



CONDUIT SIZE -

OF CABLES -

OF CONDUCTORS

PER CABLE

(2)1-3C8LTG T3)

- CIRCUIT #

- TYPE OF

CIRCUIT

└ SIZE OF CONDUCTORS

	LUMINAIRE SUMMARY												
TYPE	SYMBOL	MAKE	MODEL	LAMP	CCT*	DISTRIBUTION	VOLTAGE	DRIVE CURRENT	COLOR	OPTIONS	MOUNT		
ROADWAY	ROADWAY CREE LEDWAY SEE				4000K	SEE ROADWAY LUMINAIRE SCHEDULE	240	525mA	SILVER	SEE ROADWAY LUMINAIRE SCHEDULE	MAST ARM		
PEDESTRIAN LIGHT COLUMN	•	PHILIPS LUMEC	ULLC100	32 LED	4000K	MEDIUM, TYPE 3	240	SEE PEDESTRIAN LIGHT COLUMN SCHEDULE	RAL XXXX	7-PIN RECEPTACLE, UPLIGHT DEFLECTOR	-		

*CCT = CORRELATED COLOR TEMPERATURE

	LIGHT LEVELS TABLE												
LOCATION	MOA REQUIRED MIN. AVERAGE ILLUMINANCE (FC)	AVERAGE DESIGN ILLUMINANCE (FC)	MOA REQUIRED MAXIMUM UNIFORMITY RATIO	DESIGN UNIFORMITY RATIO	MOA REQUIRED MAX. VEILING LUMINANCE RATIO	DESIGN VEILING LUMINANCE RATIO							
30TH AVENUE	0.7	0.9	6.0:1	2.9:1	0.4:1	0.4:1							
NORTH STAR STREET	0.7	0.9	6.0:1	3.1:1	-	-							
TRAIL - ARCTIC BOULEVARD to NORTH STAR STREET	0.5	1.4	10:1	7.0:1	-	_							
30TH AVENUE/NORTH STAR STREET INTX	1.4	1.4	6.0:1	1.6:1	_	_							

NOTES:

- 1. MOA REQUIREMENTS ARE FROM 2007 DCM CHAPTER 5 FOR A LOCAL ROADWAY WITH MEDIUM PEDESTRIAN CONFLICT (MEDIUM DENSITY RESIDENTIAL).
- 2. LIGHT LOSS FACTOR (LLF) = 0.93 FOR ROADWAY LUMINAIRES, 0.882 FOR PATHWAY LUMINAIRES.
- 3. MOUNTING HEIGHTS ARE 30' FOR ROADWAY LUMINAIRES, 12' FOR PEDESTRIAN LIGHT COLUMN.

	ROADWAY LUMINAIRE SCHEDULE												
POLE	STATION	OFFSET	SHAFT LENGTH	MAST ARM LENGTH	# OF LED	DISTRIBUTION	OPTIONS	CIRCUIT					
L1	10+55.2	27.29 RT	26'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1					
L2	12+21.6	23.63 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1					
L3	13+66.9	23.50 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1					
L4	15+27.0	23.50 RT	28'	8'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1					
L5	16+87.8	23.50 RT	28'	8'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T1					
L6	10 10 15 7 07 00 07	18+45.7 23.68 RT	23.68 RT	23.68 RT	23.68 RT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4		
	10+45.7	25.06 KT	20	16'**	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4					
L7	19+95.4	23.98 RT	27'	11'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4					
L8	21+17.8	24.71 RT	28'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4					
L9	22+67.4	24.76 RT	28'	12'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T4					
L10	34+77.9	23.00 RT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	Т3					
	34+77.9	23.00 KT	20	6'	80	TYPE 3, MEDIUM	7 PIN RECEPTACLE	13					
111	33±29 /	22.50 PT	26'	12'	40	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T3					
	L11 33+28.4 22.50 RT 26'		20	6'	80	TYPE 3, MEDIUM	7 PIN RECEPTACLE	13					
L12	31+92.8	29.46 RT	28'	9'	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE	T3					
L13	30+56.2	19.00 LT	28'	9,	60	TYPE 2, MEDIUM	7 PIN RECEPTACLE, BACKLIGHT SHIELD	T3					
EACT													

	PEDESTRIAN LIGHT COLUMN SCHEDULE												
POLE	STATION	OFFSET	HEIGHT	LAMP	DRIVE CURRENT	CIRCUIT							
P1	41+11.1	6.33 RT	12'	32 LED	350mA	T6							
P2	41+39.3	6.33 RT	12'	32 LED	350mA	T6							
Р3	42+06.7	6.34 RT	12'	32 LED	350mA	T6							
P4	42+76.7	6.33 RT	12'	32 LED	530mA	T6							
P5	43+61.7	6.33 RT	12'	32 LED	530mA	T6							
P6	44+41.7	6.33 RT	12'	32 LED	350mA	T6							
P7	45+16.7	6.33 RT	12'	32 LED	350mA	T6							
P8	45+96.7	6.33 RT	12'	32 LED	350mA	T6							

		JUNCTI	ON BOX SCI	HEDULE							
J-BOX TYPE CIRCUIT STATION OFFSE											
	J5A	1A	T1	17+89.6	23.08 RT						
	J12A	1A	T3	31+90.3	15.00 LT						
	NOTE: JUNC	TION BOX NOT CENTER AF	ASSOCIATED WI		LE OR LOAD						

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PROJECT MANAGEMENT AND ENGINEERING
DEPARTMENT

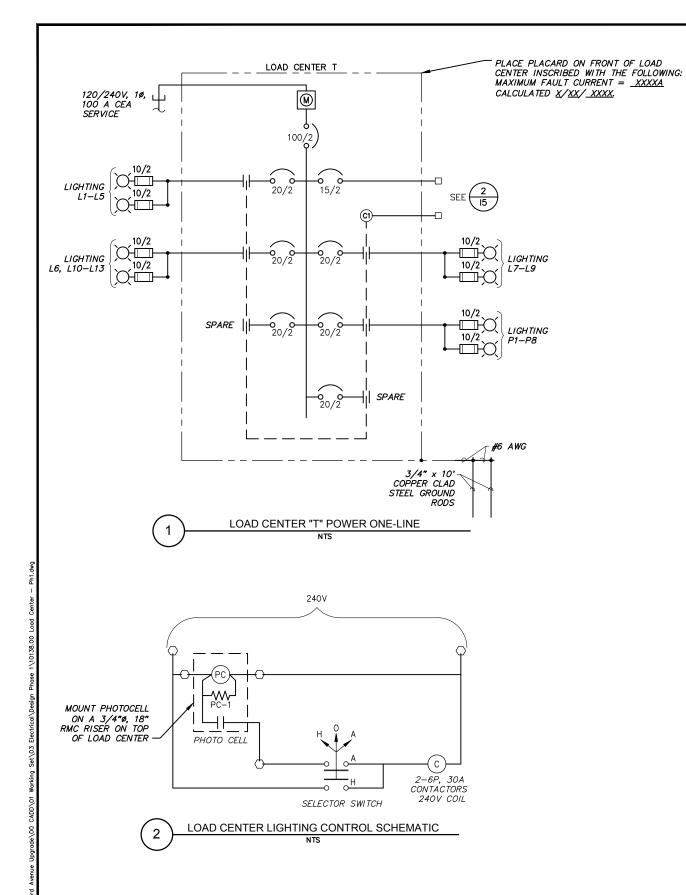
16-29 W. 32ND AVENUE & E. 33RD AVENUE UPGRADES SCHED C
SPENARD ROAD TO OLD SEWARD HIGHWAY - PHASE 1

ILLUMINATION SCHEDULES

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DATE NOV 2019 STATUS 65% SHEET

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3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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DATE:

LOAD CENTER NOTES:

- 1. PLACARDS FOR LOAD CENTERS SHALL HAVE PLACARDS FOR LOAD CENTERS SHALL HAVE SUFFICIENT DURABILITY TO WITHSTAND THE ENVIRONMENT INVOLVED. CONTACT ENGINEER PRIOR TO ORDER OF PLACARD TO VERIFY MAXIMUM FAULT CURRENT.
- 2. LABEL THE FRONT WITH 3M SCOTCH CAL REFLECTIVE DECALS NOTING OWNERSHIP: MOA, PURPOSE: LU (ILLUMINATION) AND THE
- 3. PROVIDE ARC FLASH WARNING LABELS WITH INCIDENT ENERGY VALUES AND PERSONAL PROTECTIVE EQUIPMENT (PPE) ON EACH PIECE OF EQUIPMENT IN ACCORDANCE WITH NEC ARTICLE 110.16 AND NFPA 70E.

		VOLTA	GE DROP		
CIRCUIT	SIZE	LENGTH	VOLTAGE	CURRENT	V.D.
T1	#8 AWG	789'	240V	2.2A	1.0%
Т3	#8 AWG	618'	240V	3.3A	1.2%
T4	#8 AWG	464'	240V	1.3A	0.4%
T6	#8 AWG	1023'	240V	3.2A	2.0%

 MN NO.
 Leconion

 GB 7D See MOA Benchmark Book, Page D-56
 94.777

 CB 7C See MOA Benchmark Book, Page D-17
 106.10

 AAB 86 See MOA Benchmark Book, Page D-18
 104.53

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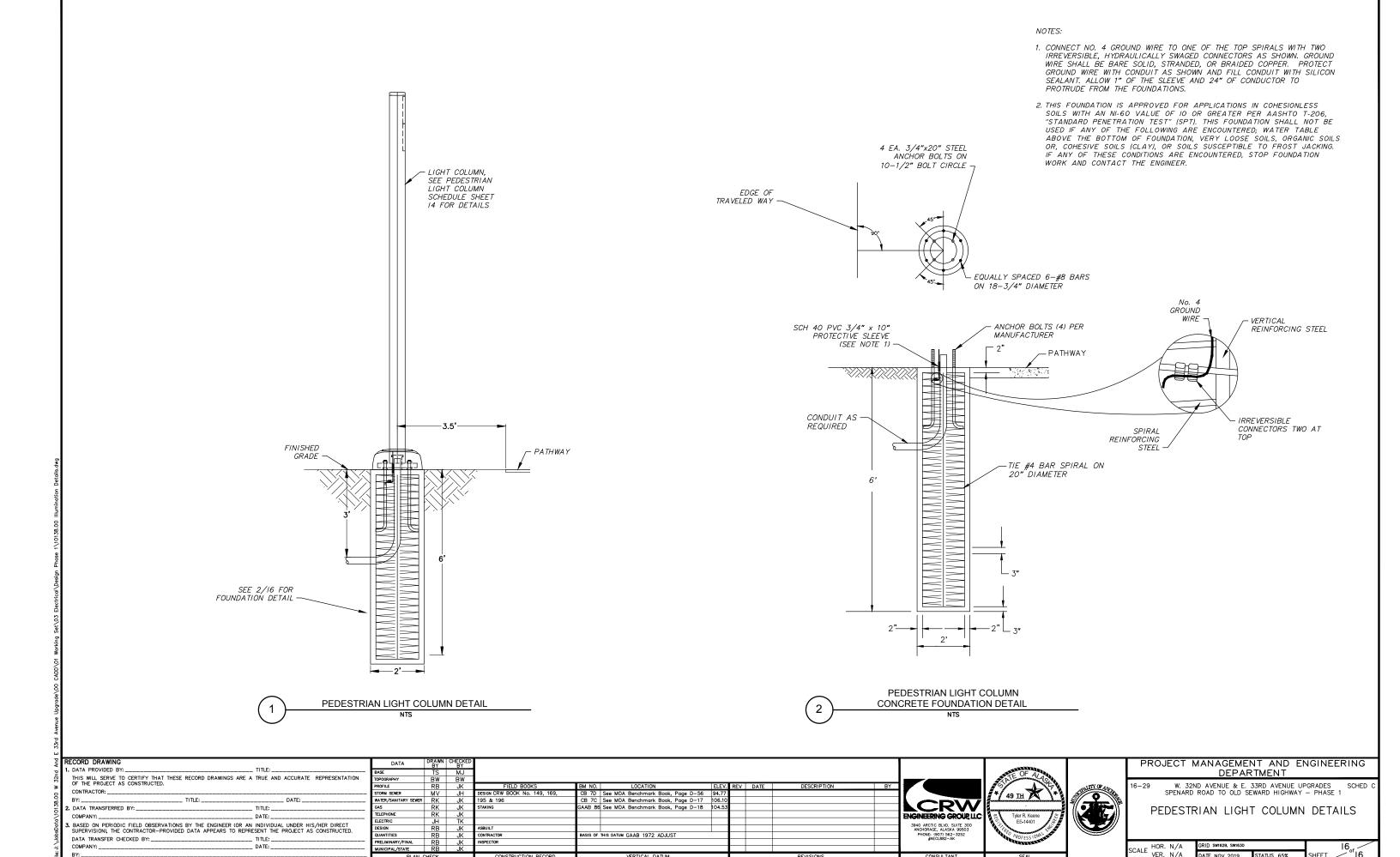
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		MAIN BRE	LAKER A:	2 POLE	, 10	MP	S, 240	VOLIS		
PANEL	A 100	AMPS MAIN LU	GS,		120/240	VOLTS 10,00	0	SINGLE	E PHASE 3 AMPS INTERRUPT CAPACITY	WIRE
CKT.	CIRCUIT D	DESCRIPTION	KVA	AMP		● N	AMP	KVA	CIRCUIT DESCRIPTION	CKT.
T1	LUMINAIRES L1-L	.5	0.5	20/2	1 - 3 -	2 4	15/2	0.2	PHOTOELECTRIC CONTROL	T2
Т3	LUMINAIRES L6, I	_10-L13	0.8 20,		5 - 7 -	6 8	20/2	0.3	LUMINAIRES L7-L9	T4
T5	SPARE	RE			9 11	10	20/2	0.7	PEDESTRIAN LUMINAIRES P1-P8	Т6
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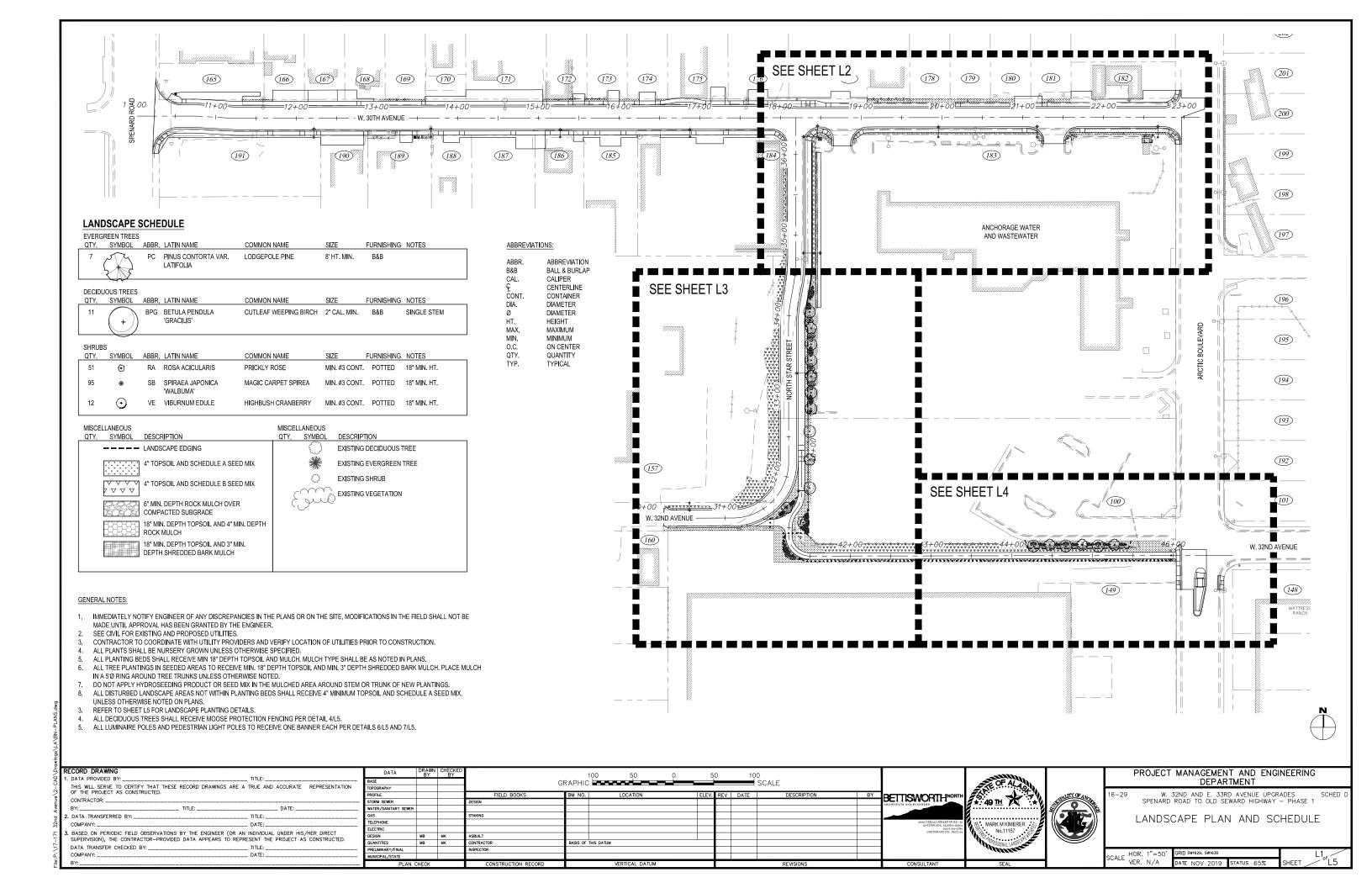
3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AECL882-AK

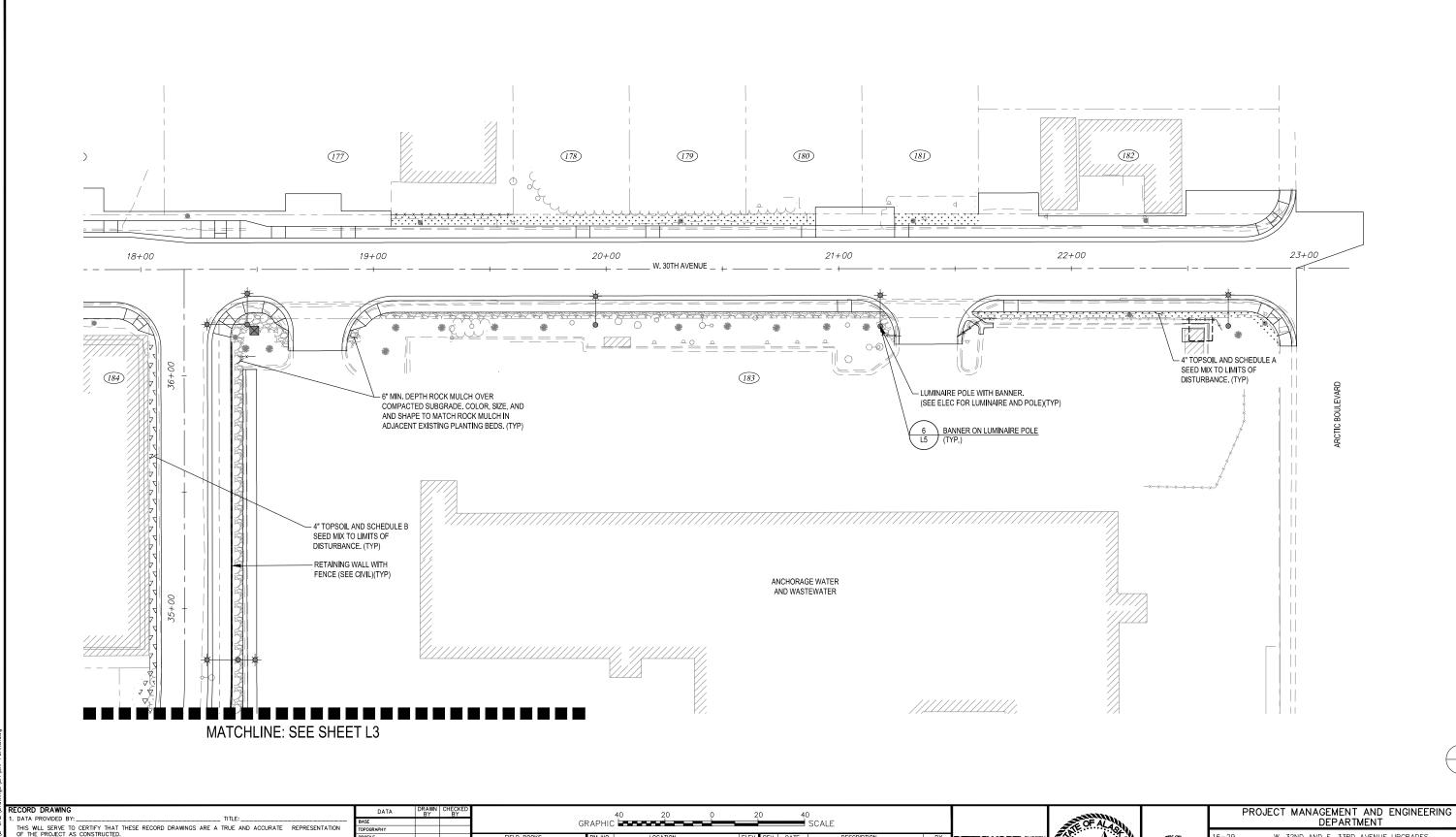
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LCRW ENGINEERING GROUP, LLC	Tyler R. Keene	2 E		LOAD CENTER DETAILS	

Tyler R. Keene EE-14401

GRID SW1629, SW1630 SCALE HOR. N/A DATE NOV 2019 STATUS 65%







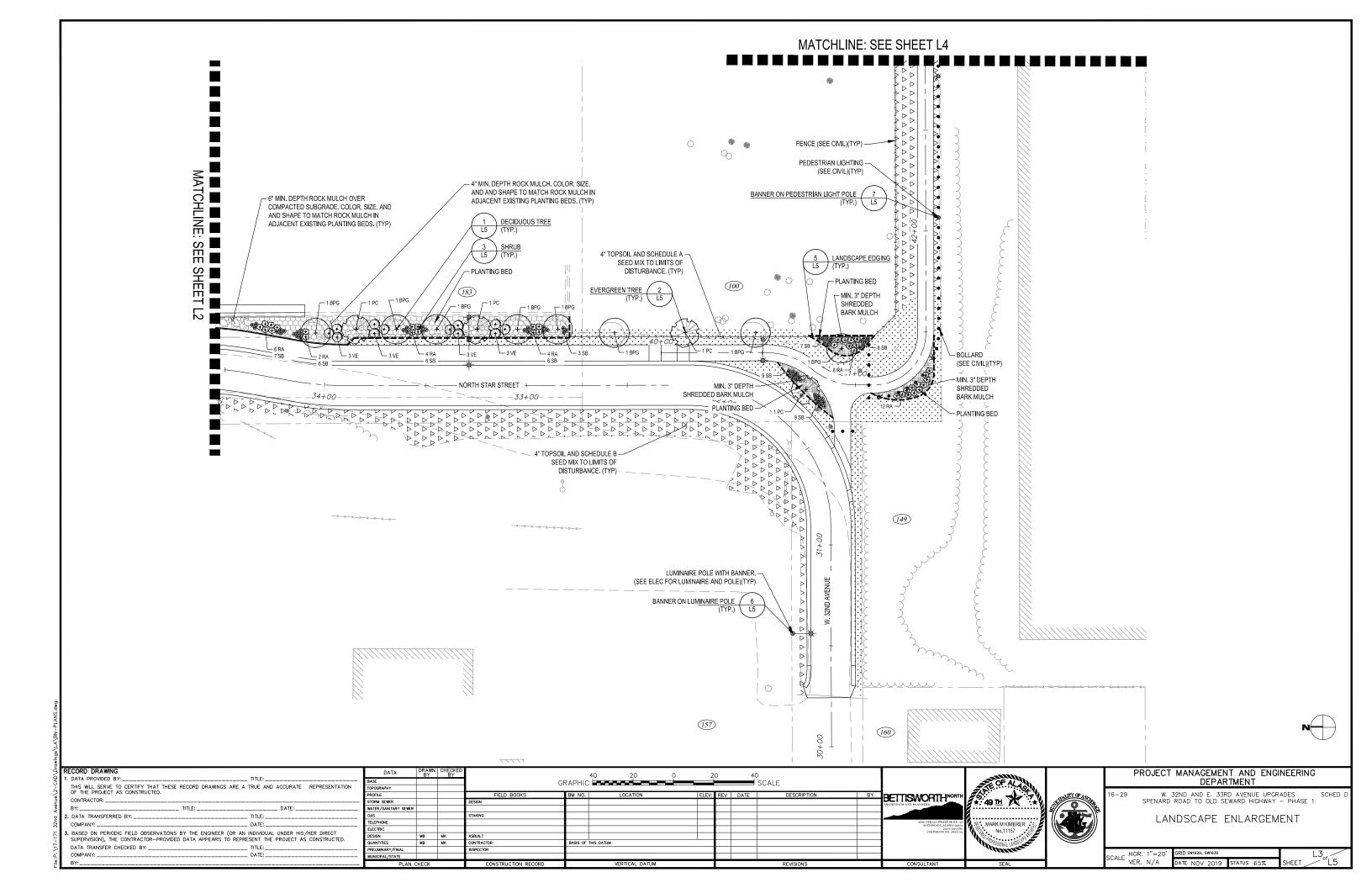
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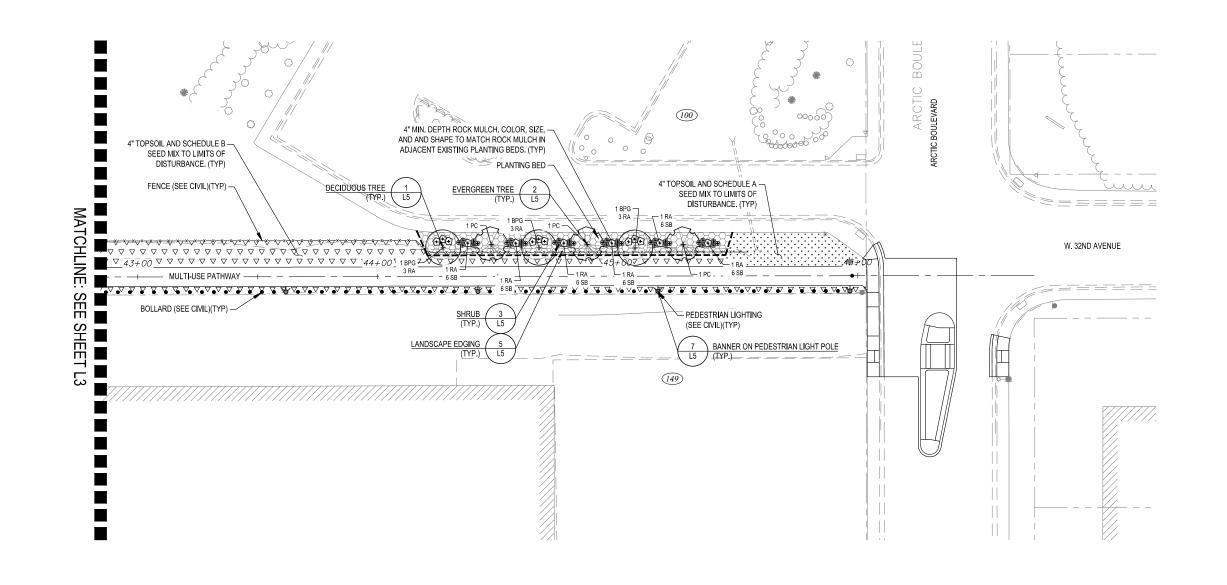
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W. 32ND AND E. 33RD AVENUE UPGRADES SCHED SPENARD ROAD TO OLD SEWARD HIGHWAY — PHASE 1

LANDSCAPE ENLARGEMENT







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